*ONLY STANDARD SHEETS MARKED WITH AN "✓" ARE IN THIS PROJECT #

**REVISED OR ADDED

✓*	SHEET NO.	TITLE	APPROVAL DATE**
	HW-506_01	ENDWALLS, SLOPE PAVED INLETS AND OUTLETS	1-26-12
	HW-506_02	TYPE "D-G" & "L" ENDWALLS	7-13-12
	HW-506_03	ENDWALLS FOR PIPE ARCH	9-18-09
✓	HW-507_01	TYPE "C", "C-L" & DROP INLET CATCH BASIN	7-24-13
	HW-507_02	TYPE "C", "C-L" & DOUBLE GRATE TYPE - I	7-24-13
	HW-507_03	TYPE "C", "C-L" & DOUBLE GRATE TYPE - II	7-24-13
	HW-507_04	TYPE "C", "C-L" & ROUND PRECAST CONCRETE CB	11-10-11
	HW-507_05	TYPE "C" & "C-L" PRECAST CONCRETE CB DOUBLE GRATE TYPE - I	11-10-11
	HW-507_06	TYPE "C" & "C-L" PRECAST CONCRETE CB DOUBLE GRATE TYPE - II	11-10-11
✓	HW-507_07	TYPE "C" & "C-L" CATCH BASIN TOPS AND CURBS	11-10-11
✓	HW-507_08	CATCH BASIN FRAMES AND GRATES	9-18-09
	HW-507_09	HEAVY DUTY LOCK DOWN TOPS	7-12-12
✓	HW-507_10	MANHOLE - FRAME & COVER	7-24-13
	HW-601_01	FIGURES FOR DATES ON BRIDGE PARAPETS	6-09-11
✓	HW-651_01	C.C.M. PIPE INSTALLATIONS IN FILL & ROCK SLOPES & PIPE TRENCH DETAIL	7-24-13
	HW-651_02	SLOTTED DRAIN PIPE 12"- 15"-18"-24"-30" (305-381-457-610-762)	7-12-12
✓	HW-652_01	PIPE ENDS	7-24-13
	HW-751_01	UNDERDRAINS AND UNDERDRAIN OUTLETS	7-12-12
	HW-803_01	PAVED DITCH AND PAVED APRON	7-12-12
✓	HW-811_01	CURBING	7-12-12
	HW-813_01	GRANITE STONE TRANSITION CURBING	7-24-13
	HW-821_01a	TRANSITION 45" (1145) F-SHAPE TO 45" (1145) VERTICAL SHAPE SHEET 1	1-26-12
	HW-821_01b	TRANSITION 45" (1145) F-SHAPE TO 45" (1145) VERTICAL SHAPE SHEET 2	10-18-10
	HW-821_01c	TRANSITION 45" (1145) F-SHAPE TO 45" (1145) VERTICAL SHAPE SHEET 3	1-26-12
✓	HW-821_02a	45" (1145) F-SHAPE PRECAST CONCRETE BARRIER CURB SHEET 1	7-24-13
✓	HW-821_02b	45" (1145) F-SHAPE PRECAST CONCRETE BARRIER CURB SHEET 2	7-24-13
	HW-821_03a	TRANSITION - 32" (813) JERSEY SHAPE TO 45" (1145) VERTICAL SHAPE SHEET 1	1-26-12
	HW-821_03b	TRANSITION - 32" (813) JERSEY SHAPE TO 45" (1145) VERTICAL SHAPE SHEET 2	10-18-10
	HW-821_03c	TRANSITION - 32" (813) JERSEY SHAPE TO 45" (1145) VERTICAL SHAPE SHEET 3	10-18-10
	HW-821_03d	TRANSITION - 32" (813) JERSEY SHAPE TO 45" (1145) VERTICAL SHAPE SHEET 4	10-18-10
	HW-821_03e	TRANSITION - 32" (813) JERSEY SHAPE TO 45" (1145) F-SHAPE	7-24-13
	HW-821_04a	MERRITT PARKWAY NARROW MEDIAN BARRIER	6-09-11
	HW-821_04b	MERRITT PARKWAY - 2' (610) WIDE MEDIAN BARRIER AND ROADSIDE BARRIER	7-24-13

✓*	SHEET NO.	TITLE	APPROVAL DATE**
	HW-821_05a	TRANSITION - 45" (1145) F-SHAPE TO 54" (1372) VERTICAL SHAPE SHEET 1	1-26-12
	HW-821_05b	TRANSITION - 45" (1145) F-SHAPE TO 54" (1372) VERTICAL SHAPE SHEET 2	1-26-12
	HW-821_06	54" (1372) VERTICAL SHAPE BARRIER	2-6-12
	HW-821_07	MISCELLANEOUS DETAILS FOR BARRIER TRANSITIONS	7-12-12
	HW-822_01	TEMPORARY PRECAST CONCRETE BARRIER CURB	7-24-13
	HW-905_01	FENCES AND BARWAYS	7-13-12
✓	HW-910_01	W- BEAM METAL BEAM RAIL HARDWARE	6-09-11
✓	HW-910_02	METAL BEAM RAIL (TYPE R-B 350) GUIDERAIL	6-09-11
	HW-910_03	METAL BEAM RAIL (TYPE MD-B 350)	6-09-11
	HW-910_04	METAL BEAM RAIL (TYPE R-B 350) SYSTEMS 5, 5A, & 6	6-09-11
	HW-910_05	METAL BEAM RAIL R-B 350 SPAN TYPE I, II, III SECTIONS	7-24-13
✓	HW-910_06	R-B 350 BRIDGE ATTACHMENT SAFETY SHAPE PARAPET	6-09-11
	HW-910_07	R-B 350 BRIDGE ATTACHMENT VERTICAL SHAPE PARAPET	6-09-11
	HW-910_08	R-B 350 BRIDGE ATTACHMENT TRAILING END	6-09-11
	HW-910_09a	MISCELLANEOUS GUIDERAIL TRANSITIONS SHEET 1	1-26-12
	HW-910_09b	MISCELLANEOUS GUIDERAIL TRANSITIONS SHEET 2	7-25-12
	HW-910_10	METAL BEAM RAIL 8" (203) X 6" (152) BOX BEAM	7-24-13
	HW-910_11	CURVED GUIDERAIL TREATMENT DETAIL	7-25-12
	HW-910_12a	MERRITT PARKWAY GUIDERAIL ATTACHMENT - SYSTEM 2 & 3	7-24-13
	HW-910_12b	MERRITT PARKWAY GUIDERAIL	7-24-13
	HW-910_12c	MERRITT PARKWAY GUIDERAIL TRAILING END ATTACHMENTS	7-24-13
	HW-910_12d	MERRITT PARKWAY MEDIAN GUIDERAIL AND END ANCHOR	6-09-11
	HW-910_13a	THRIE-BEAM METAL BEAM RAIL HARDWARE	7-24-13
	HW-910_13b	THRIE-BEAM TRANSITIONS	7-24-13
	HW-910_14a	THRIE-BEAM 350 BRIDGE ATTACHMENT	6-09-11
	HW-910_14b	THRIE-BEAM 350 GUIDERAIL TRANSITION TO R-B 350 GUIDERAIL	6-09-11
	HW-910_15	MD-B 350 MEDIAN BARRIER SAFETY SHAPE ATTACHMENT TYPE I	6-09-11
	HW-910_16	MD-B 350 MEDIAN BARRIER SAFETY SHAPE ATTACHMENT TYPE II	6-09-11
	HW-910_17	R-B TERMINAL SECTION	7-24-13
	HW-910_18	METAL BEAM RAIL (TYPE MD-I)	10-18-10
	HW-910_19a	METAL BEAM RAIL (MODIFIED TYPE R-I) AND END ANCHORAGE TYPE I	7-24-13
	HW-910_19b	METAL BEAM RAIL (MODIFIED TYPE R-I) AND END ANCHORAGE TYPE II	7-24-13
	HW-910_19c	METAL BEAM RAIL (MODIFIED TYPE R-I) SYSTEMS 2 AND 3	7-24-13

REV.	DATE	REVISION DESCRIPTION
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-
1	7-24-13	REVISED 23 SHEETS

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 7/25/2013

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_HIGHWAY_STD2013.dgn Model: 1 - HW-INX_1

CTDOT
STANDARD SHEET

OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

HIGHWAY STANDARD SHEET INDEX

STANDARD SHEET NO.:

HW_INX

*ONLY STANDARD SHEETS MARKED WITH AN "✓" ARE IN THIS PROJECT #

**REVISED OR ADDED

✓*	SHEET NO.	TITLE	APPROVAL DATE**
✓	HW-911_01	R-B END ANCHORAGE TYPE I AND II	7-24-13
	HW-911_02	MD-B END ANCHORAGE TYPE I	10-18-10
	HW-911_03	ANCHOR IN EARTH CUT SLOPE & ANCHOR IN ROCK CUT SLOPE	10-18-10
	HW-911_04	TYPICAL GRADING PLAN FOR W-BEAM GUIDERAIL TURN-DOWN END ANCHOR	10-18-10
	HW-911_05	MERRITT PARKWAY GUIDERAIL END ANCHORS	7-24-13
✓	HW-913_01	CHAIN LINK FENCE	7-12-12
	HW-918_01a	THREE CABLE GUIDERAIL (I-BEAM POSTS) SHEET 1	7-24-13
	HW-918_01b	THREE CABLE GUIDERAIL (I-BEAM POSTS) SHEET 2	1-26-12
	HW-918_01c	THREE CABLE GUIDERAIL (I-BEAM POSTS) SHEET 3	7-24-13
✓	HW-921_01	DRIVEWAY RAMPS AND SIDEWALKS	1-26-12
✓	HW-921_02a	SIDEWALK RAMPS SHEET 1	7-24-13
✓	HW-921_02b	SIDEWALK RAMPS SHEET 2	7-24-13
✓	HW-921_02c	SIDEWALK RAMPS SHEET 3	7-24-13
✓	HW-921_02d	SIDEWALK RAMPS SHEET 4	7-24-13
	HW-925_01	PAVEMENT FOR RAILING	6-09-11
✓	HW-949_01	PLANTING DETAILS FOR TREES	7-12-12
	HW-949_02	PLANTING DETAILS FOR SHRUBS	7-12-12
	HW-1800_01	GRADING PLAN FOR TYPE B IMPACT ATTENUATION SYSTEM (FLARED)	6-20-11
	HW-1800_02	GRADING PLAN FOR TYPE B IMPACT ATTENUATION SYSTEM (MEDIAN/GORE)	6-09-11
	HW-1800_03	GRADING PLAN FOR TYPE B IMPACT ATTENUATION SYSTEM (TANGENTIAL)	6-20-11
✓	HW-1806_01a	CT TRUCK MOUNTED IMPACT ATTENUATOR SHEET 1	10-18-10
✓	HW-1806_01b	CT TRUCK MOUNTED IMPACT ATTENUATOR SHEET 2	10-18-10
✓	HW-1806_01c	CT TRUCK MOUNTED IMPACT ATTENUATOR SHEET 3	10-18-10

✓*	SHEET NO.	TITLE	APPROVAL DATE**

REV.	DATE	REVISION DESCRIPTION
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-
1	7/24/13	REVISED 8 SHEETS

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Plotted Date: 7/25/2013

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_HIGHWAY_STD2013.dgn Model: 2 - HW-INX_2

CTDOT
STANDARD SHEET

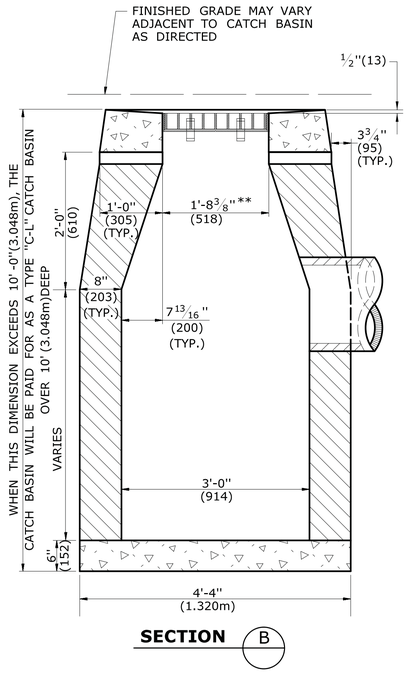
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

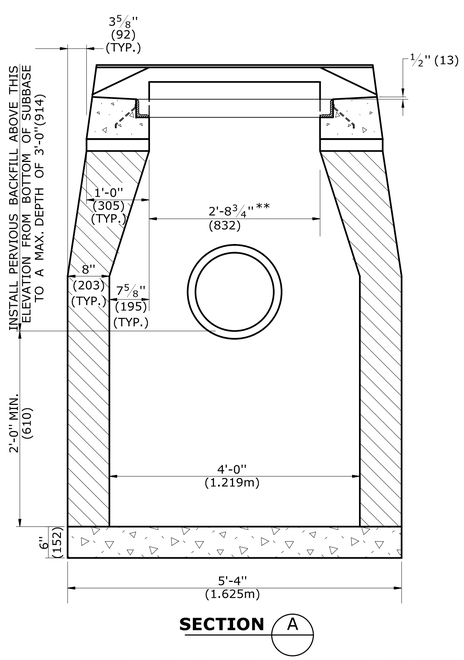
HIGHWAY
STANDARD SHEET INDEX

STANDARD SHEET NO.:

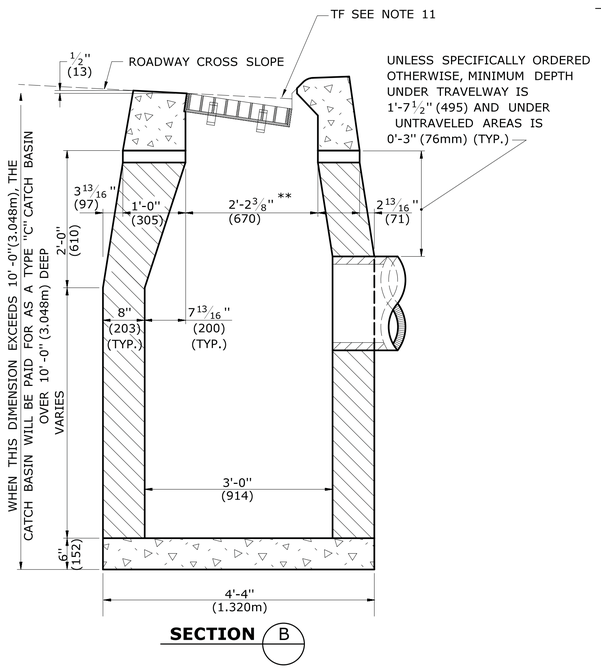
HW_INX
2 of 2



TYPE "C-L" CATCH BASIN



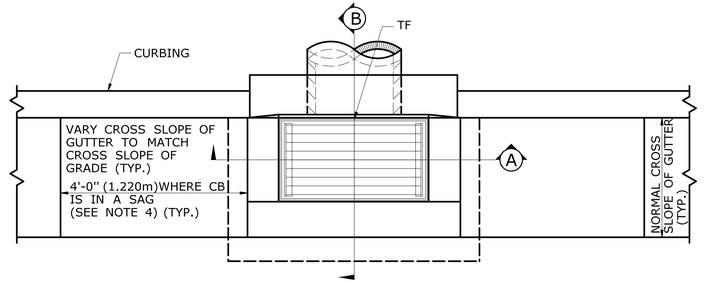
TYPE "C" & "C-L" CATCH BASIN (TYPE "C" TOP SHOWN)



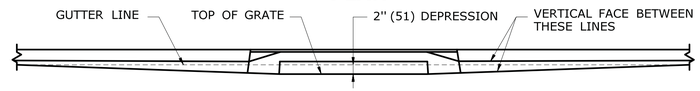
TYPE "C" CATCH BASIN

GENERAL NOTES:

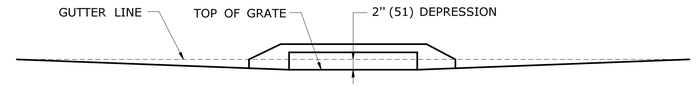
1. FOR DETAILS OF FRAME AND GRATE SEE STANDARD SHEET HW-507_08.
2. USE APPROPRIATE CONCRETE TOP FOR CURBING SHOWN ON PLANS. IF CURBING IS NOT SPECIFIED ON THE PLANS, IT SHALL BE CONSTRUCTED AS DIRECTED BY THE ENGINEER.
3. ALL FACES OF STRUCTURES IN CONTACT WITH CONCRETE PAVEMENT SHALL BE COVERED WITH A LAYER OF TAR PAPER OR APPROVED EQUAL. THE COST FOR THE PAPER SHALL BE INCLUDED IN THE BID PRICE FOR THE TYPE OF CATCH BASIN INSTALLED.
4. USE 6'-0" (1.830m) ON UPGRADE SIDE OF CONTINUOUS GRADE AND 1'-0" (305mm) ON DOWNGRADE SIDE OF CONTINUOUS GRADE OR AS DIRECTED.
5. IF MASONRY UNITS ARE REQUIRED, THE BASIN SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE OVER ALL DIMENSIONS SHOWN HERE AND SECTION 5.07 OF THE STATE OF CONNECTICUT'S STANDARD SPECIFICATIONS. CORBELLING SHALL BE PERMITTED TO A MAXIMUM OF 3" (75mm.) NO PROJECTION SHALL EXTEND INSIDE THE LIMITS NOTED BY **.
6. WALL THICKNESS OF ALL CB'S OVER 10'(3.048m) DEEP SHALL BE INCREASED TO 12" (305mm) THICK. INSIDE DIMENSION SHALL REMAIN THE SAME. 12" (305mm) THICKNESS WILL START AFTER THE FIRST 10'(3.048m).
7. TO CONVEY SUBSURFACE DRAINAGE, OPENINGS SHALL BE FORMED IN THE FOUR WALLS AT OR IMMEDIATELY ABOVE THE BOTTOM OF THE PERVIOUS BACKFILL.
8. MINIMUM CONCRETE COMPRESSIVE STRENGTH OF F'c = 4000 PSI (27,580 kPa) SHALL BE OBTAINED PRIOR TO SHIPPING.
9. LATEST STATE OF CONNECTICUT'S STANDARD SPECIFICATIONS AND SUPPLEMENTALS SHALL GOVERN.
10. SPACER MAY BE CMU OR PRECAST WITH REQUIRED REINFORCING (RECOMMENDED BY THE MANUFACTURER) AS NEEDED TO PROVIDE PROPER GRADE SHOWN ON PLANS.
11. TOP OF FRAME (TF) ELEVATION SHALL BE MEASURED IN THE CENTER OF GRATE @ GUTTER LINE.



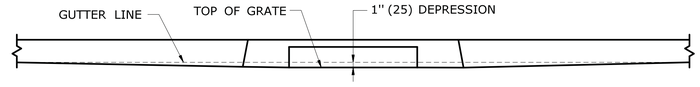
PLAN



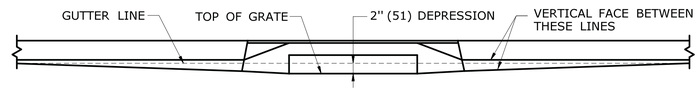
FOR CATCH BASINS IN A LINE OF 4" (102) CONCRETE PARK CURBING OR 4" (102) BITUMINOUS CONCRETE PARK CURBING



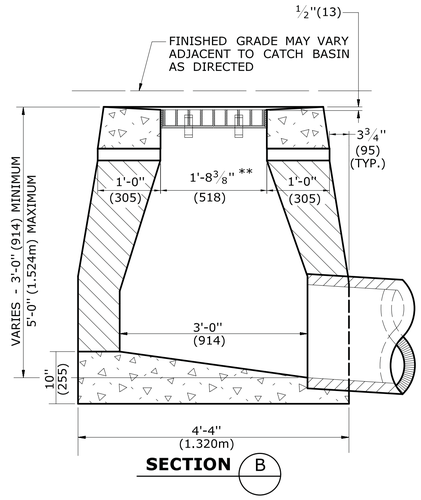
FOR CATCH BASINS WHERE NO CURBING OF ANY TYPE EXISTS OR IS PROPOSED



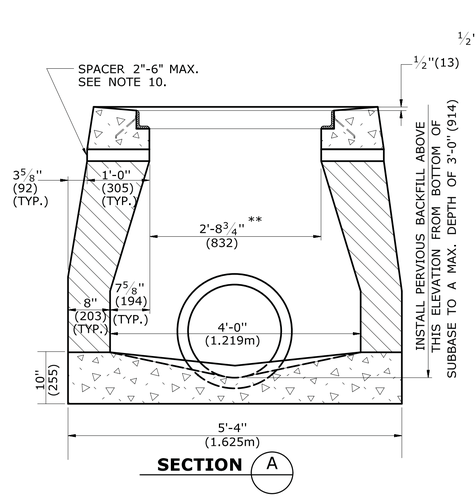
FOR CATCH BASINS IN A LINE OF 6" (152) CONCRETE CURBING OR 6" (152) STONE CURBING



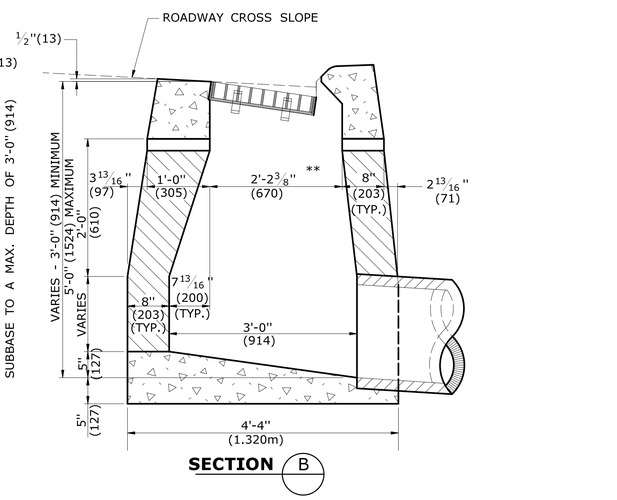
FOR CATCH BASINS IN A LINE OF 6" (152) BITUMINOUS CONCRETE LIP CURBING (MACHINE FORMED)



TYPE "C-L" DROP INLET



TYPE "C" & "C-L" DROP INLET (TYPE "C-L" TOP SHOWN)



TYPE "C" DROP INLET

DETAILS OF DEPRESSED GUTTER STRIP FOR TYPE "C" CATCH BASIN

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

REV.	DATE	REVISION DESCRIPTION
4	7/13	ADD NOTE 11 -
3	9/30/11	ADD SPACERS AND NOTE 10. -
2	9/15/11	MODIFIED DETAILS TO BE CONSISTANT WITH PRECAST -
1	7/28/11	REMOVE MIN. DROP NOTE -
-	-	- -
-	-	- -

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Plotted Date: 6/10/2013

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_HIGHWAY_STD2013.dgn Model: 6-HW-507_01

SUBMITTED BY: NAME/DATE/TIME:

APPROVED BY: NAME/DATE/TIME:

CTDOT STANDARD SHEET

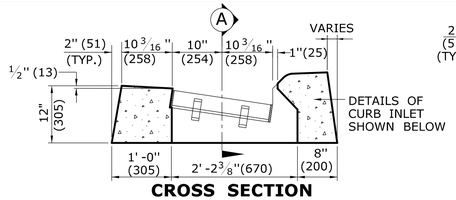
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

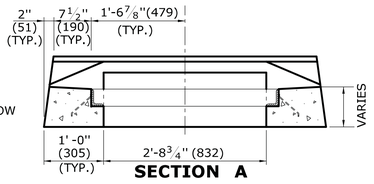
TYPE "C", "C-L" & DROP INLET CATCH BASIN

STANDARD SHEET NO.:

HW-507_01

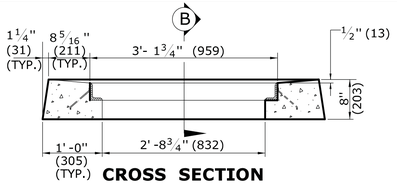


CROSS SECTION



SECTION A

TYPE "C" CATCH BASIN TOP



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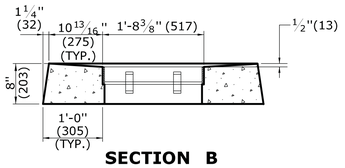
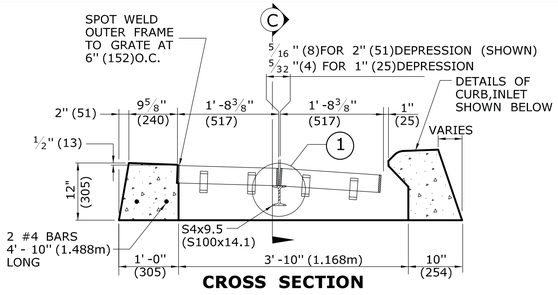


SECTION B

TYPE "C-L" CATCH BASIN TOP

GENERAL NOTES:

1. FOR DETAILS OF FRAME AND GRATE SEE STANDARD SHEET HW-507_08.
2. ALL STEEL, EXCEPT REINFORCING BARS, SHALL BE GALVANIZED IN CONFORMANCE WITH SECTION M06.03 OF CONNECTICUT'S STANDARD SPECIFICATIONS.
3. ALL BARS SHALL HAVE A MINIMUM 2" (51) COVER.



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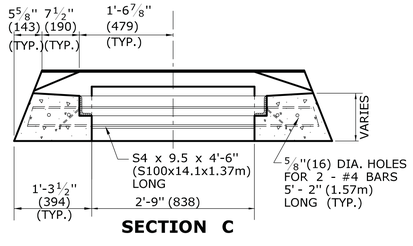
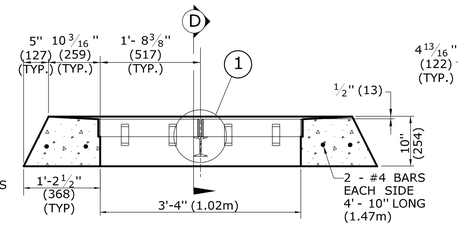


SECTION C

TYPE "C" CATCH BASIN DOUBLE GRATE - TYPE I TOP



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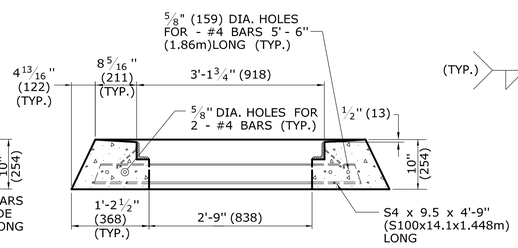
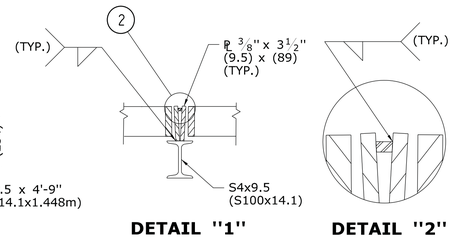


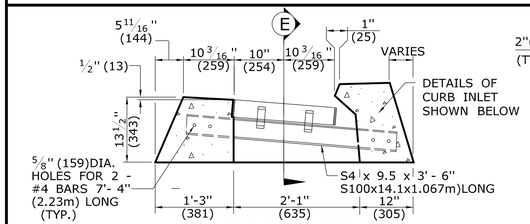
SECTION D



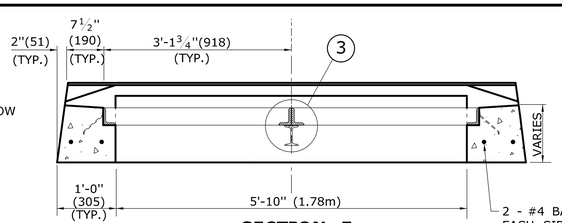
DETAIL "1"

DETAIL "2"

TYPE "C-L" CATCH BASIN DOUBLE GRATE - TYPE I TOP

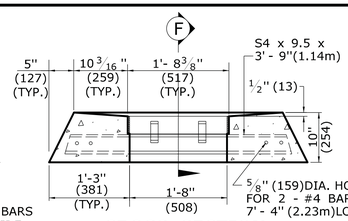


CROSS SECTION

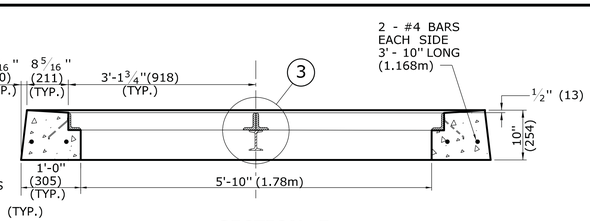


SECTION E

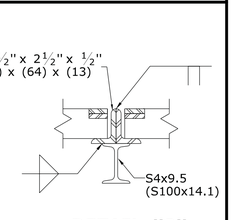
TYPE "C" CATCH BASIN DOUBLE GRATE - TYPE II TOP



CROSS SECTION

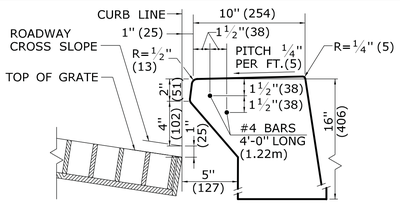


SECTION F

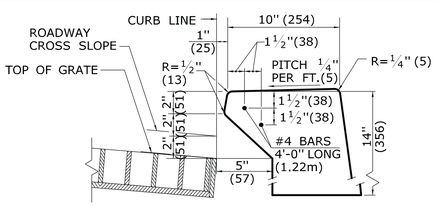


DETAIL "3"

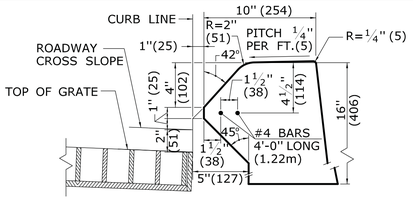
TYPE "C-L" CATCH BASIN DOUBLE GRATE - TYPE II TOP



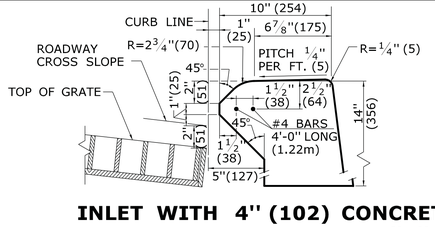
INLET WITH 6" (152) CONCRETE OR STONE CURBING FOR TYPE "C" CB



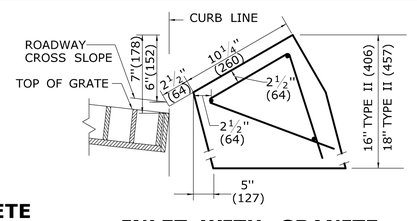
INLET WITH NO CURBING (PLAIN TYPE) FOR TYPE "C" CB



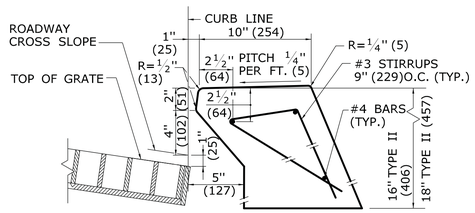
INLET WITH 6" (152) BITUMINOUS CONCRETE LIP CURBING FOR TYPE "C" CB



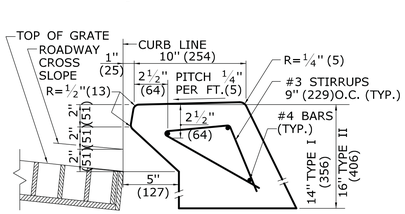
INLET WITH 4" (102) CONCRETE PARK CURBING FOR TYPE "C" CB



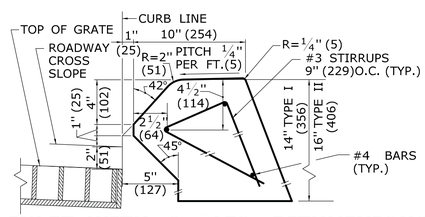
INLET WITH GRANITE SLOPE CURB FOR TYPE "C" CB



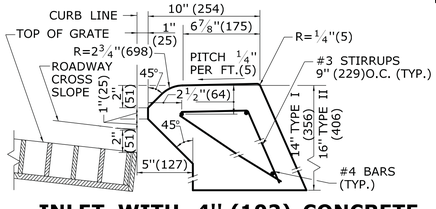
INLET WITH 6" (152) CONCRETE OR STONE CURBING FOR TYPE "C" CB DOUBLE GRATE TYPE I & II



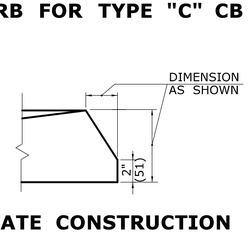
INLET WITH NO CURBING (PLAIN TYPE) FOR TYPE "C" CB DOUBLE GRATE TYPE I & II



INLET WITH 6" (152) BITUMINIOUS CONCRETE LIP CURBING FOR TYPE "C" CB DOUBLE GRATE TYPE I & II



INLET WITH 4" (102) CONCRETE PARK CURBING FOR TYPE "C" CB DOUBLE GRATE TYPE I & II



ALTERNATE CONSTRUCTION OF TYPE II TOP

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

REV.	DATE	REVISION DESCRIPTION
-	-	-
-	-	-
-	-	-
-	-	-
2	7/28/11	REMOVE MIN. DROP NOTE
1	6/01/10	REVISE CALL-OUT

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 11/10/2011

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_HWY_STD_Nov2011.dgn Model: 12-HW-507_07

SUBMITTED BY: NAME/DATE/TIME:

APPROVED BY: NAME/DATE/TIME:

CTDOT STANDARD SHEET

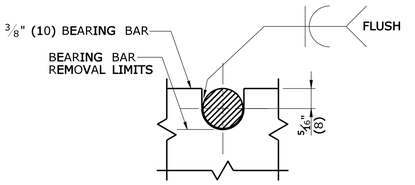
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

TYPE "C" & "C-L" CATCH BASIN TOPS AND CURBS

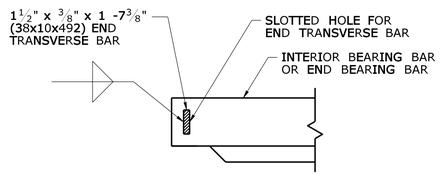
STANDARD SHEET NO.:

HW-507_07

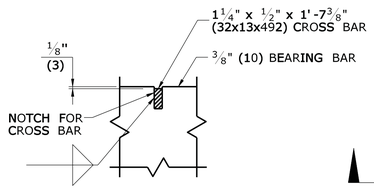


NOTE:
5/8" (16) DIA. ROUND BAR SHALL CONTACT BEARING BAR AT BOTTOM AND BE FLUSH AT TOP.

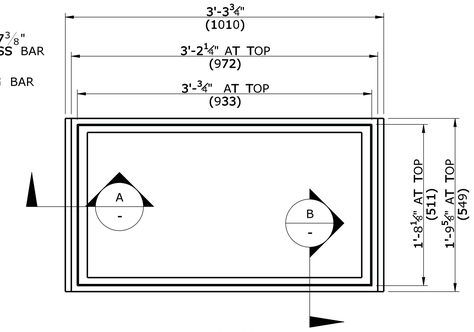
ROUND BAR ATTACHMENT
CATCH BASIN GRATE TYPE A



END TRANSVERSE BAR ATTACHMENT
CATCH BASIN GRATE TYPE A & B



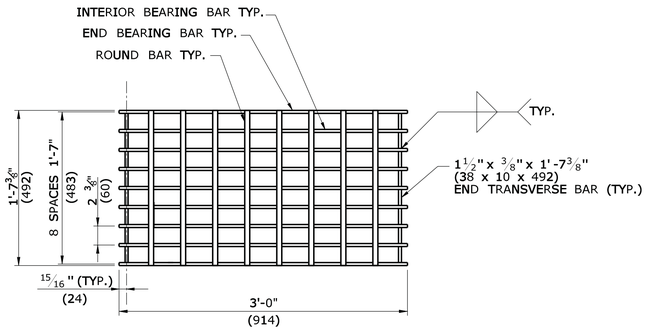
CROSS BAR ATTACHMENT
CATCH BASIN GRATE TYPE B



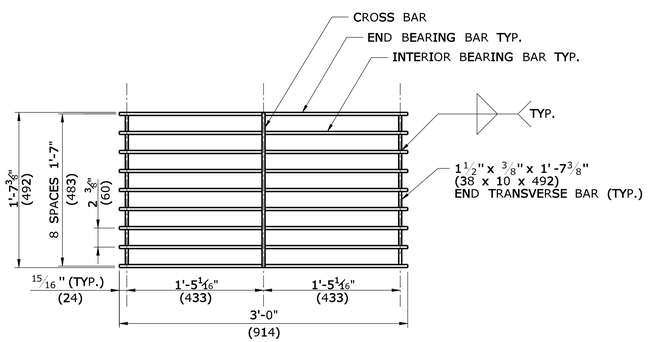
PLAN

GENERAL NOTES:

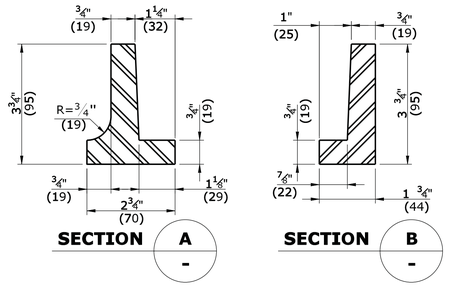
1. STEEL OR CAST IRON SHALL BE USED FOR FRAMES. STEEL SHALL BE USED FOR TYPE "A" & "B" GRATES.
2. TYPE "A" GRATES SHALL BE USED ON ALL ROADWAYS WHERE BICYCLE TRAFFIC IS ALLOWED OR AS DIRECTED BY THE ENGINEER.
3. TYPE "B" GRATES SHALL BE USED ON ALL LIMITED ACCESS HIGHWAYS, RAMPS AND WHERE BICYCLE TRAFFIC IS NOT ALLOWED OR AS DIRECTED BY THE ENGINEER.
4. STEEL FRAMES AND GRATES SHALL BE GALVANIZED IN ACCORDANCE WITH ARTICLE M.06.03.
5. DO NOT GALVANIZE CAST IRON FRAMES.
6. DIMENSIONAL TOLERANCES SHALL BE ±1/16".(1.6)
7. ALL STEEL BARS SHALL BE WELDED AT ALL INTERSECTIONS.
8. ALL WELDING SHALL CONFORM TO THE REQUIREMENTS OF AWS STRUCTURAL WELDING CODE, D1.1.



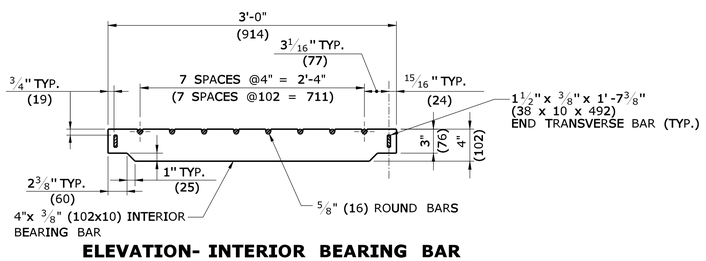
PLAN



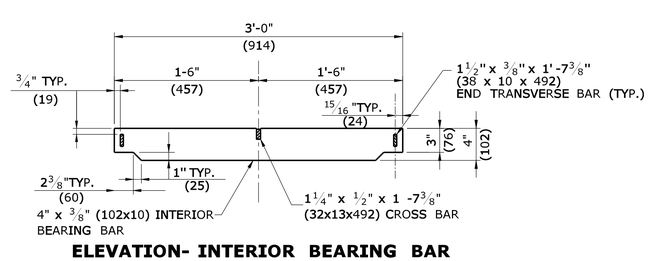
PLAN



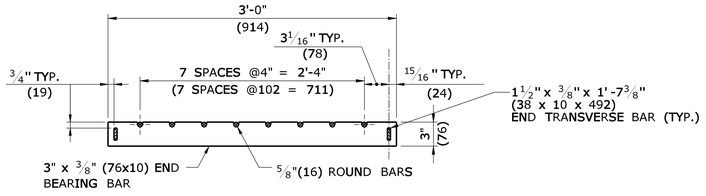
CAST IRON FRAME ALTERNATE



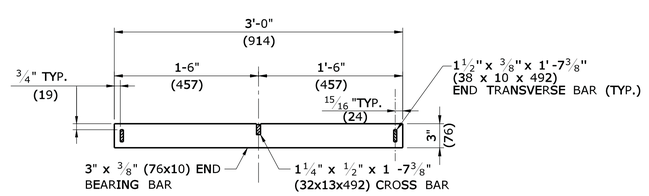
ELEVATION- INTERIOR BEARING BAR



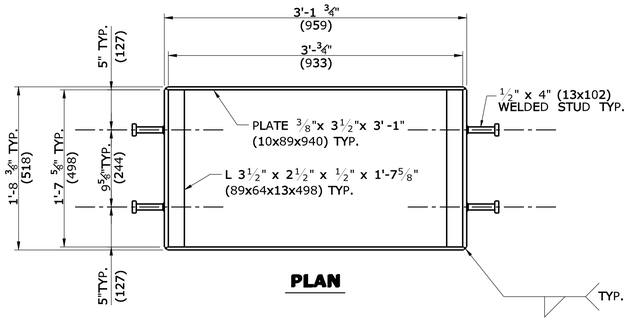
ELEVATION- INTERIOR BEARING BAR



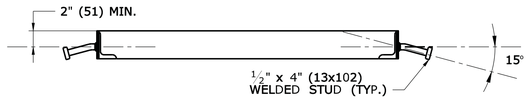
ELEVATION- END BEARING BAR
CATCH BASIN GRATE TYPE A



ELEVATION- END BEARING BAR
CATCH BASIN GRATE TYPE B



PLAN



WELDED STUD ANCHOR DETAILS
STEEL FRAME

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

REV.	DATE	REVISION DESCRIPTION
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 9/11/2009

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_HIGHWAY_STD.dgn Model: HW-507_08

SUBMITTED BY: NAME/DATE/TIME:

APPROVED BY: NAME/DATE/TIME:

CTDOT
STANDARD SHEET

OFFICE OF ENGINEERING

STANDARD SHEET TITLE:
CATCH BASIN FRAMES AND GRATES

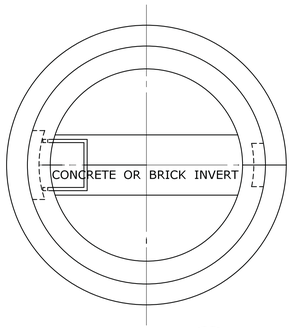
STANDARD SHEET NO.:
HW-507_08

GENERAL NOTES:

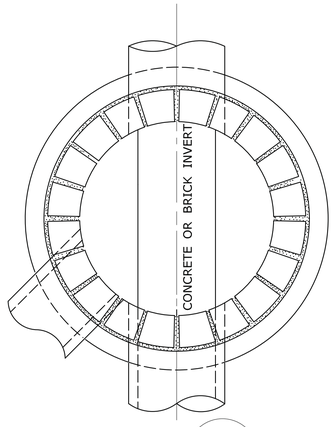
1. CHANNELS MAY BE SHAPED IN CONCRETE BASE OF MANHOLE OR FORMED USING BRICK OR MASONRY.
2. A FRAME DIAMETER OF 3'-3" (991) WITH 4" (102) FLANGE MUST BE USED WHEN THE TOP DIAMETER OF THE PRECAST CONE IS LESS THAN 3'-6" (1067). ALL OTHER FRAME DIMENSIONS SHALL REMAIN THE SAME.
3. COVER:

	CAST IRON	STEEL
MIN. COVER WEIGHT	134LB.(61kg)	134LB.(61kg)

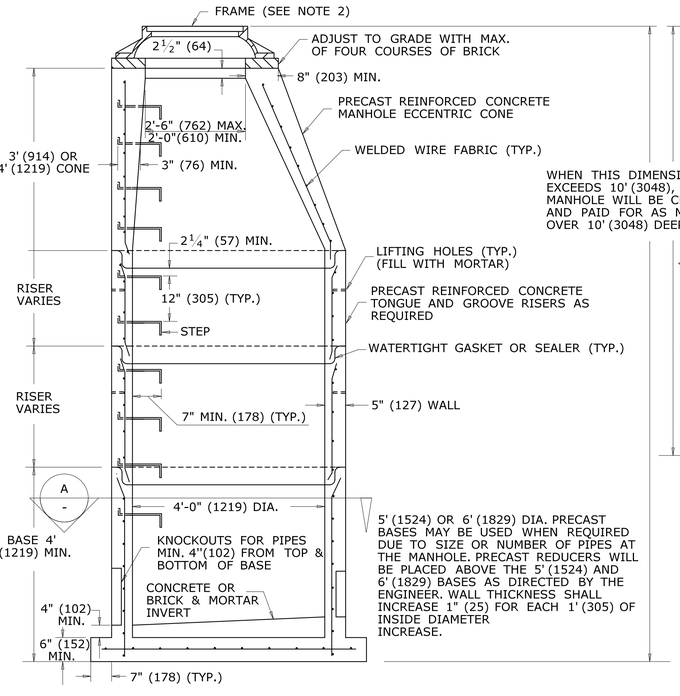
4. ALL DIMENSIONS SUBJECT TO MANUFACTURING TOLERANCES.



SECTION A -



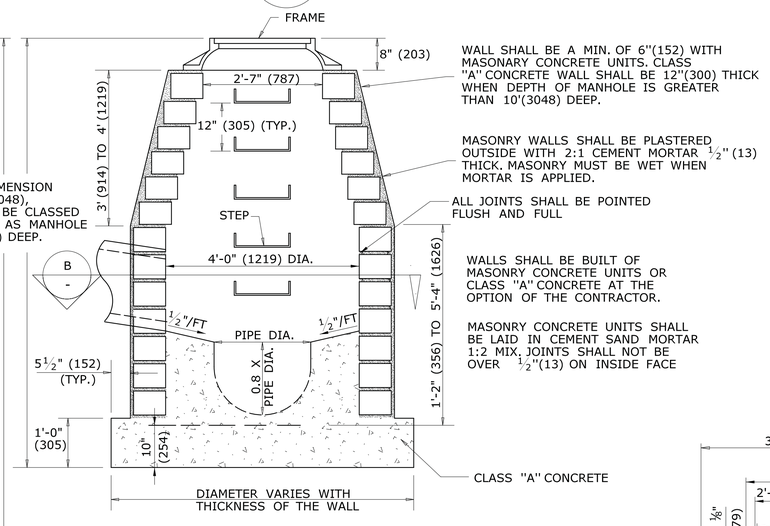
SECTION B -



ELEVATION

MANHOLE

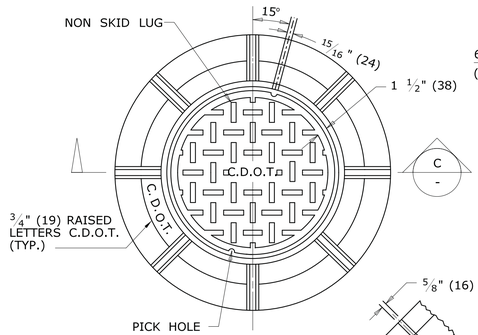
REINFORCED PRECAST CONCRETE UNIT



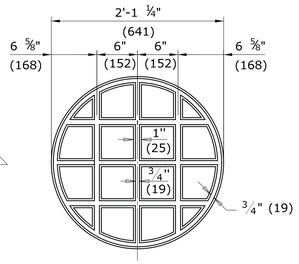
ELEVATION

MANHOLE

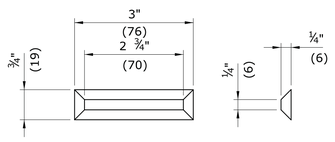
MASONARY CONCRETE UNIT OR CLASS "A" CONCRETE



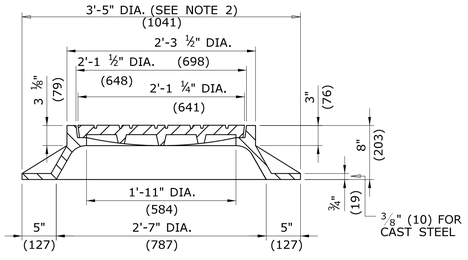
PLAN



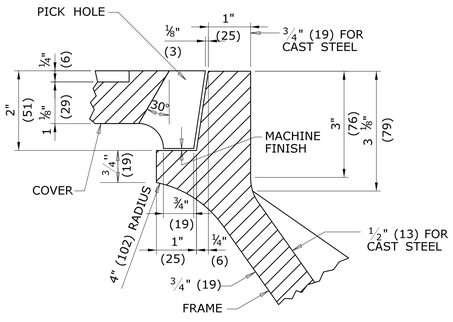
UNDERSIDE OF COVER DETAIL



NON SKID LUG DETAIL



SECTION C -



DETAIL OF SEAT

FRAME AND COVER DETAILS

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

REV.	DATE	REVISION DESCRIPTION
1	6/11	REVISE STEP WIDTH PER OSHA
2	7/13	REVISE COVER FRAME WEIGHT
-	-	-
-	-	-
-	-	-
-	-	-

Plotted Date: 6/13/2013

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_HIGHWAY_STD2013.dgn Model: 15-HW-507_10

SUBMITTED BY: NAME/DATE/TIME:

APPROVED BY: NAME/DATE/TIME:

CTDOT STANDARD SHEET

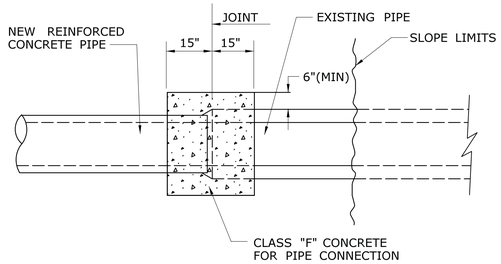
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

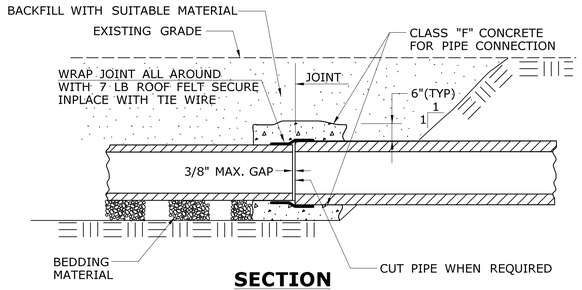
MANHOLE - FRAME & COVER

STANDARD SHEET NO.:

HW-507_10



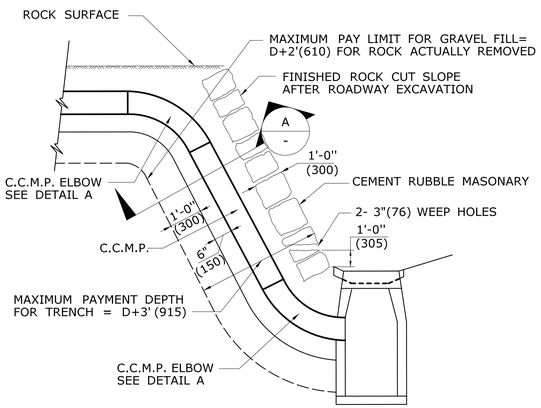
PLAN



SECTION
CONCRETE PIPE CONNECTION

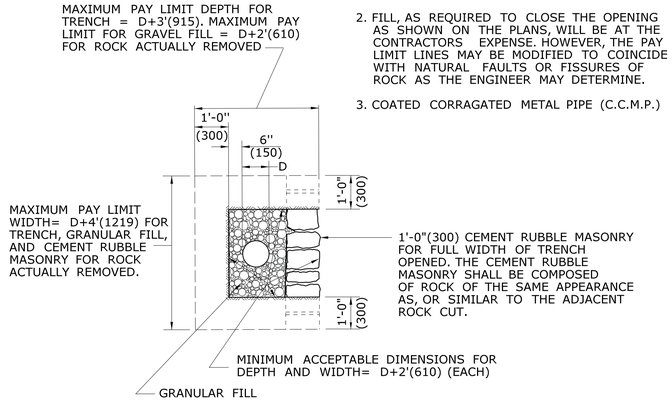
NOTES:

1. "CONCRETE PIPE CONNECTION" IS INTENDED FOR USE WHERE A REINFORCED CONCRETE PIPE REPAIR OR MODIFICATION IS NEEDED SOMEWHERE WITHIN A PIPE RUN WHERE A BELL/SPIGOT JOINT CANNOT BE ACHIEVED.
2. MAINTAIN INTERIOR ALIGNMENT OF PIPE AT JOINTS UNTIL CONCRETE IS PROPERLY CURED.
3. BACKFILL OF PIPE REPAIR WITH SUITABLE MATERIAL MAY NOT TAKE PLACE UNTIL CONCRETE IS PROPERLY CURED.
4. CONTRACTOR SHALL MAINTAIN LINE AND GRADE OF PIPE REPAIR OR MODIFICATION BY METHODS APPROVED BY THE ENGINEER.
5. HOLES OR GAPS AT JOINT LARGER THAN 3/8" SHALL BE FILLED OR WRAPED TO PREVENT CONCRETE FROM ENTERING PIPE.
6. TRENCH EXCAVATION SHALL BE TO THE MAXIMUM EXTENT NEEDED TO PERFORM WORK.



GENERAL NOTES:

1. ROCK REMOVED BEYOND THE MAXIMUM PAY LIMIT SHOWN SHALL BE REPLACED WITH CEMENT RUBBLE MASONRY AND GRANULAR FILL.
2. FILL, AS REQUIRED TO CLOSE THE OPENING AS SHOWN ON THE PLANS, WILL BE AT THE CONTRACTORS EXPENSE. HOWEVER, THE PAY LIMIT LINES MAY BE MODIFIED TO COINCIDE WITH NATURAL FAULTS OR FISSURES OF ROCK AS THE ENGINEER MAY DETERMINE.
3. COATED CORRAGATED METAL PIPE (C.C.M.P.)



SECTION A -

TYPICAL INSTALLATION OF C.C.M.P. IN ROCK SLOPE

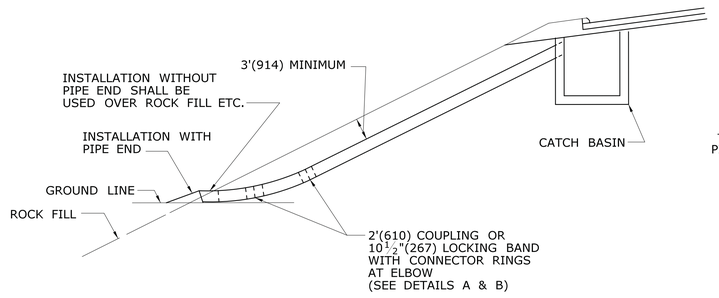
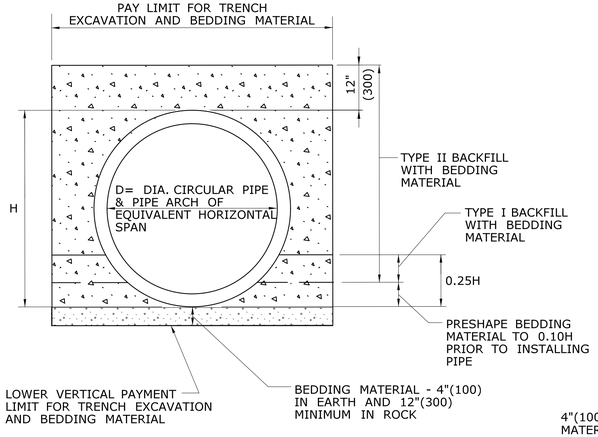


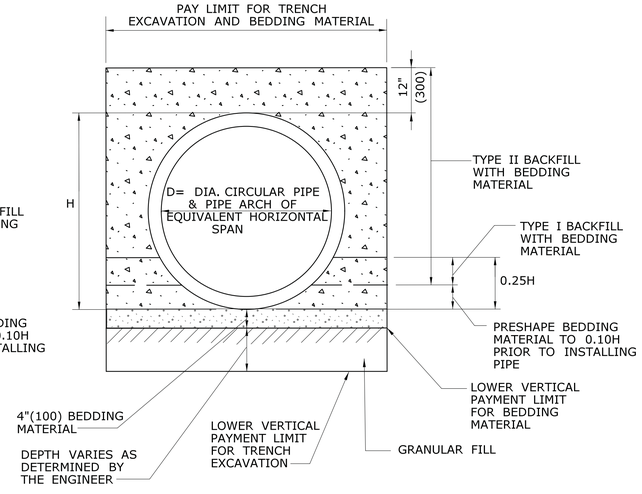
TABLE C
CONNECTOR RINGS

PIPE DIAMETER	LENGTH OF RING
12" (300)	52" (1321)
15" (381)	61" (1549)
18" (457)	71" (1803)
21" (533)	80" (2032)
24" (610)	90" (2286)
30" (762)	108" (2743)
36" (915)	128" (3251)
42" (1066)	147" (3734)
48" (1219)	166" (4216)

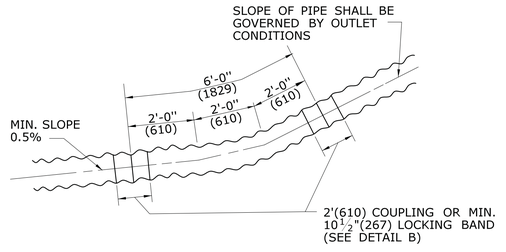
TYPICAL INSTALLATION OF C.C.M.P ELBOW IN FILL SLOPE



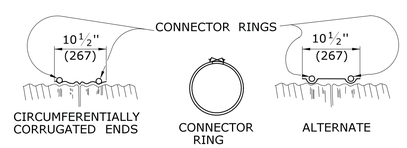
PIPE TRENCH DETAIL
WHERE GRANULAR FILL IS NOT USED



PIPE TRENCH DETAIL
WHERE GRANULAR FILL IS USED AS BEDDING



DETAIL A
C.C.M.P. ELBOW AND COUPLING



RODS: 7/16 "(11) DIAMETER ELECTRO-GALVANIZED WITH 6"(152) LENGTH OF ½"(13) ROLLED THREADS EACH END, FURNISHED CURVED, TO FIT PIPE. SEE TABLE C

LUGS: DOUBLE TAKE UP, CAST IRON, ELECTRO-GALVINIZED.

NOTE: THE COUPLER FASTENING DEVICE SHALL NOT INTERFERE WITH INSTALLATION OF CONNECTOR RINGS.

DETAIL B
ELBOW DIMENSIONS

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

REV.	DATE	REVISION DESCRIPTION
-	-	-
-	-	-
-	-	-
3	7/13	ADD CONCRETE PIPE CONNECTION DETAIL
2	6/01/10	REVISE TITLE TO SAY TRENCH DETAIL
1	6/01/10	REMOVE GRAVEL, REPLACE W/ GRANULAR

Plotted Date: 6/13/2013

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_HIGHWAY_STD2013.dgn Model: 17-HW-651_01

SUBMITTED BY: NAME/DATE/TIME:

APPROVED BY: NAME/DATE/TIME:

CTDOT STANDARD SHEET

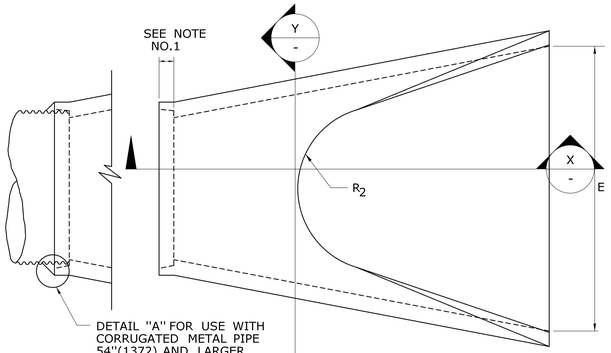
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

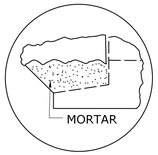
C.C.M.PIPE INSTALLATIONS IN FILL & ROCK SLOPES & PIPE TRENCH DETAIL

STANDARD SHEET NO.:

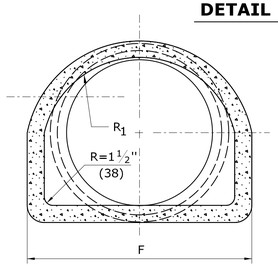
HW-651_01



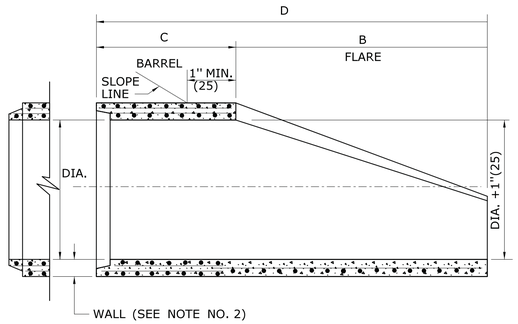
PLAN

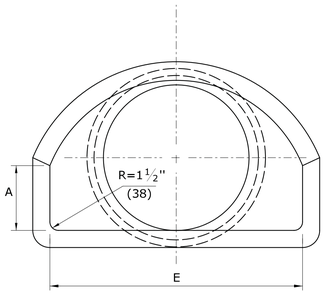


DETAIL "A"



SECTION Y -

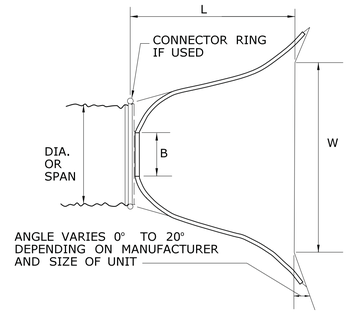




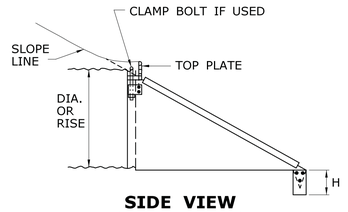
SECTION X -

END VIEW

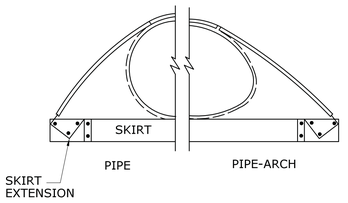
REINFORCED CONCRETE PIPE END



PLAN



SIDE VIEW

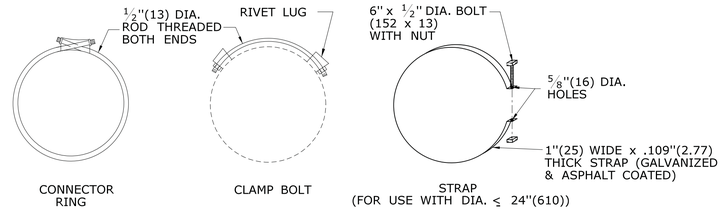


END VIEW

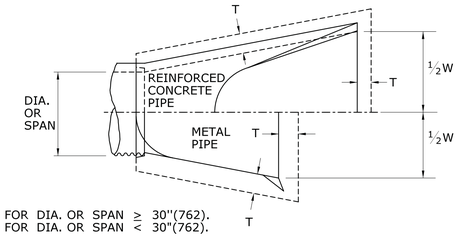
METAL PIPE END

GENERAL NOTES:

1. JOINTS SHALL BE TONGUE AND GROOVE OR BELL AND SPIGOT AS REQUIRED TO CONFORM TO PIPE INSTALLED.
2. WALL THICKNESS SHALL CONFORM TO PIPE THICKNESS.

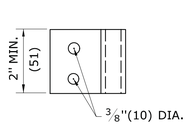


ATTACHMETNT SYSTEMS

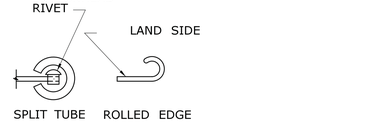


NOTE:
T = 18"(457) FOR DIA. OR SPAN ≥ 30"(762).
T = 12"(305) FOR DIA. OR SPAN < 30"(762).

HORIZONTAL PAY LIMITS FOR TRENCH EXCAVATION-PIPE ENDS



RIVET LUG



EDGE REINFORCEMENT

DIMENSIONS FOR REINFORCED CONCRETE PIPE END									FLARE REINFORCEMENT ONE LAYER ONLY IN CENTER OF WALL	
DIA.	A	B	C	D	E	F	R_1	R_2	MIN. AREA OF LONGITUDINAL STEEL SQ. IN. PER FT.	MIN. AREA OF TRANSVERSE STEEL SQ. IN. PER FT.
12"(305)	4"(102)	2'-0"(610)	4'-0 3/8"(1241)	6'-0 3/8"(1851)	2'-0"(610)	1'-7 15/16"(506)	10 1/4"(260)	9"(229)	0.048	0.048
15"(381)	6"(152)	2'-3"(686)	3'-10"(1168)	6'-1"(1854)	2'-6"(762)	2'-0 5/16"(618)	1'-0 1/2"(318)	11"(279)	0.054	0.054
18"(457)	9"(229)	2'-3"(686)	3'-10"(1168)	6'-1"(1854)	3'-0"(914)	2'-5"(737)	1'-3 1/2"(394)	1'-0"(305)	0.060	0.060
21"(533)	9"(229)	2'-11"(889)	3'-2"(965)	6'-1"(1854)	3'-6"(1067)	2'-7 1/2"(800)	1'-4"(406)	1'-1"(330)	0.066	0.066
24"(610)	9 1/2"(241)	3'-7 1/2"(1105)	2'-6"(762)	6'-1 1/2"(1867)	4'-0"(1219)	2'-9 3/16"(843)	1'-4 13/16"(427)	1'-2"(356)	0.072	0.072
30"(762)	1'-0"(305)	4'-6"(1371)	1'-7 3/4"(502)	6'-1 3/4"(1873)	5'-0"(1524)	3'-1"(940)	1'-6 1/2"(470)	1'-3"(381)	0.084	0.084
36"(914)	1'-3"(381)	5'-3"(1600)	2'-10 3/4"(883)	8'-1 3/4"(2483)	6'-0"(1829)	3'-11 13/16"(1214)	2'-0 5/16"(618)	1'-8"(508)	0.096	0.096
42"(1067)	1'-9"(534)	5'-3"(1600)	2'-11"(889)	8'-2"(2489)	6'-6"(1981)	4'-5 7/8"(1368)	2'-3 1/2"(699)	1'-10"(559)	0.108	0.108
48"(1219)	2'-0"(610)	6'-0"(1829)	2'-2"(660)	8'-2"(2489)	7'-0"(2134)	4'-8 1/2"(1435)	2'-4 1/2"(724)	1'-10"(559)	0.120	0.120
54"(1372)	2'-3"(686)	5'-5"(1651)	2'-11"(889)	8'-4"(2540)	7'-6"(2286)	5'-5 1/2"(1664)	2'-9 1/8"(841)	2'-0"(610)	0.132	0.132
60"(1524)	2'-9"(838)	5'-0"(1524)	3'-3"(991)	8'-3"(2515)	8'-0"(2438)	6'-0 1/2"(1842)	3'-0 11/16"(932)	2'-0"(610)	0.144	0.144

PIPE DIA.	DIMENSIONS FOR METAL PIPE END				THICKNESS INS.(mm)
	B (MAX.)	H±1" (±25)	L±1 1/2" (±35)	W±2" (±50)	
12"(305)	6"(152)	6"(152)	21"(533)	24"(610)	.064(1.63)
15"(381)	8"(203)	6"(152)	26"(660)	30"(762)	.064(1.63)
18"(457)	10"(254)	6"(152)	31"(787)	36"(914)	.064(1.63)
21"(533)	12"(305)	6"(152)	36"(914)	42"(1067)	.064(1.63)
24"(610)	13"(330)	6"(152)	41"(1041)	48"(1219)	.064(1.63)
30"(762)	16"(406)	8"(203)	51"(1295)	60"(1524)	.079(2.01)
36"(914)	19"(483)	9"(229)	60"(1524)	72"(1829)	.079(2.01)
42"(1067)	22"(559)	11"(279)	69"(1753)	84"(2134)	.109(2.77)
48"(1219)	27"(686)	12"(305)	78"(1981)	90"(2286)	.109(2.77)
54"(1372)	30"(762)	12"(305)	84"(2134)	102"(2591)	.109(2.77)
60"(1524)	33"(838)	12"(305)	87"(2210)	114"(2896)	.109(2.77)
66"(1676)	36"(914)	12"(305)	87"(2210)	120"(3048)	.109(2.77)
72"(1829)	39"(991)	12"(305)	87"(2210)	126"(3200)	.109(2.77)
78"(1981)	42"(1067)	12"(305)	87"(2210)	132"(3353)	.109(2.77)
84"(2134)	45"(1143)	12"(305)	87"(2210)	138"(3450)	.109(2.77)

PIPE ARCH SIZE		DIMENSIONS FOR METAL PIPE END				
SPAN	RISE	B (MAX.)	H±1" (±25)	L±1 1/2" (±35)	W±2" (±50)	THICKNESS INS.(mm)
17"(432)	13"(330)	9"(229)	6"(152)	19"(483)	30"(762)	.064(1.63)
21"(533)	15"(381)	10"(254)	6"(152)	23"(584)	36"(914)	.064(1.63)
24"(610)	18"(457)	12"(305)	6"(152)	28"(711)	42"(1067)	.064(1.63)
28"(711)	20"(508)	14"(356)	6"(152)	32"(813)	48"1219)	.064(1.63)
35"(889)	24"(610)	16"(406)	6"(152)	39"(991)	60"(1524)	.079(2.01)
42"(1067)	29"(737)	18"(457)	8"(203)	46"(1168)	75"(1905)	.079(2.01)
49"(1245)	33"(838)	21"(533)	9"(229)	53"(1346)	85"(2159)	.109(2.77)
57"(1448)	38"(965)	26"(660)	12"(305)	63"(1600)	90"(2286)	.109(2.77)
64"(1626)	43"(1092)	30"(762)	12"(305)	70"(1778)	102"(2591)	.109(2.77)
71"(1803)	47"(1194)	33"(838)	12"(305)	77"(1956)	114"(2896)	.109(2.77)

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

REV.	DATE	REVISION DESCRIPTION
1	7/13	CHANGE ALL CULVERT LABELS TO SAY PIPE
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 3/28/2013

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_HIGHWAY_STD2013.dgn Model: 19-HW-652_01

SUBMITTED BY: NAME/DATE/TIME:

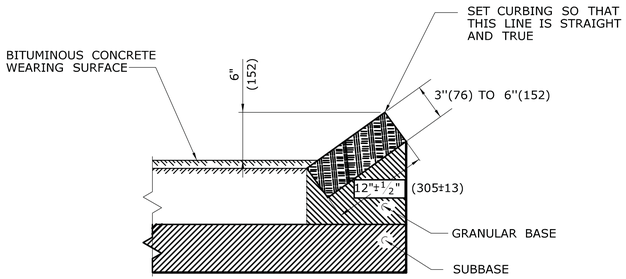
APPROVED BY: NAME/DATE/TIME:

CTDOT STANDARD SHEET

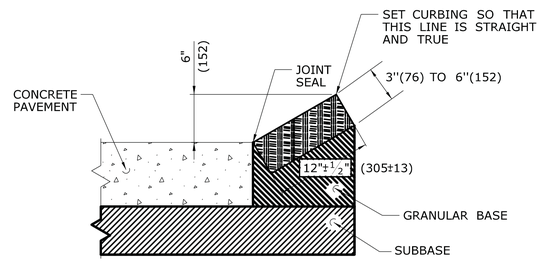
OFFICE OF ENGINEERING

STANDARD SHEET TITLE: PIPE ENDS

STANDARD SHEET NO.: HW-652_01



TYPICAL SECTION SHOWING SLOPE CURBING SET ADJACENT TO BITUMINOUS CONCRETE SURFACES



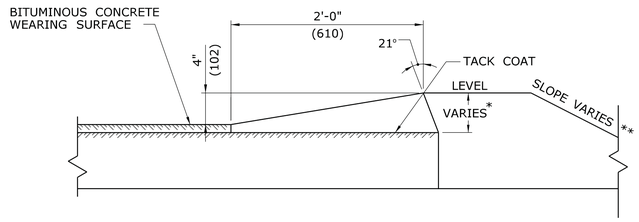
TYPICAL SECTION SHOWING SLOPE CURBING SET ADJACENT TO CONCRETE SURFACES

GRANITE SLOPE CURBING

1/2"(13) MORTAR JOINT REQUIRED

NOTE:

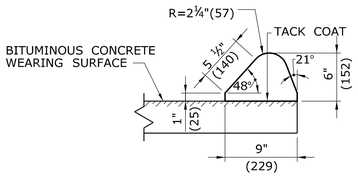
1. ALL CONSTRUCTION DIMENSIONS ARE NOMINAL.



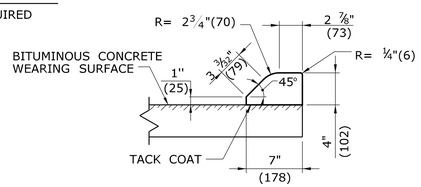
* THIS DIMENSION VARIES WITH THE THICKNESS OF THE TOP COURSE AND SLOPE OF SHOULDER.

** SEE TYPICAL SECTIONS FOR PROJECT. IN FILL AREAS 2'(610) LEVEL BEHIND THE CURB IS REQUIRED.

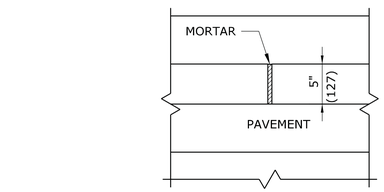
BITUMINOUS CONCRETE BERM



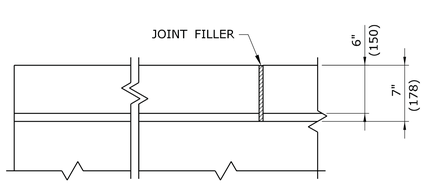
6"(150) BITUMINOUS CONCRETE LIP CURBING



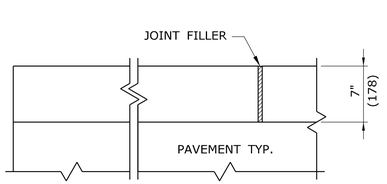
4"(100) BITUMINOUS CONCRETE PARK CURBING



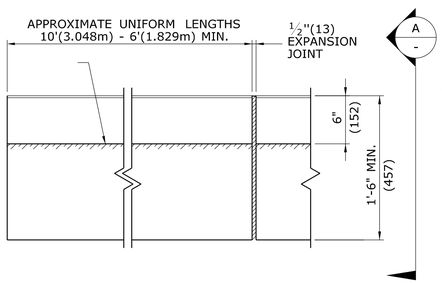
PLAN



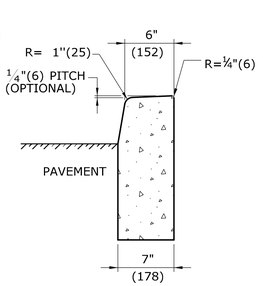
PLAN



PLAN

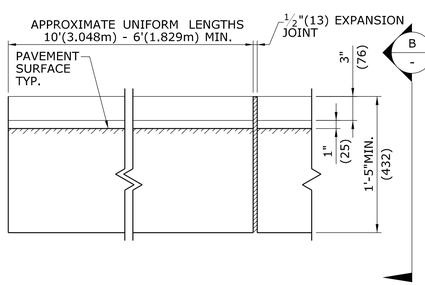


ELEVATION

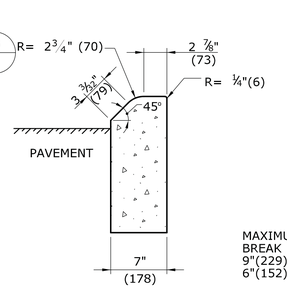


SECTION A -

6" (150) CONCRETE CURBING

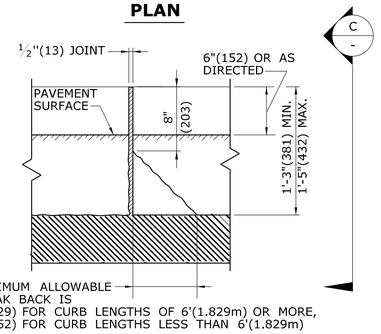


ELEVATION

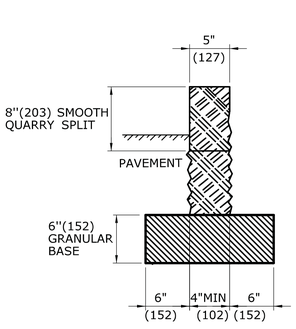


SECTION B -

4"(100) CONCRETE PARK CURBING



ELEVATION



SECTION C -

STONE CURBING

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

REV.	DATE	REVISION DESCRIPTION
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-
1	6/01/10	REVISED TITLE FOR 6" CONC. CURB

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 6/23/2011

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_HIGHWAY_STD_JUNE2011.dgn Model: 22 - HW-811_01

SUBMITTED BY: NAME/DATE/TIME:

APPROVED BY: NAME/DATE/TIME:

CTDOT STANDARD SHEET

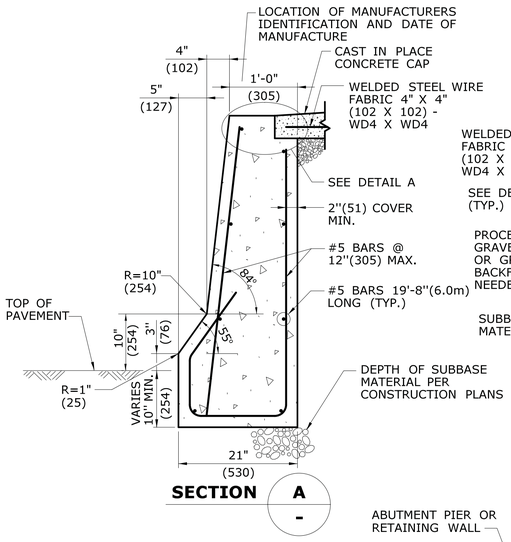
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

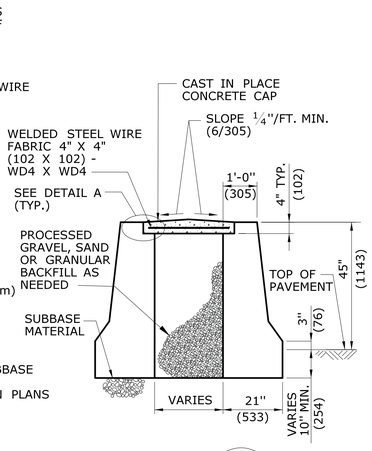
CURBING

STANDARD SHEET NO.:

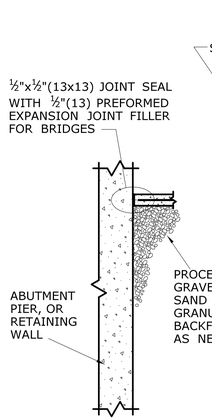
HW-811_01



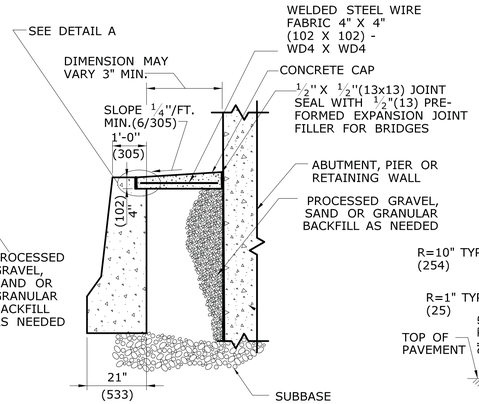
SECTION A
-



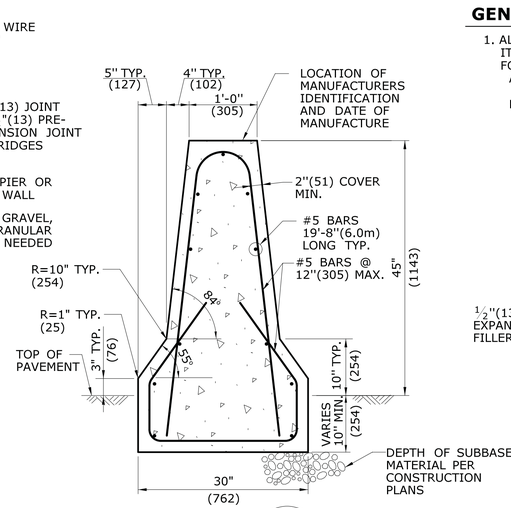
SECTION B
-



SECTION C
-



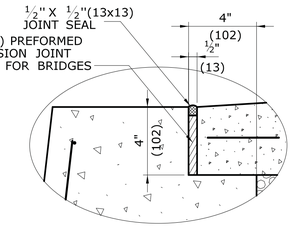
SECTION D
-



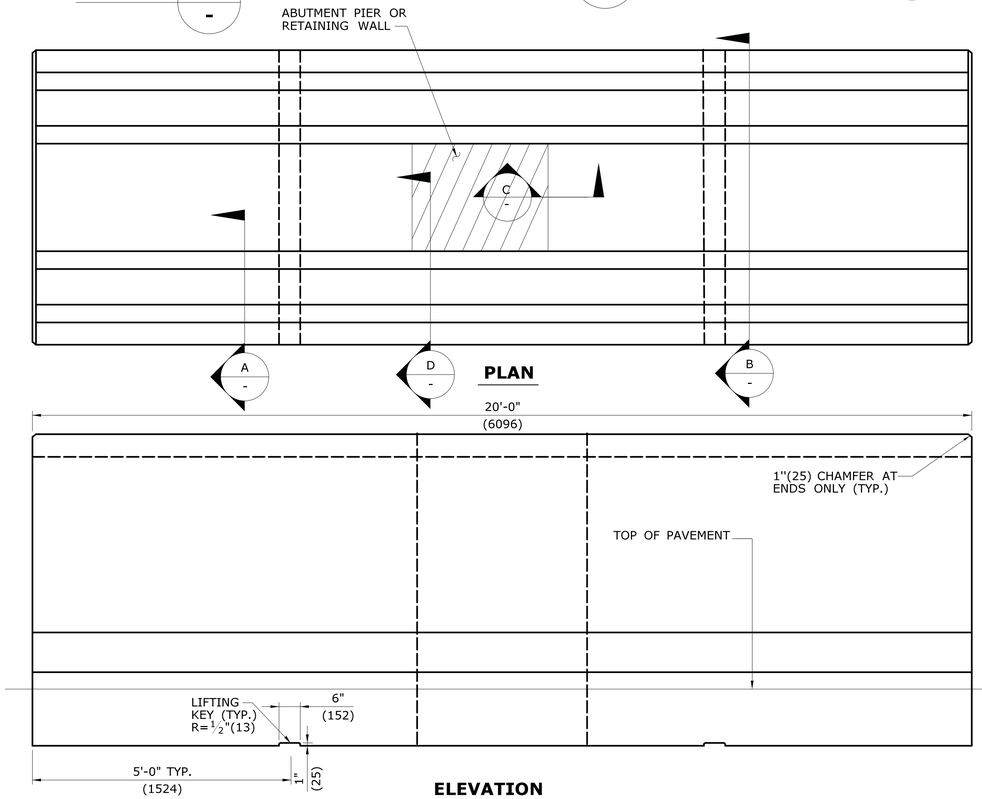
SECTION E
-

GENERAL NOTES:

1. ALTERNATE DESIGNS FOR THE FOLLOWING ITEMS MAY BE SUBSTITUTED TO THE ENGINEER FOR APPROVAL :
   A) UNIT END CONNECTIONS SIMILAR TO DESIGNS SHOWN.
   B) LIFTING KEYS OR OTHER HANDLING DEVICES.



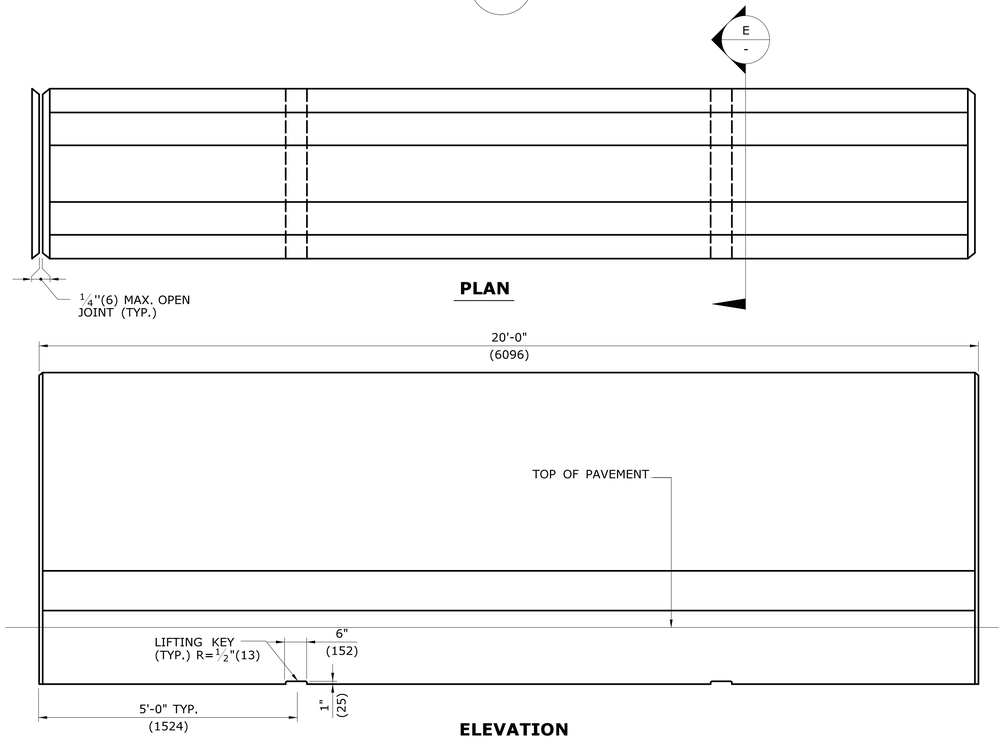
DETAIL A



PLAN

ELEVATION

21" x 45"(530x1145) PRECAST CONCRETE BARRIER CURB UNIT AT PIER ABUTMENT OR RETAINING WALL



PLAN

ELEVATION

30" x 45"(755 x1145) PRECAST CONCRETE BARRIER CURB UNIT

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

REV.	DATE	REVISION DESCRIPTION
1	5/13/10	REVISE GENERAL NOTE 2 AND ADD NOTE 3
2	1/12	REVISE GENERAL NOTES
3	1/12	ENLARGE PLANS & ELEVATIONS, ADD SHEET 2b FOR GUIDERAIL
-	-	ATTACHMENTS
4	7/13	ERRATA - SPELLING

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 3/21/2013

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: June2013.dgn Model: HW-821_02a

SUBMITTED BY: NAME/DATE/TIME:

APPROVED BY: NAME/DATE/TIME:

CTDOT STANDARD SHEET

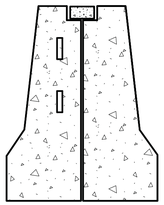
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

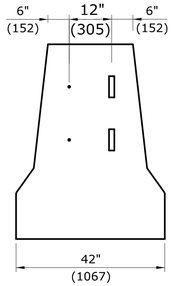
45" (1145) F- SHAPE PRECAST CONCRETE BARRIER CURB

STANDARD SHEET NO.:

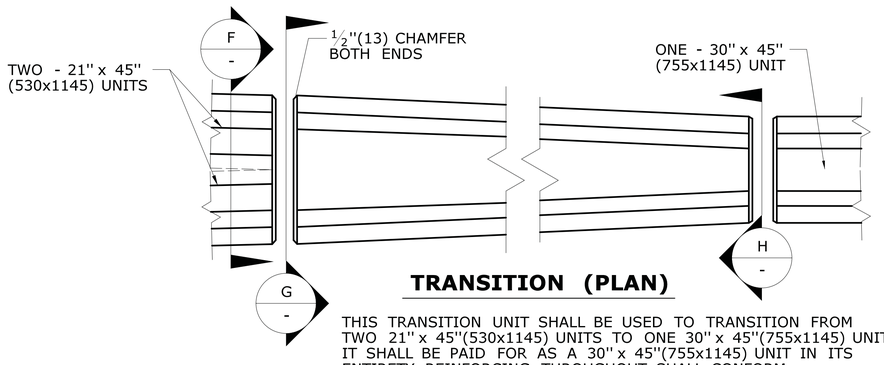
HW-821_02a



SECTION F -

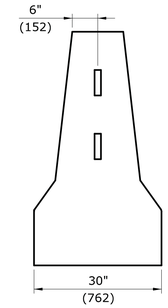


VIEW G (ROTATED)



TRANSITION (PLAN)

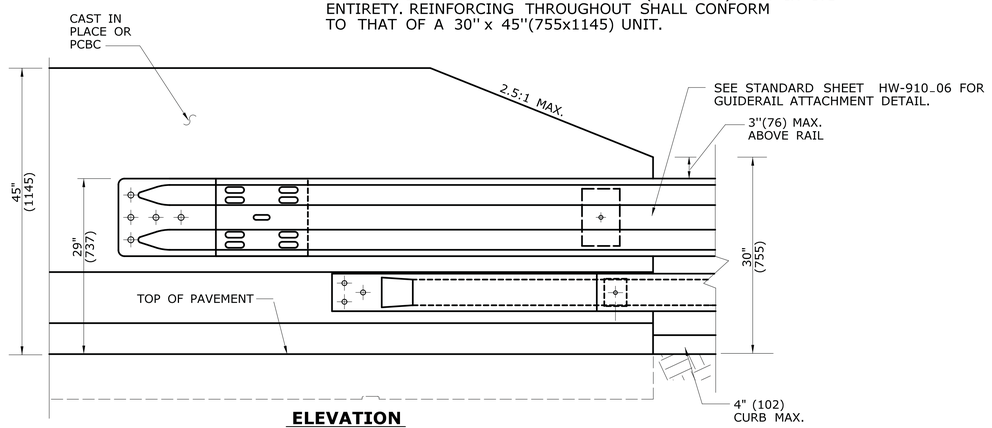
THIS TRANSITION UNIT SHALL BE USED TO TRANSITION FROM TWO 21'' x 45''(530x1145) UNITS TO ONE 30'' x 45''(755x1145) UNIT. IT SHALL BE PAID FOR AS A 30'' x 45''(755x1145) UNIT IN ITS ENTIRETY. REINFORCING THROUGHOUT SHALL CONFORM TO THAT OF A 30'' x 45''(755x1145) UNIT.



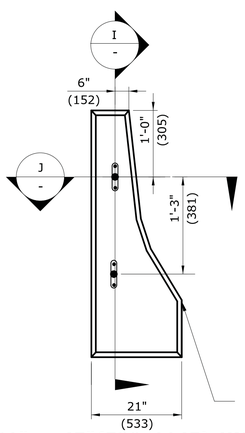
VIEW H (ROTATED)

GENERAL NOTES:

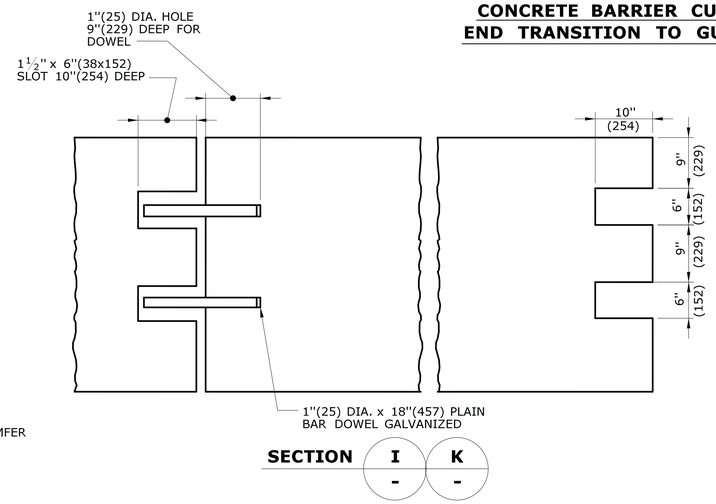
1. GUIDERAIL ATTACHMENT WHEN REQUIRED SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
2. WHEN ATTACHING GUIDERAIL, THE END OF THE PCBC SHALL NOT EXTEND VERTICALLY MORE THAN 3" (76) ABOVE THE RAIL HEIGHT FOR ALL LEADING END TREATMENTS. A 2.5:1 MAX. SLOPE OF THE BARRIER MAY BE USED TO ACCOMPLISH THIS.



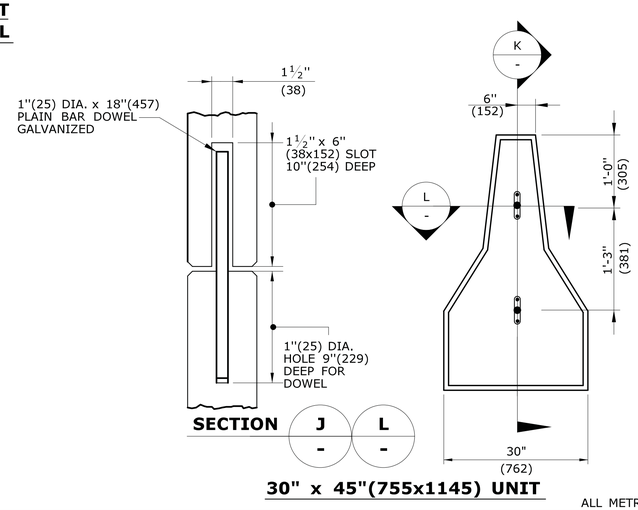
ELEVATION
CONCRETE BARRIER CURB UNIT
END TRANSITION TO GUIDERAIL



21" x 45"(530x1145) UNIT

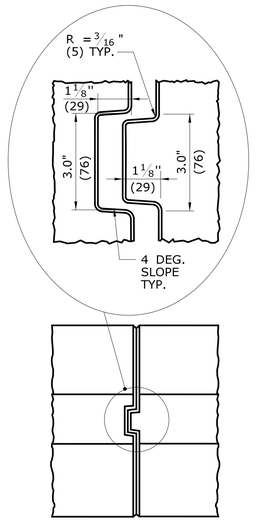


SECTION I - K -



SECTION J - L -

30" x 45"(755x1145) UNIT



PLAN VIEW 30" x 45"
(755x1145) UNIT ALTERNATE
CONNECTION AT ENDS

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

REV.	DATE	REVISION DESCRIPTION
1	5/13/10	REVISE GENERAL NOTE 2 AND ADD NOTE 3
2	1/12	ADD DETAIL FOR GUIDERAIL ATTACHMENT
3	7/13	ERRATA - REMOVE SECTION
-	-	-
-	-	-
-	-	-

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 6/14/2013

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_HIGHWAY_STD2013.dgn Model: 28 - HW-821_02b

SUBMITTED BY: NAME/DATE/TIME:

APPROVED BY: NAME/DATE/TIME:

CTDOT STANDARD SHEET

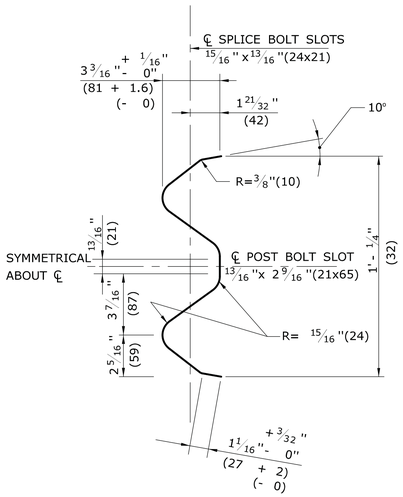
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

45" (1145) F- SHAPE PRECAST CONCRETE BARRIER CURB

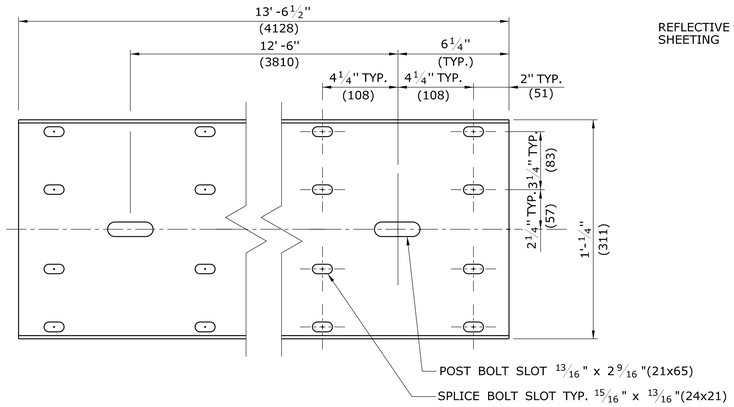
STANDARD SHEET NO.:

HW-821_02b

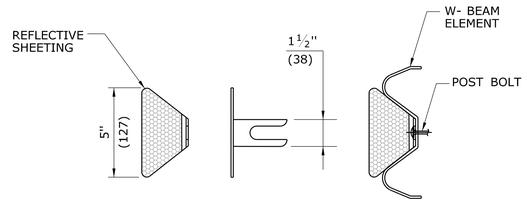


SELECTION THRU RAIL ELEMENT
END VIEW

NOTE: ALL DIMENSIONS SUBJECT TO MANUFACTURING TOLERANCES



TYPICAL W-BEAM RAIL ELEMENT
CLASS A, TYPE II



DELINEATOR DETAIL

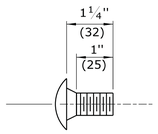
GENERAL NOTES:

1. NEW R-B 350 GUIDERAIL INCLUDING SYSTEMS, ANCHORS AND TRANSITIONS INSTALLED ON EXPRESSWAYS AND RAMPS SHALL USE CLASS B TYPE II (10 GAUGE) W-BEAM RAIL ELEMENTS.
2. W6x9 (W150x14) POSTS MAY BE USED IN PLACE OF W6 x 8.5(W150x13) POSTS.
3. W8x13 (W200x19) POSTS, 7'-6"(2286) LONG, ARE USED WITH TRANSITIONS TO VERTICAL OR SAFETY SHAPE PARAPETS (POSTS 1 AND 2) AND SYSTEM 6.
4. W6x8.5 (W150x13) POSTS, 6'-0"(1829) LONG, ARE USED WITH TRANSITIONS TO VERTICAL OR SAFETY SHAPE PARAPETS (POSTS 3 THOUGH 6), MD-B 350, SYSTEM 5 & 5A, AND STANDARD R-B 350 GUIDERAIL.

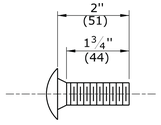
DELINEATOR NOTES:

1. DELINEATORS SHALL BE FORMED OF .080 POLY-CARBONATE OR .080 SHEET ALUMINUM IN ACCORDANCE WITH M.18.13.
2. REFLECTIVE SHEETING SHALL CONFORM TO M.18.09.2.
3. DELINEATORS SHALL BE INSTALLED ON THE POST CLOSEST TO THE DESIGNATED SPACING.
4. REFLECTIVE SHEETING SHALL BE WHITE EXCEPT ON THE LEFT SIDE OF DIVIDED STREETS, HIGHWAYS, RAMPS, AND ONE WAY ROADS IN THE DIRECTION OF TRAVEL WHERE IT SHALL BE YELLOW.
5. INSTALL DELINEATORS ON RAIL THAT IS PARALLEL TO AND NOT GREATER THAN 6'(1829) FROM THE EDGE OF THE ROADWAY. A MINIMUM OF THREE DELINEATORS MUST BE INSTALLED ON ANY RUN OF RAIL.

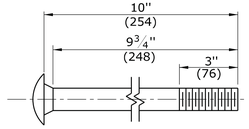
DELINEATOR SPACING:
RADIUS ≥ 300'(91440) - SPACE EVERY 50'(15.24m)
RADIUS < 300'(91440) - SPACE EVERY 25'(7.62m)



W-BEAM SPLICE
BOLT DETAIL

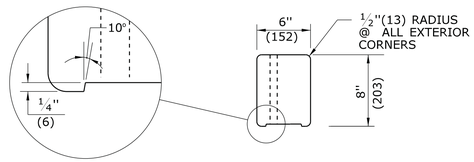


POST BOLT DETAIL
FOR R-B 350
SYSTEM 6 RUBRAIL

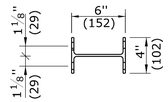


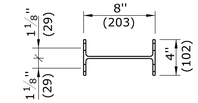
POST BOLT DETAIL FOR R-B 350
AND MD-B 350 GUIDERAIL

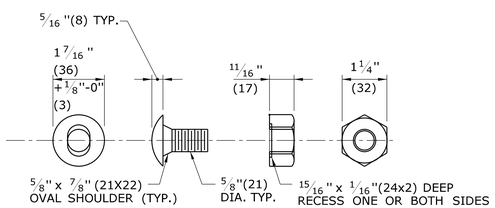
(UNTHREADED PORTION NOT TO EXCEED 6 3/4"(171))



PLAN

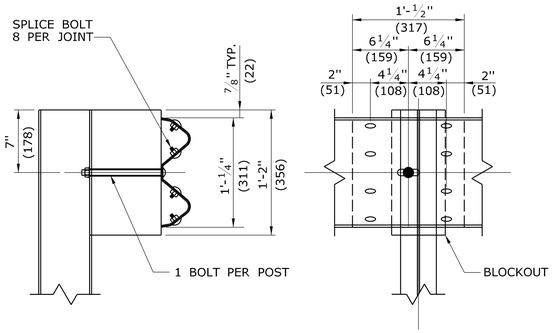






BUTTONHEAD BOLT **HEX NUT**

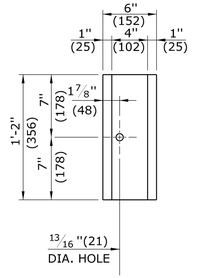
NOTE: AFTER GALVANIZING, THE NUT SHALL BE FREE RUNNING ON THE BOLT. DIAMETER SHOWN IS TYPICAL FOR ALL GUIDERAIL BOLTS. SEE DETAILS ABOVE FOR SPECIFIC LENGTHS.



SECTION **ELEVATION**

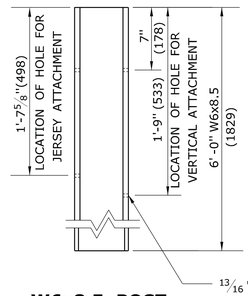
LAP DETAIL

NOTE:
LAP RAIL SECTION IN DIRECTION OF TRAFFIC

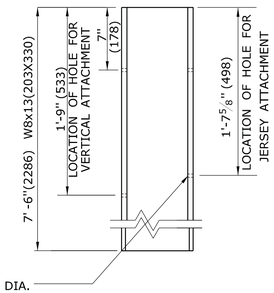


ELEVATION

R-B 350 PLASTIC
BLOCKOUT DETAIL



W6x8.5 POST
(W150x13)
6'-0"(1829) LONG



W8x13 POST
(W200x19)
7'-6"(2286) LONG

BOLT HOLE LAYOUT FOR W8x13(W200x19)
AND W6x8.5 (W150x13)UNIFORM POST

(REFER TO GENERAL NOTES)

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

REV.	DATE	REVISION DESCRIPTION
1	6/11	REMOVE WEATHERING STEEL NOTES
		-
-	-	-
-	-	-
-	-	-
-	-	-

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Plotted Date: 5/10/2011

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_HIGHWAY_STD_JUNE2011.dgn Model: 36-HW-910_01

SUBMITTED BY: NAME/DATE/TIME:

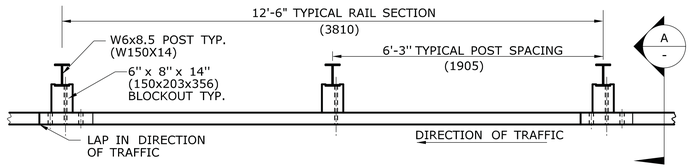
APPROVED BY: NAME/DATE/TIME:

CTDOT
STANDARD SHEET

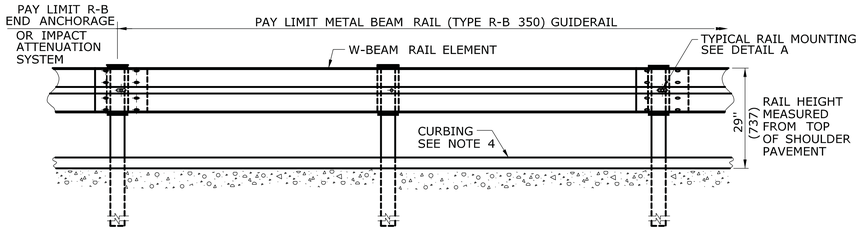
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:
W-BEAM METAL
BEAM RAIL HARDWARE

STANDARD SHEET NO.:
HW-910_01

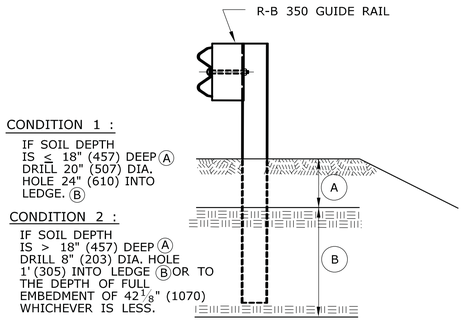


PLAN

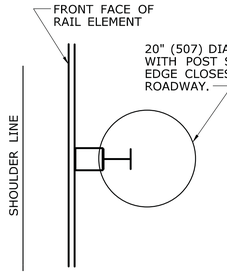


ELEVATION

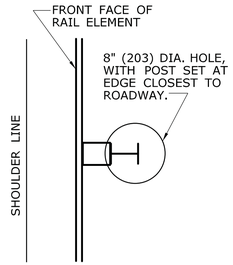
METAL BEAM RAIL (TYPE R-B 350)



ELEVATION
(SEE NOTE 8)



PLAN
CONDITION 1
(SEE NOTE 8)

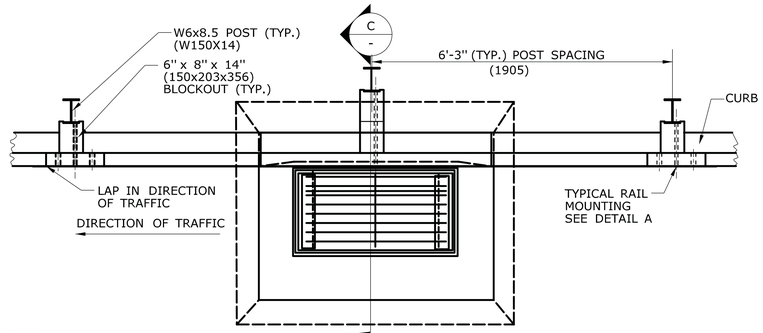


PLAN
CONDITION 2
(SEE NOTE 8)

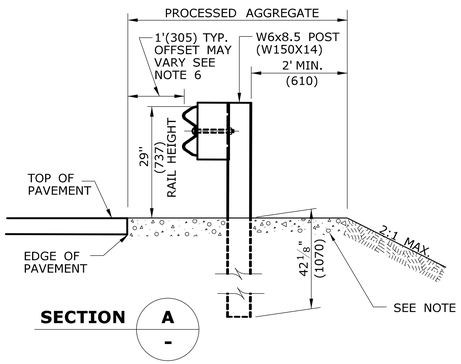
DRILLING IN ROCK FOR GUIDERAIL POSTS

GENERAL NOTES:

1. SEE SHEET HW-910_01 FOR HARDWARE AND DELINEATOR DETAILS.
2. MAXIMUM DESIGN DEFLECTION FOR R-B 350 GUIDERAIL AT THE STANDARD POST SPACING OF 6'-3"(1905) IS 4'-3"(1295). DEFLECTION REQUIREMENT IS MEASURED FROM THE BACK OF POST TO THE FACE OF OBJECT.
3. FOR CURVES WITH RADII OF 150'(45.7m) OR LESS, ALL RAIL ELEMENTS SHALL BE SHOP FABRICATED TO THE PROPER RADIUS AND GALVANIZED AFTER FABRICATION. RADIUS RAIL WHEN REQUIRED AND NOTED ON THE PLANS, IS INCLUDED IN THE PAY ITEM FOR GUIDERAIL.
4. RAIL HEIGHT WITH CURBING SHALL BE MEASURED FROM THE TOP OF PAVEMENT. ON HIGH SPEED ROADWAYS (≥45mph 72.4kph), 4"(102) CURBING MAY BE USED IN CONJUNCTION WITH GUIDERAIL AND THE RAIL ELEMENT SHALL BE PLACED FLUSH WITH THE FACE OF CURB. ON LOW SPEED ROADWAYS (<45mph 72.4kph), 6"(152) CURBING MAY BE USED IN CONJUNCTION WITH GUIDERAIL AND THE RAIL ELEMENT SHALL BE PLACED A MAXIMUM OF 9"(229) BEHIND THE FACE OF CURB.
5. THREE BLOCKOUTS MAY BE USED FOR ONE POST ONLY. TWO BLOCKOUTS MAY BE USED FOR A SERIES OF POSTS. THE COST OF ADDITIONAL BLOCKOUTS AND LONGER BOLTS SHALL BE INCLUDED IN THE BID PRICE PER FOOT OF GUIDERAIL. EXTRA BLOCKOUTS AT TRANSITION TO BRIDGE PARAPETS SHOULD BE AVOIDED.
6. W-BEAM GUIDERAIL MAY BE PLACED 1'(305) OR MORE FROM THE EDGE OF PAVEMENT ONLY ON SLOPES 10:1 OR FLATTER AND WITHOUT CURBING. IF THE RAIL IS INSTALLED WITHIN 2'(610) OF THE EDGE OF PAVEMENT, THE RAIL HEIGHT IS MEASURED FROM THE SHOULDER SLOPE EXTENDED TO THE RAIL. IF THE RAIL IS INSTALLED BEYOND 2'(610) FROM THE EDGE OF PAVEMENT, THE RAIL HEIGHT IS MEASURED FROM THE GROUND DIRECTLY BELOW THE RAIL.
7. ALL R-B 350 GUIDERAIL TYPES INSTALLED ON EXPRESSWAYS AND RAMPS SHALL USE CLASS B, TYPE-II (10 GAUGE) W-BEAM RAIL ELEMENTS.
8. 20" (507) DIA. EXCAVATED HOLE SHALL BE BACKFILLED WITH SUITABLE MATERIAL, OR GRANULAR FILL COMPACTED IN 6" (150) LIFTS BEFORE DRIVING POST OR POSTS MAY BE SET IN EXCAVATED HOLE AND BACKFILLED WITH CONTROLLED LOW STRENGTH MATERIAL (CLSM). 8" (203) DIA. HOLE SHALL BE BACKFILLED WITH SUITABLE MATERIAL.
9. AS DIRECTED BY THE ENGINEER AND WHERE PAVEMENT FOR RAILING IS NOT BEING INSTALLED, A MIN. 6" DEPTH OF PROCESSED AGGREGATE SHALL BE INSTALLED FROM THE PAVEMENT EDGE OR BACK OF CURB TO A MINIMUM OF 2'(610) BEHIND THE GUIDERAIL POST AND COMPACTED IN 6" (150) LIFTS.
10. MINIMUM RAIL HEIGHT FOR NEW CONSTRUCTION SHALL BE 29" (737) ± 1" (25).

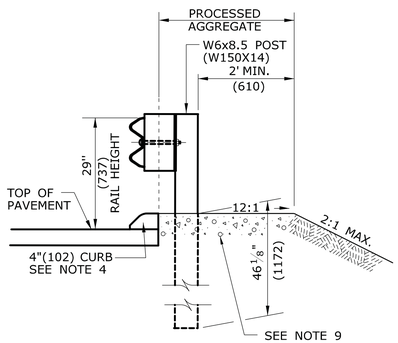


PLAN



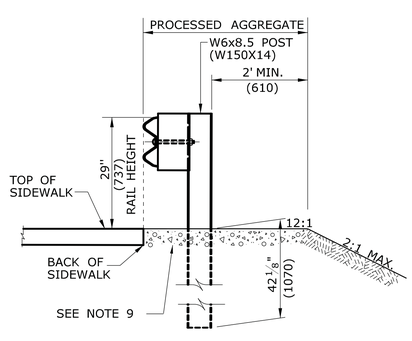
SECTION A -

NO CURB APPLICATION



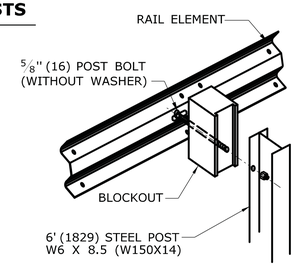
SECTION A -

CURB APPLICATION

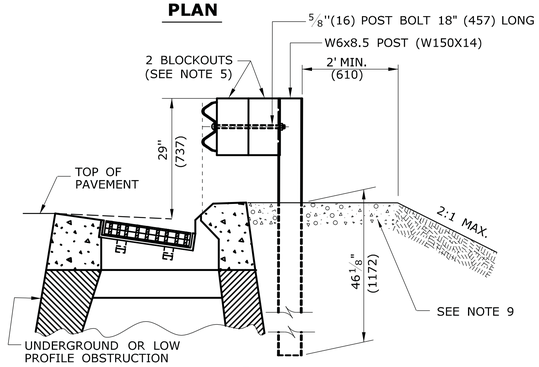


SECTION A -

SIDEWALK APPLICATION



DETAIL A
RAIL MOUNTING



SECTION C -

MULTIPLE BLOCKOUT APPLICATION (MAY BE USED TO AVOID UNDERGROUND OR LOW PROFILE OBSTRUCTION)

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

REV.	DATE	REVISION DESCRIPTION
1	6/11	REVISED NOTE 9 FOR USE OF PROCESSED AGGREGATE -
		AND REMOVED NOTE 6 FOR WEATHERING STEEL -
-	-	- -
-	-	- -
-	-	- -
-	-	- -

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 5/10/2011

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_HIGHWAY_STD_JUNE2011.dgn Model: 37- HW-910_02

SUBMITTED BY: NAME/DATE/TIME:

APPROVED BY: NAME/DATE/TIME:

CTDOT
STANDARD SHEET

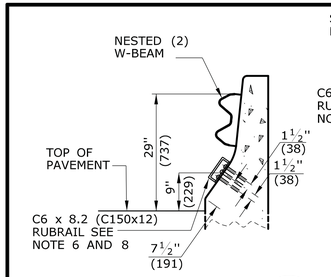
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

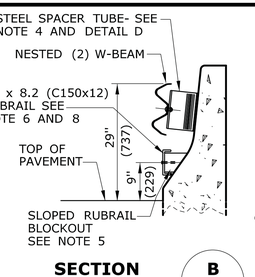
METAL BEAM RAIL (TYPE R-B 350) GUIDERAIL

STANDARD SHEET NO.:

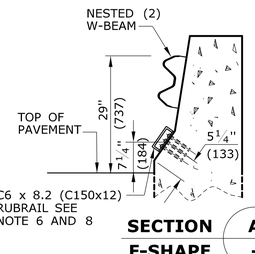
HW-910_02



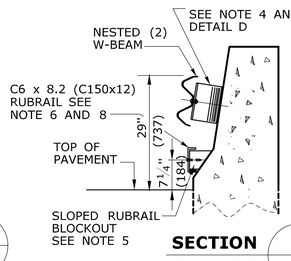
SECTION A
JERSEY SHAPE
A -



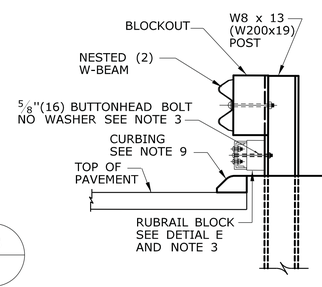
SECTION B
JERSEY SHAPE
B -



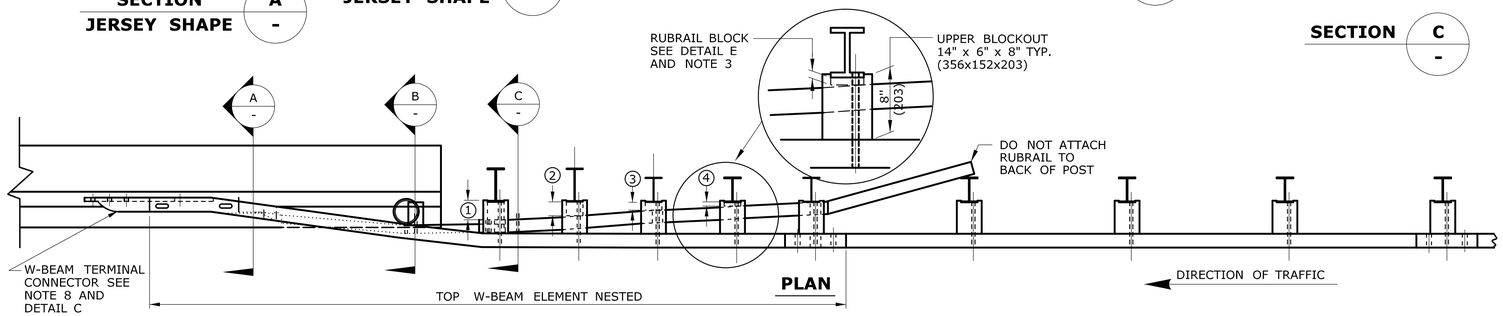
SECTION A
F-SHAPE
A -



SECTION B
F-SHAPE
B -



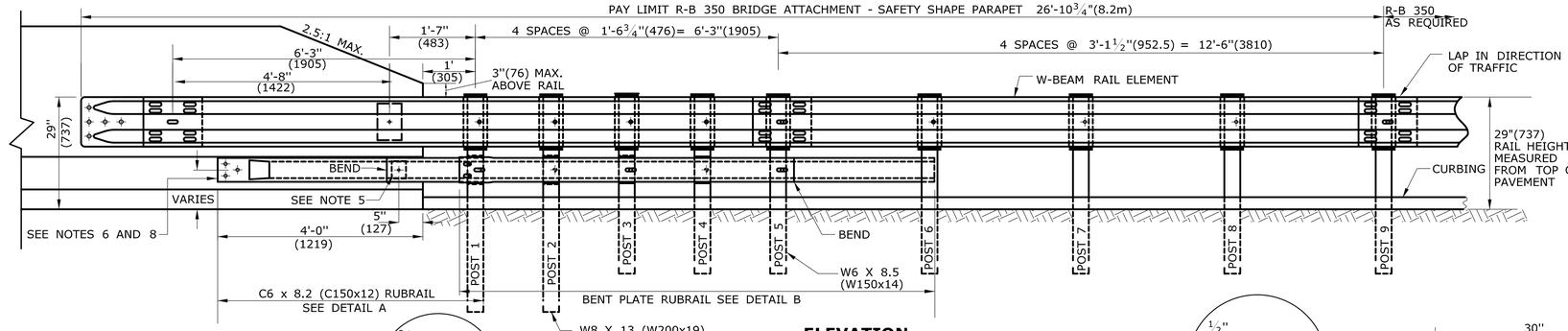
SECTION C
C -



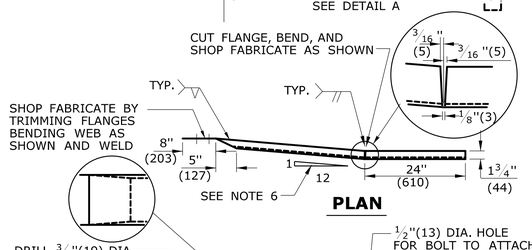
PLAN

GENERAL NOTES:

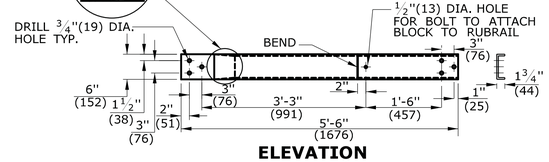
1. THIS R-B 350 GUIDERAIL TRANSITION IS APPROPRIATE FOR CONNECTION AT THE FOLLOWING LOCATIONS:
   (A) JERSEY SHAPE OR F-SHAPE PARAPET OR BARRIER WITH NO ELECTRICAL JUNCTION BOX WITHIN THE LIMITS OF THE ATTACHMENT (APPROXIMATELY 8'(2.44m)).
   (B) BRIDGE PARAPETS RETROFITTED FROM SAFETY WALK TO MODIFIED SAFETY SHAPE WITH NO ELECTRICAL JUNCTION BOX WITHIN THE LIMITS OF THE ATTACHMENT (APPROXIMATELY 8'(2.44m)).
   (C) LEADING AND TRAILING ENDS ON DUAL DIRECTION ROADWAYS.
2. POSTS 1 THROUGH 5 REQUIRE AN ADDITIONAL HOLE TO ATTACH LOWER BLOCKOUTS AND/OR RUBRAIL.
3. RUBRAIL BLOCKOUTS LOCATED ON POSTS 1 THROUGH 4 ARE OFFSET DRILLED AND SECURED WITH ⅝"(16) BUTTONHEAD BOLTS (SEE CHART FOR BOLT LENGTHS). SECURE BLOCKS TO POSTS 2 AND 4. SECURE RUBRAIL AND BLOCKOUTS TO POSTS 1 AND 3. RUBRAIL IS SECURED TO POST 5 WITH A ⅝" x 4½" (16x114) BUTTONHEAD BOLT. RUBRAIL IS FLARED TO BACK OF POST 6 AND NOT SECURED.
4. STEEL SPACER TUBE IS A SCHEDULE 40 GALVANIZED PIPE 6"(152) INSIDE DIAMETER x 9"(229) LONG. ATTACH TUBE TO RAIL ONLY WITH ⅝" x 1¼"(16x32) LONG BUTTONHEAD BOLT AND RECTANGULAR PLATE WASHER. TUBE IS NOT ATTACHED TO PARAPET.
5. SEE DETAIL F FOR SLOPED RUBRAIL BLOCKOUT. BLOCKOUT IS ATTACHED TO RAIL ELEMENT ONLY. USE ⅜" x 3"(10x76) LAG BOLT WITH FLAT WASHER.
6. SHOP FABRICATE THE C6 x 8.2 (C150x12) RUBRAIL END TO BE CONSISTENT WITH THE SLOPE OF THE JERSEY SHAPE OR F-SHAPE AND ATTACH FLUSH WITH THE SLOPED TOE OF THE PARAPET OR BARRIER.
7. USE CLASS B TYPE II W-BEAM RAIL ELEMENTS (10 GAUGE) FOR INSTALLATIONS ON EXPRESSWAYS AND RAMPS.
8. ANCHORAGE:
   (A) AT EXISTING PARAPETS OR BARRIERS, RUBRAIL SHALL BE ANCHORED USING THREE ⅝" x 6"(16x152) CHEMICALLY ANCHORED BOLTS WITH WASHERS OR AS DETAILED ON STRUCTURE SHEETS. MAXIMUM BOLT PROJECTION BEYOND THE NUT SHALL BE ½"(13).
   (B) AT EXISTING PARAPETS OR BARRIERS, THE W-BEAM TERMINAL CONNECTOR SHALL BE ANCHORED USING FIVE ⅞" x 12"(22x305) CHEMICALLY ANCHORED BOLTS WITH WASHERS, OR AS DETAILED ON THE STRUCTURE SHEETS. MAXIMUM BOLT PROJECTION BEYOND THE NUT SHALL BE ½"(13). THE 12"(305) MINIMUM LENGTH OF CHEMICALLY ANCHORED BOLTS SHALL INCLUDE A MINIMUM EMBEDMENT DEPTH OF 10"(254) INTO SUITABLY REINFORCED CONCRETE, OR AS RECOMMENDED BY MANUFACTURER OF BONDING MATERIAL.
   (C) NEW PARAPETS OR BARRIERS, THE W-BEAM TERMINAL CONNECTOR AND RUBRAIL SHALL BE ANCHORED AS DETAILED ON THE STRUCTURE PLANS.
9. FOR NEW CONSTRUCTION WHERE CURBING IS NEEDED, USE EITHER 4"(102) BITUMINOUS CONCRETE PARK CURBING OR PRECAST CONCRETE TRANSITION CURBING SET WITH A 4"(102) REVEAL. IF EXISTING CURBING IS GRANITE STONE TRANSITION CURBING, RESET CURB TO A 4"(102) REVEAL OR REMOVE IT AND REPLACE IT WITH 4"(102) CURBING.
10. POSTS 1 AND 2 ARE W8 x 13 (W200x19), 7'-6"(2286) LONG. ALL OTHER POSTS IN TRANSITION ARE W6 x 8.5 (W150x14), 6'(1829) LONG.
11. ADDITIONAL BLOCKOUTS WITH POSTS 1 THROUGH 6 SHOULD BE AVOIDED.
12. FOR SINGLE DIRECTION ROADWAY:
    INSTALL W-BEAM TERMINAL CONNECTOR BETWEEN NESTED GUIDERAIL ELEMENTS.
    FOR DUAL DIRECTION ROADWAY FOR APPROACHING TRAFFIC:
    INSTALL W-BEAM TERMINAL CONNECTOR BETWEEN NESTED GUIDERAIL ELEMENTS.
    FOR TRAILING END:
    INSTALL W-BEAM TERMINAL CONNECTOR OUTSIDE OF THE NESTED GUIDERAIL ELEMENTS.
13. MINIMUM RAIL HEIGHT FOR NEW CONSTRUCTION SHALL BE 29" (737) + 1" (25).



ELEVATION

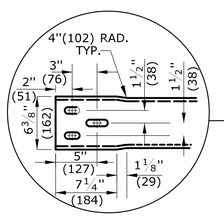


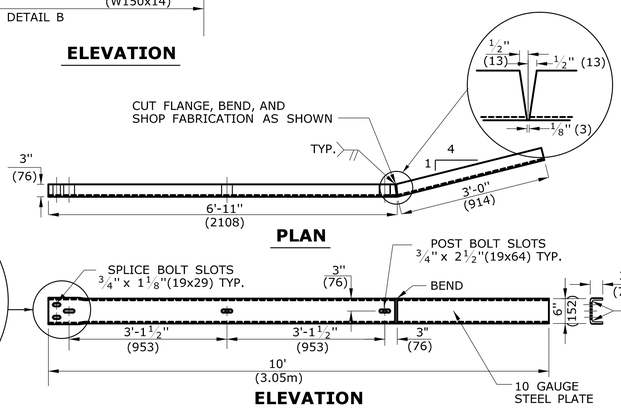
PLAN



ELEVATION

DETAIL A - C6 x 8.2 (C150x12) RUBRAIL

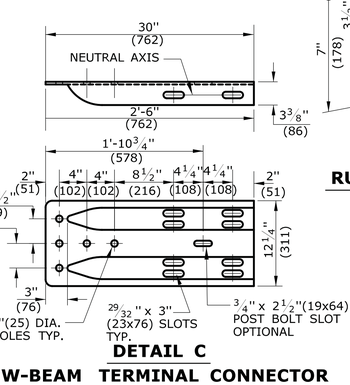




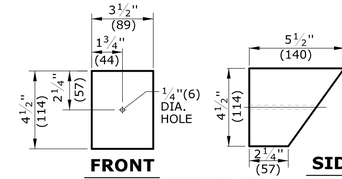
PLAN

ELEVATION

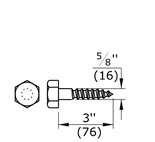
DETAIL B BENT PLATE RUBRAIL



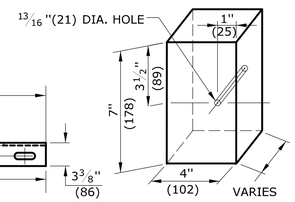
DETAIL C
W-BEAM TERMINAL CONNECTOR
CLASS B TYPE II
SEE NOTE 12



DETAIL F
SLOPED RUBRAIL BLOCKOUT



DETAIL G
LAG BOLT

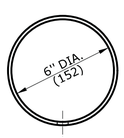


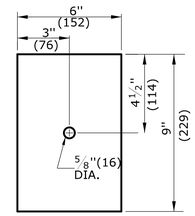
DETAIL E
RUBRAIL BLOCKOUT

RUBRAIL BLOCKS
7"(178) HIGH x 4"(102) WIDE

POST	THICKNESS	BOLT LENGTH
1	4¼"(108)	9"(229)
2	3¼"(83)	5"(127)*
3	2"(51)	6"(152)
4	1"(25)	3"(76)*

* BOLTS FOR POSTS 2 AND 4 ARE USED TO ATTACH BLOCK TO POST. RUBRAIL NOT ATTACHED TO BLOCK.





DETAIL D
STEEL SPACER TUBE
SEE NOTE 4

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

REV.	DATE	REVISION DESCRIPTION
1	6/11	REVISED NOTE 8 FOR USE OF CHEMICAL ANCHORS AND PAY ITEM CHANGED TO SAFETY SHAPE. -
-	-	- -
-	-	- -
-	-	- -
-	-	- -

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 5/10/2011

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_HIGHWAY_STD_JUNE2011.dgn Model: 41 - HW-910_06

SUBMITTED BY: NAME/DATE/TIME:

APPROVED BY: NAME/DATE/TIME:

CTDOT STANDARD SHEET

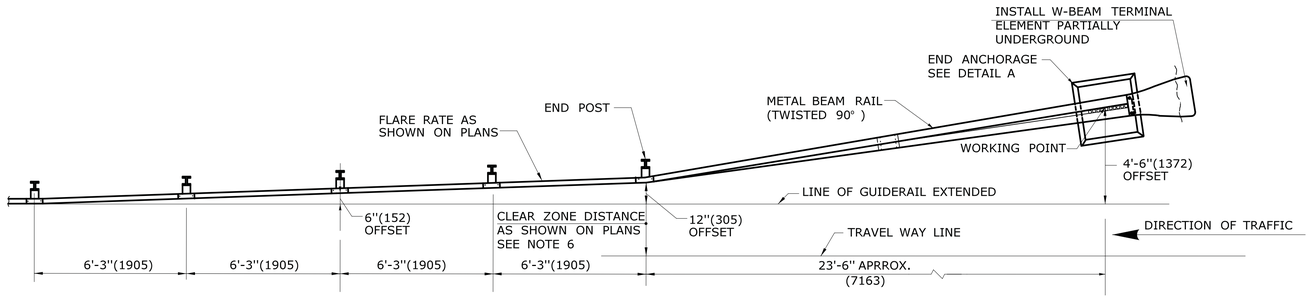
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

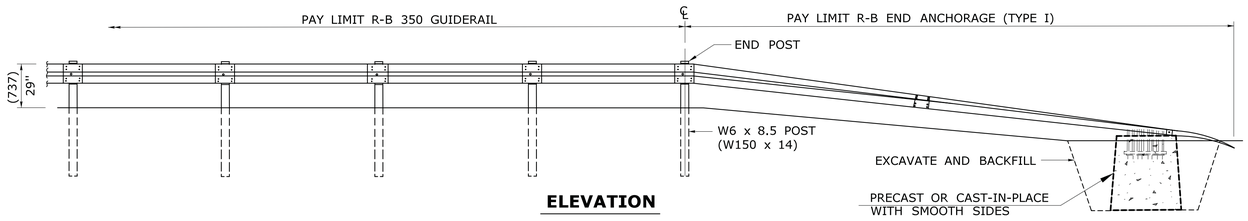
R-B 350 BRIDGE ATTACHMENT TO SAFETY SHAPE PARAPET

STANDARD SHEET NO.:

HW-910_06



PLAN



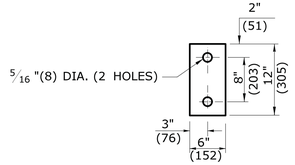
ELEVATION

R-B END ANCHORAGE TYPE I (ROADSIDE APPLICATION)

GENERAL NOTES:

1. R-B END ANCHORAGE TYPE I INSTALLED ON FREEWAYS AND RAMPS SHALL USE CLASS B (10 GAUGE) TERMINAL AND W-BEAM RAIL ELEMENTS. ALL OTHER R-B END ANCHORAGE TYPE I SHALL USE CLASS A (12 GAUGE) TERMINAL AND W-BEAM RAIL ELEMENTS.
2. R-B END ANGHORAGE TYPE II SHALL USE CLASS A (12 GAUGE) TERMINAL AND W-BEAM RAIL ELEMENTS.
3. OTHER RADII WHICH CAN BE DEMONSTRATED TO PROVIDE THE INSTALLATIONS SHOWN FOR END ANCHORAGE TYPE II MAY BE APPROVED.
4. J-HOOK BOLTS MAY BE SUBSTITUTED FOR BOTTOM PLATE ANCHORAGE IN CONCRETE END ANCHORS USING THE SAME SIZE, STRENGTH, AND LENGTH AS NOTED ON THE PLANS.
5. MINIMUM RAIL HEIGHT FOR NEW CONSTRUCTION SHALL BE 29" (737) + 1' (25).
6. R-B TYPE I END ANCHOR: INSTALL LEADING END USING DESIGN CLEAR ZONE DISTANCE MEASURED FROM TRAVEL WAY LINE TO END POST AS SHOWN ON PLANS, THEN PLACE CONCRETE ANCHOR 4'-6"(1372) FROM THE LINE OF GUIDERAIL EXTENSION. INSTALL TRAILING END ON DUAL DIRECTION ROADWAYS IN THE SAME MANNER EXCEPT MEASURE CLEAR ZONE FROM THE DOUBLE YELLOW LINE. INSTALL TRAILING END ON SINGLE DIRECTION ROADWAYS MEASURED FROM THE GUIDERAIL LINE EXTENDED AND OFFFSET 4'-6" (1372).

   R-B TYPE II END ANCHOR: INSTALL ONLY ON ROADWAYS WITH DESIGN SPEEDS < 45mph (72kph). FOR DUAL DIRECTION ROADWAYS, INSTALL IN THE SAME MANNER AS TYPE I END ANCHOR FOR BOTH LEADING AND TRAILING END USING DESIGN CLEAR ZONE DISTANCE EXCEPT PLACE CONCRETE ANCHOR 2'-4" (711) MEASURED FROM THE LINE OF GUIDERAIL EXTENDED. INSTALL TRAILING END ON SINGLE DIRECTION ROADWAYS MEASURED FROM THE GUIDERAIL LINE EXTENDED AND OFFSET 2'-4" (711).
7. SEE HW-911_04 FOR TYPICAL END ANCHOR GRADING PLAN.

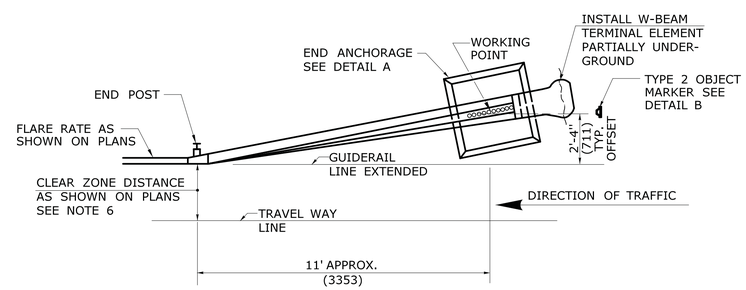


DETAIL B

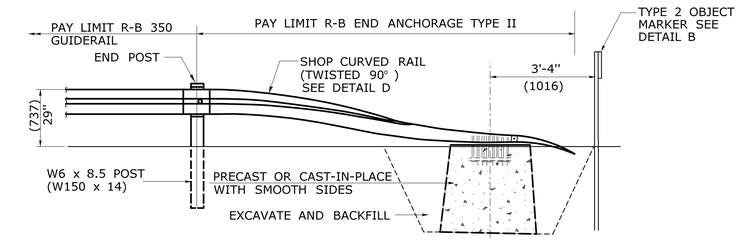
TYPE 2 OBJECT MARKER

SIGN # 51-5030

OBJECT MARKER FACE SHALL BE YELLOW TYPE III RETROREFLECTORIZED. THE BOTTOM OF TYPE 2 OBJECT MARKER SHALL BE 4'(1219) ABOVE GROUND LEVEL MOUNTED ON METAL DELINEATOR POST. IT SHALL BE USED TO IDENTIFY CULVERT ENDS AND TYPE II END ANCHORS, OR AS DIRECTED BY THE ENGINEER.



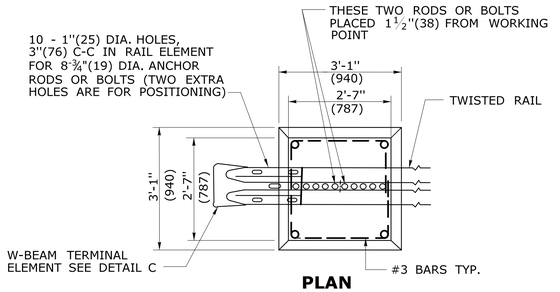
PLAN



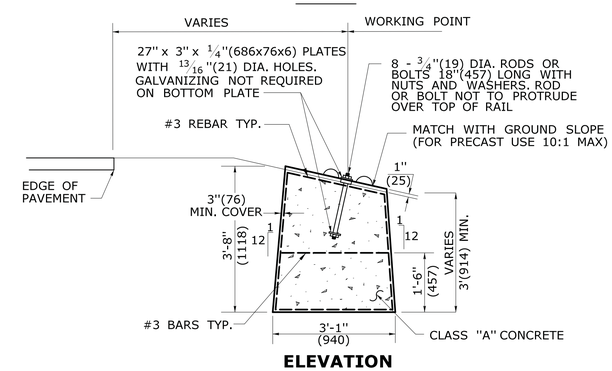
ELEVATION

R-B END ANCHORAGE TYPE II

SEE NOTE 2



PLAN

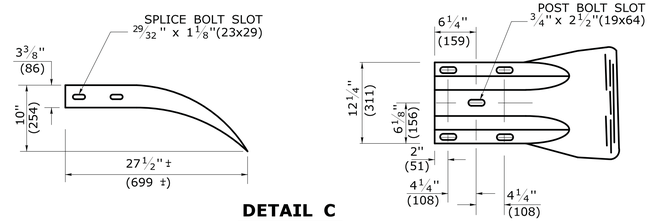


ELEVATION

DETAIL A

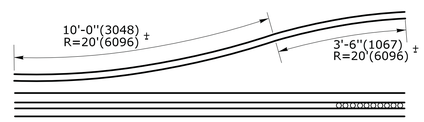
ROADSIDE CONCRETE END ANCHOR

SEE NOTE 4



DETAIL C

W-BEAM TERMINAL ELEMENT



DETAIL D

SHOP CURVED RAIL

SEE NOTE 3

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

REV.	DATE	REVISION DESCRIPTION
1	6/11	REVISED TYPE I AND II ANCHOR FOR CLEAR ZONE PLACEMENT
2	7/13	ADD POST OFFSET DISTANCE
-	-	-
-	-	-
-	-	-
-	-	-

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Plotted Date: 6/13/2013

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_HIGHWAY_STD2013.dgn Model: 70 - HW-911_01

SUBMITTED BY: NAME/DATE/TIME:

APPROVED BY: NAME/DATE/TIME:

CTDOT STANDARD SHEET

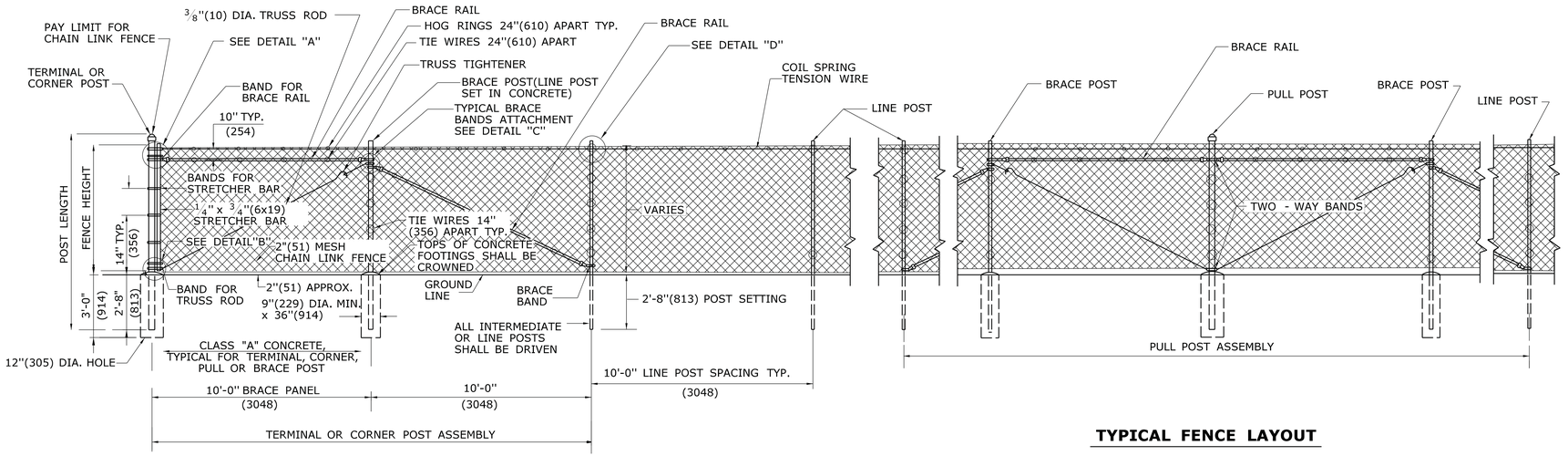
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

R-B END ANCHORAGE TYPE I AND II

STANDARD SHEET NO.:

HW-911_01

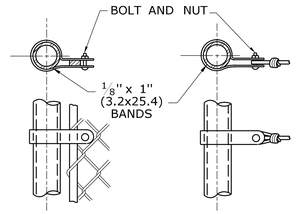


ROUND PIPE TERMINAL CORNER OR PULL POST

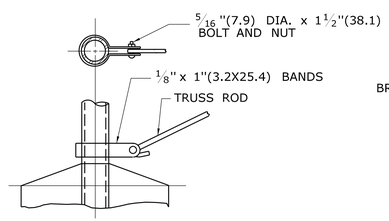
TYPICAL FENCE LAYOUT

GENERAL NOTES:

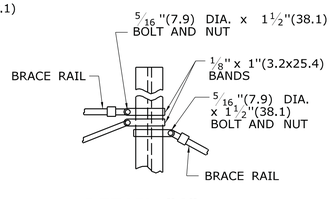
1. REFER TO SPECIFICATIONS FOR LOCATION OF PULL POST ASSEMBLIES.
2. ALL SQUARE AND ROUND POSTS WILL BE CAPPED TO PREVENT WATER FROM ENTERING.
3. WHERE ROCK IS ENCOUNTERED, IT SHALL BE DRILLED AND THE POSTS SET IN CONCRETE OR MORTAR.
4. FENCE SHALL BE PLACED WITH FABRIC FACING OUTSIDE HIGHWAY RIGHT OF WAY OR AS DIRECTED BY THE ENGINEER.



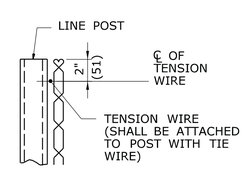
DETAIL "A"
STRETCHER BAR AND TENSION WIRE ATTACHMENTS



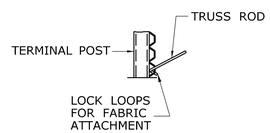
DETAIL "B"
TRUSS ROD ATTACHMENT



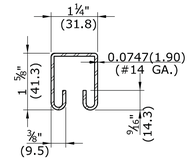
DETAIL "C"
ATTACHMENT OF BRACE RAILS TO LINE POSTS (TYPICAL)



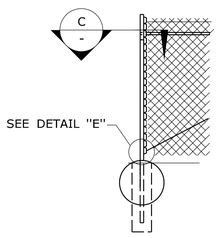
DETAIL "D"
LOCATION OF TENSION WIRE



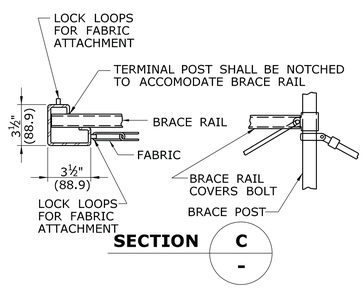
DETAIL "E"
TRUSS ROD ATTACHMENT



DETAIL "F"
BRACE RAIL



TERMINAL, CORNER OR PULL POST



FABRIC AND BRACE RAIL ATTACHMENT

LINE BRACE OR INTERMEDIATE POST	TYPE	MATERIAL	FENCE HEIGHT 6'(1.8m) OR LESS: DIMENSIONS INCHES(mm)	6'(1.8m) OR LESS: WEIGHT LBS/FT(kg/m)	GREATER THAN 6'(1.8m): DIMENSIONS INCHES(mm)	GREATER THAN 6'(1.8m): WEIGHT LBS/FT(kg/m)
	"C"	STEEL	1.87 X 1.62 (47.5x41.1)	2.40(3.57)	2.25 x 1.70(57.2x43.2)	2.78(4.14)
	PIPE	STEEL CLASS 1	1.90(48.3) O.D.	2.72(4.05)	2.37(60.2) O.D.	3.65(5.43)
		STEEL CLASS 2		2.28(3.39)		3.12(4.64)
		ALUM. ALLOY		0.94(1.40)		1.25(1.86)
BRACE RAIL (OR TOP RAIL WHEN SPECIFIED)	ROLL- FORMED*	STEEL	1.62 X 1.25 (41.1x31.75)	1.35(2.01)	——	——
	PIPE	STEEL CLASS 1	1.31(33.3) O.D.	1.68(2.50)	1.66(42.2) O.D.	2.27(3.38)
		STEEL CLASS 2		1.34(1.99)		1.84(2.74)
		ALUM. ALLOY	1.62(41.1) O.D.	0.78(1.16)	1.62(41.1) O.D.	0.78(1.16)
TERMINAL CORNER OR PULL POST	PIPE	STEEL CLASS 1 STEEL CLASS 2 ALUM. ALLOY	2.37(60.2) O.D.	3.65(5.43) 3.12(4.64) 1.25(1.86)	2.87(72.9) O.D.	5.79(8.62) 4.64(6.91) 2.00(2.98)

NOTE: A MINUS TOLERANCE OF 5% IN SIZE AND WEIGHT SHALL BE ALLOWED FOR THESE MEMBERS, BUT WILL NOT APPLY TO THE ZINC OR POLYVINYL CHLORINE COATING. * DIMENSIONS AND WEIGHT ARE FOR A FENCE HEIGHT OF 9'(2.7m) OR LESS.

MINIMUM DIMENSIONS AND WEIGHTS FOR POSTS AND RAILS

REV.	DATE	REVISION DESCRIPTION
1	6/01/10	REMOVE "H" POST & SQUARE TUBULAR POST DETAILS AND NOTES
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 6/23/2011

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_HIGHWAY_STD_JUNE2011.dgn Model: 65 - HW-913_01

SUBMITTED BY: NAME/DATE/TIME:

APPROVED BY: NAME/DATE/TIME:

CTDOT STANDARD SHEET

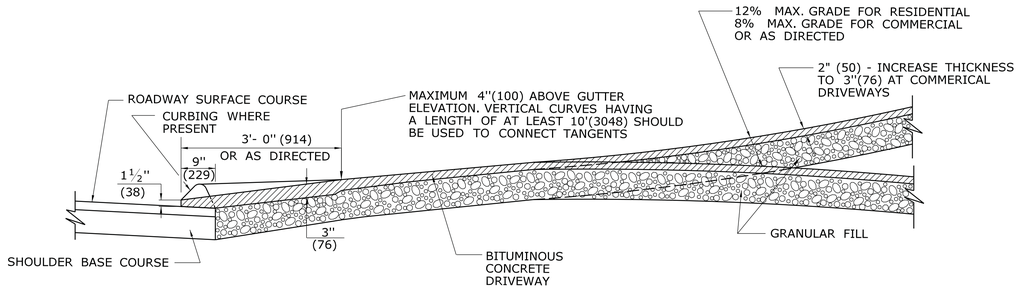
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

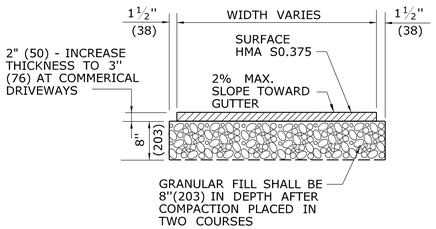
CHAIN LINK FENCE

STANDARD SHEET NO.:

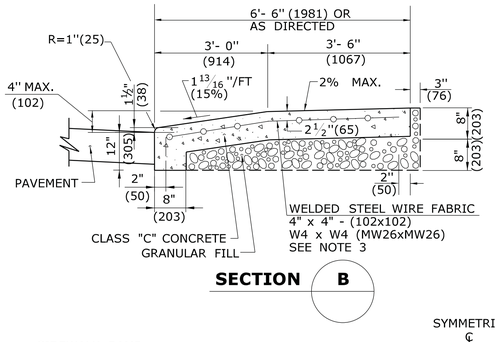
HW-913_01



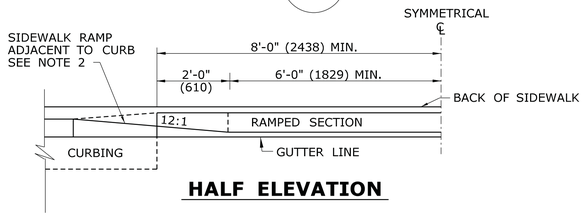
SECTION A



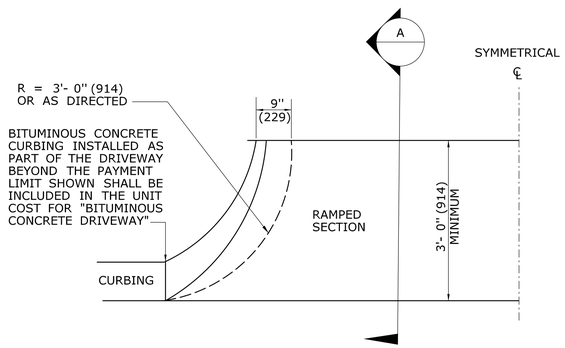
TYPICAL SECTION
BITUMINOUS CONCRETE
SIDEWALK AND DRIVE



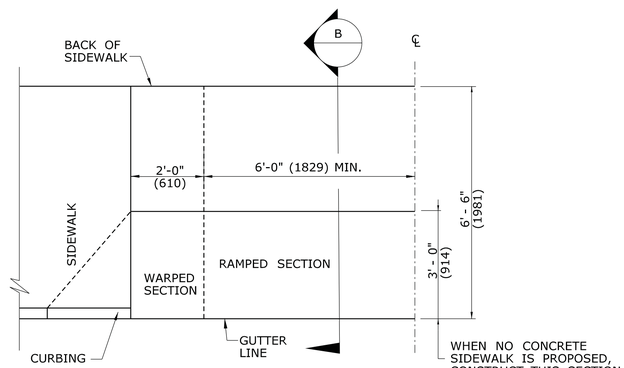
SECTION B



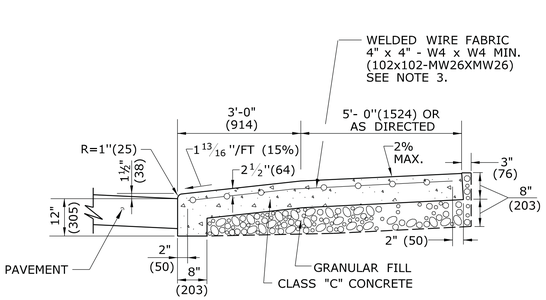
HALF ELEVATION



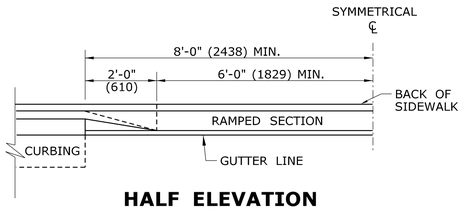
HALF BITUMINOUS CONCRETE
DRIVEWAY PLAN



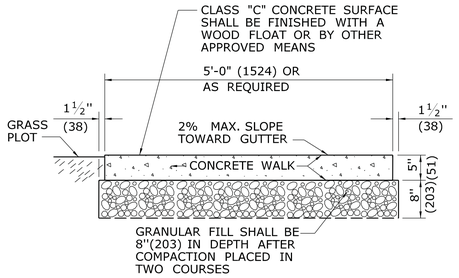
HALF PLAN OF
CONCRETE DRIVEWAY RAMP WHERE
SIDEWALK ADJOINS CURBING



SECTION C

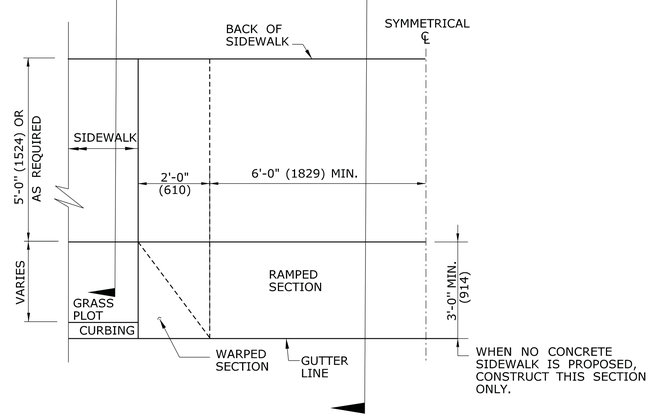


HALF ELEVATION



SECTION D

5' (1524) WIDE CONCRETE
SIDEWALK WITH GRASS PLOT



HALF PLAN OF
CONCRETE DRIVEWAY RAMP WHERE
CURB IS SEPARATED FROM
SIDEWALK BY GRASS PLOT

GENERAL NOTES:

1. DRIVEWAY ENTRANCE SHALL BE A MINIMUM OF 12' (3658) WIDE, EXCLUDING CURBING WHEN PRESENT.
2. SIDEWALK RAMPS SHALL BE A MINIMUM OF 36" (914) TO 40" (1016) MAXIMUM, WITH A MAXIMUM SLOPE OF 12:1. THERE SHALL BE NO LIP AT THE DRIVEWAY SIDEWALK INTERFACE.
3. WELDED WIRE FABRIC MATS WITH REINFORCING AT CLOSER SPACING MAY BE USED.

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

REV.	DATE	REVISION DESCRIPTION
1	6/01/10	REVISED BORDER TITLE
2	6/01/10	REVISED HALF ELEVATION DETAILS
3	1/12	REVISE 2% MAX. SLOPE NOTE
-	-	-
-	-	-
-	-	-

Plotted Date: 1/9/2012

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: Jan2012.dgn Model: HW-921_01

SUBMITTED BY: NAME/DATE/TIME:

APPROVED BY: NAME/DATE/TIME:

CTDOT
STANDARD SHEET

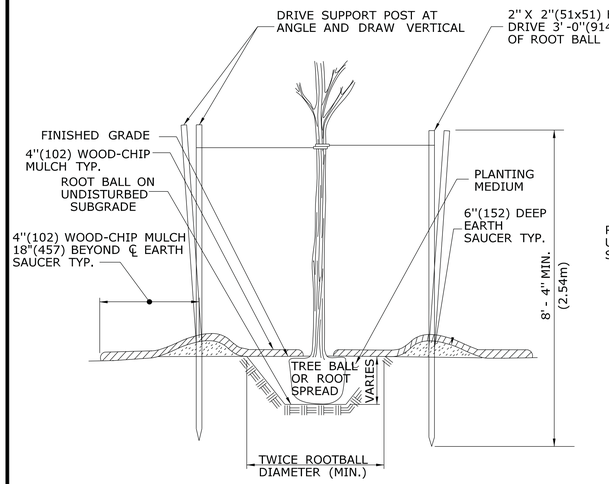
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

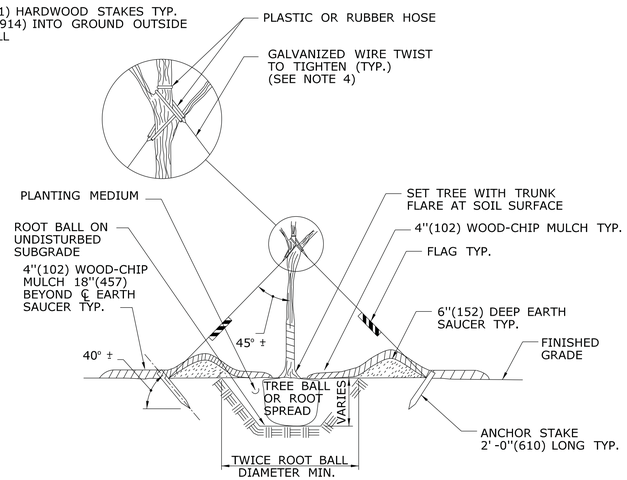
DRIVEWAY RAMPS AND SIDEWALKS

STANDARD SHEET NO.:

HW-921_01



SECTION A -

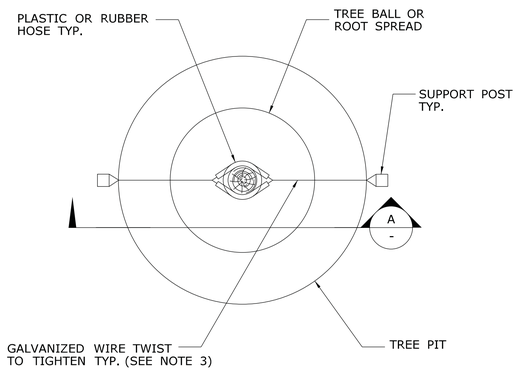


SECTION B -

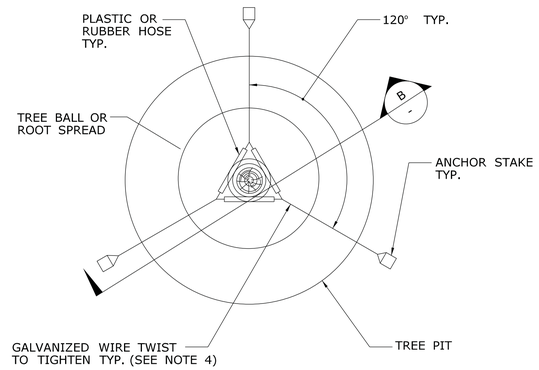
NOTE:

USE 3 POSTS FOR STAKING DECIDUOUS TREES 3"(76) CALIPER OR GREATER AND EVERGREEN TREES 8' (2.4m) HIGH OR GREATER. USE 2 POSTS FOR STAKING DECIDUOUS TREES LESS THAN 3"(76) CALIPER AND EVERGREEN TREES LESS THAN 8' (2.4m) HIGH.

GUYING PLAN

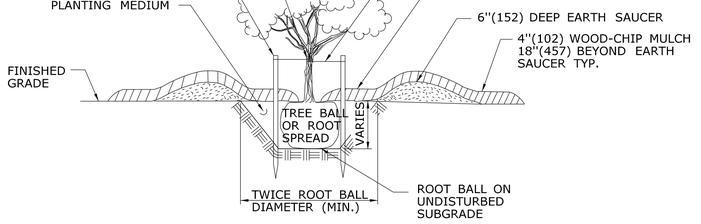


PLAN

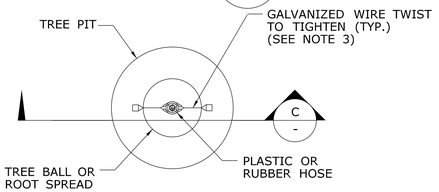


PLAN

STAKING PLAN



SECTION C -



PLAN

STAKING PLAN FOR LOW BRANCHING DECIDUOUS AND EVERGREEN TREES FROM 5'(1.5m) TO 8'(2.4m) HIGH.

EXAMPLES OF MINIMUM SIZE OF ROOT BALL FOR NURSERY GROWN PLANTS.

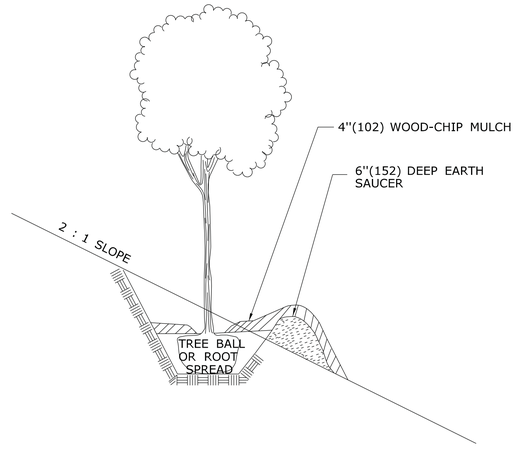
CALIPER* INCHES(mm)	BALL DIAMETER INCHES(mm)	PLANTING PIT SIZE INCHES(mm)
-	8(203)	16(406)
-	10(254)	20(508)
1/2(12.5)	12(304)	24(610)
3/4(19)	14(356)	28(711)
1(25)	16(406)	32(813)
1 1/2(38)	20(508)	40(1016)
2(50)	24(610)	48(1219)
2 1/2(65)	28(711)	56(1422)
3(75)	32(813)	64(1626)
3 1/2(90)	38(965)	76(1930)
4(100)	42(1166)	84(2134)
5(125)	54(1372)	108(2743)
6(150)**	-	-

* THE CALIPER OF THE TRUNK IS MEASURED 6"(152) ABOVE THE GROUND UP TO AND INCLUDING 4"(102) SIZES AND 12"(305) ABOVE THE GROUND FOR LARGER SIZES OR AS SPECIFED IN THE MOST RECENT EDITION OF THE AMERICAN STANDARD FOR NURSERY STOCK, PUBLISHED BY THE AMERICAN ASSOCIATION OF NURSERYMEN.

** TREES GREATER THAN OR EQUAL TO 6"(152) CALIPER SHALL HAVE A ROOT BALL DIAMETER EQUAL TO 10"(254) PER INCH(25mm) OF TRUNK CALIPER (A 7"(178) CALIPER TREE SHOULD HAVE A ROOT BALL DIAMETER EQUAL TO 70"(1778)).

GENERAL NOTES:

1. THE PLANTING PIT SIZE SHALL BE TWICE THE DIAMETER OF THE ROOT BALL IN WIDTH AND 2"(51) LESS THAN THE HEIGHT OF THE ROOT BALL.
2. ALL EXTERIOR PACKAGING MATERIAL APPLIED TO PLANTS SHALL BE REMOVED AFTER THE PLANT IS LOCATED IN THE PLANTING PIT. CUT AND REMOVE TWINE, BURLAP OR WIRE BASKETS FROM THE TOP 2/3RDS OF THE ROOT BALL.
3. USE DOUBLE STRAND NO. 12 WIRE FOR DECIDUOUS TREES GREATER THAN OR EQUAL TO 3"(76) CALIPER AND USE DOUBLE STRAND NO. 10 WIRE FOR EVERGREEN TREES GREATER THAN OR EQUAL TO 8"(203) CALIPER.
4. TREE TRUNK WRAPPING MATERIAL SHALL BE USED AS DIRECTED BY THE ENGINEER.
5. PLANTING PITS FOR INDIVIDUAL SHRUBS ON SLOPES SHALL BE THREE TIMES THE DIAMETER OF THE ROOT BALL IN WIDTH.



GRADING PLAN FOR TREES ON SLOPES

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

REV.	DATE	REVISION DESCRIPTION
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-

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Plotted Date: 6/23/2011

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_HIGHWAY_STD_JUNE2011.dgn Model: 71 - HW-949_01

SUBMITTED BY: NAME/DATE/TIME:

APPROVED BY: NAME/DATE/TIME:

CTDOT STANDARD SHEET

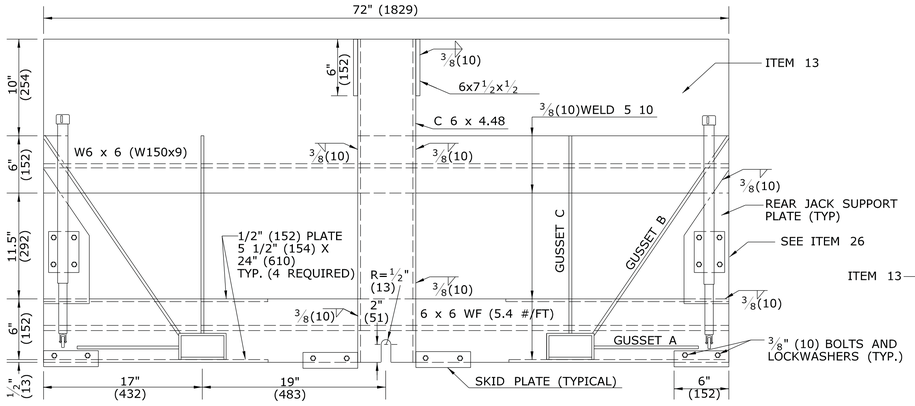
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

PLANTING DETAILS FOR TREES

STANDARD SHEET NO.:

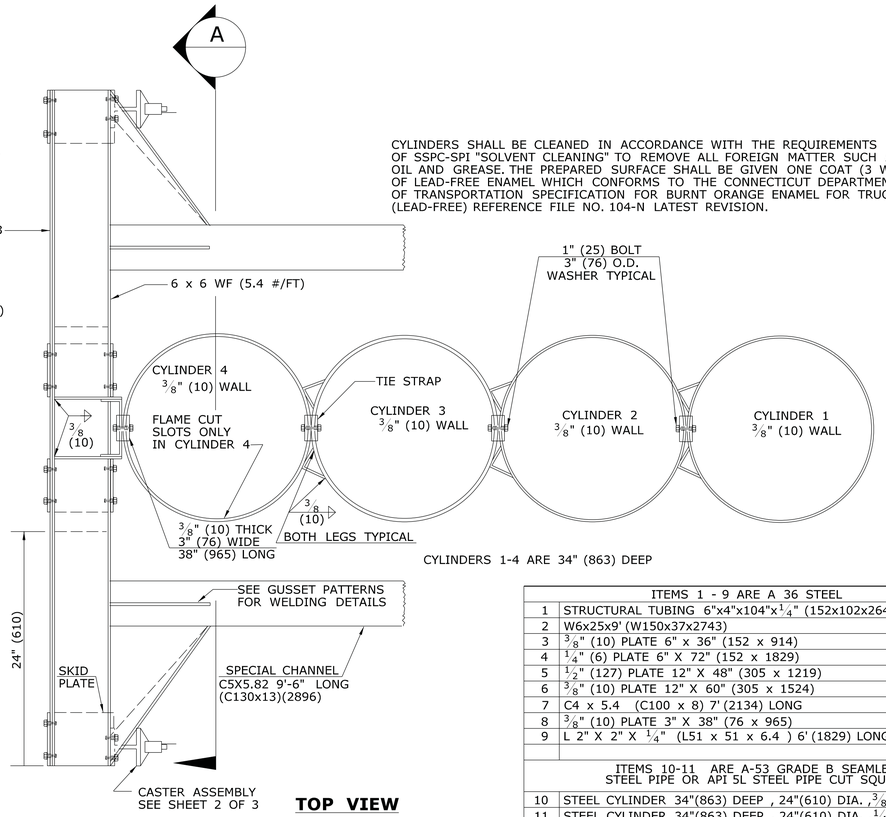
HW-949_01



FRONT VIEW

SECTION A

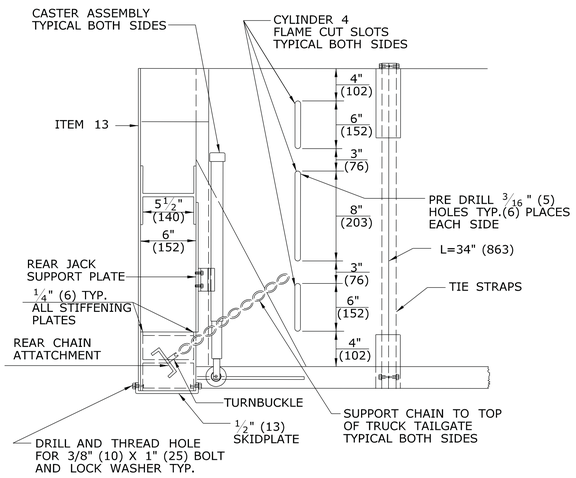
ALL COMPONENTS TO BE 6061-T6 ALLUMINUM
USE COMPATIBLE ALLOY IN WELDING MATERIAL



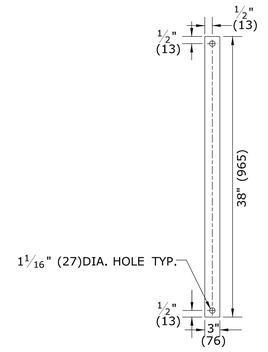
CYLINDERS SHALL BE CLEANED IN ACCORDANCE WITH THE REQUIREMENTS OF SSPC-SPI "SOLVENT CLEANING" TO REMOVE ALL FOREIGN MATTER SUCH AS OIL AND GREASE. THE PREPARED SURFACE SHALL BE GIVEN ONE COAT (3 WFT) OF LEAD-FREE ENAMEL WHICH CONFORMS TO THE CONNECTICUT DEPARTMENT OF TRANSPORTATION SPECIFICATION FOR BURNT ORANGE ENAMEL FOR TRUCKS (LEAD-FREE) REFERENCE FILE NO. 104-N LATEST REVISION.

CYLINDERS 1-4 ARE 34" (863) DEEP

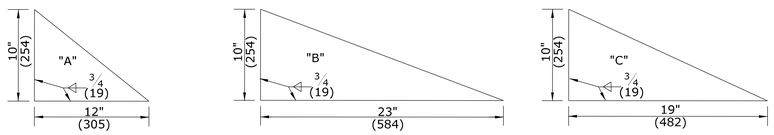
TOP VIEW



REAR ASSEMBLY SIDE VIEW



TIE STRAPS



1"(25) PLATE **1/2"(13) PLATE** **1/2"(13) PLATE**

GUSSET PATTERNS

ITEMS 1 - 9 ARE A 36 STEEL		
1	STRUCTURAL TUBING 6"x4"x104"x1/4" (152x102x2642x6)	2
2	W6x25x9' (W150x37x2743)	1
3	3/8" (10) PLATE 6" x 36" (152 x 914)	1
4	1/4" (6) PLATE 6" X 72" (152 x 1829)	4
5	1/2" (127) PLATE 12" X 48" (305 x 1219)	1
6	3/8" (10) PLATE 12" X 60" (305 x 1524)	2
7	C4 x 5.4 (C100 x 8) 7' (2134) LONG	2
8	3/8" (10) PLATE 3" X 38" (76 x 965)	10
9	L 2" X 2" X 1/4" (L51 x 51 x 6.4) 6' (1829) LONG	1
ITEMS 10-11 ARE A-53 GRADE B SEAMLESS STEEL PIPE OR API 5L STEEL PIPE CUT SQUARE		
10	STEEL CYLINDER 34"(863) DEEP , 24"(610) DIA. ,3/8"(10) THICK	1
11	STEEL CYLINDER 34"(863) DEEP , 24"(610) DIA. ,1/4"(6) THICK	3
12	25' (7620) OF 1/4" (6) CHAIN	1
ITEMS 13-19 ARE 6061 T6 OR 6061-64 ALUMINUM		
13	1/4" (6) PLATE 34" (863) x 72" (1829)	1
14	6 x 6 (152x152) WF (5.4 #/FT) - 6' (1829) LONG	2
15	SPECIAL CHANNEL-DEPTH = 5" (127), WT/FT = 5.82 WIDTH = 2.875 (73), LENGTH = 9'-6" (2896)	2
16	C 6 x 4.48 (C 152 x 7) 34" (863) LONG	1
17	1/2" (13) PLATE 48" x 48" (1219 x 1219)	1
18	1/4" (6) PLATE 4" x 108" (102 x 2743)	2
19	1" (25) PLATE 10" X 12" (254 x 305)	1
20	1 X 3 A 325 STRUCTURAL BOLT WITH NUTS	12
21	HI STR. STRUCTURAL WASHERS	12
22	1 X 4 (25x102) A 325 STRUCTURAL BOLTS WITH NUTS	2
23	3/4" x 4 (20x102) A 325 STRUCTURAL BOLTS WITH NUTS	4
24	HI STR. STRUCTURAL WASHERS	4
25	LOCK WASHERS	16
26	CASTER ASSEMBLY	4

IMPACT PLATE & CYLINDER DETAILS

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

REV.	DATE	REVISION DESCRIPTION
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 9/30/2010

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_HIGHWAY_STD.dgn Model: 75 - HW-1806_01a

SUBMITTED BY: NAME/DATE/TIME:

APPROVED BY: NAME/DATE/TIME:

CTDOT STANDARD SHEET

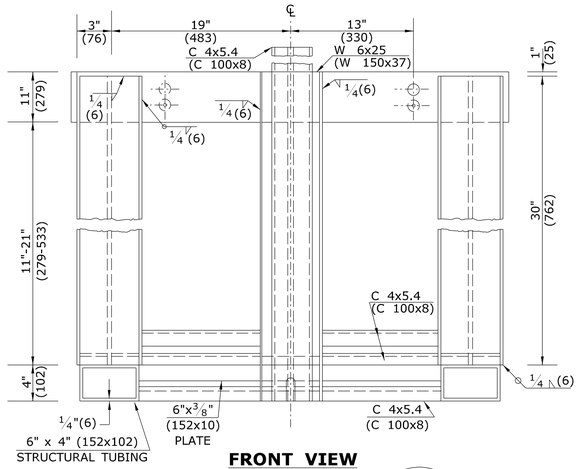
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

CT TRUCK MOUNTED IMPACT ATTENUATOR SHEET 1

STANDARD SHEET NO.:

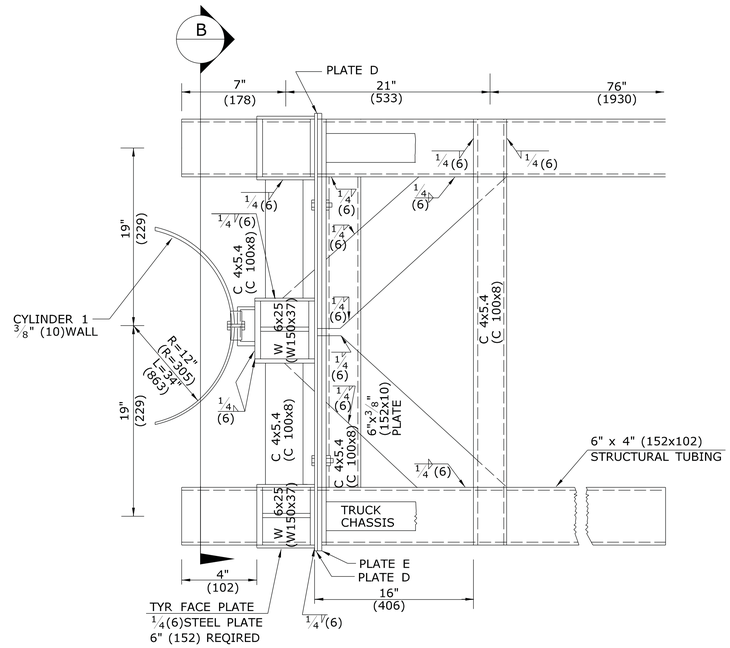
HW-1806_01a



FRONT VIEW

SECTION B

CYLINDER 1 NOT SHOWN



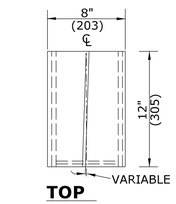
TOP VIEW

GENERAL NOTES :

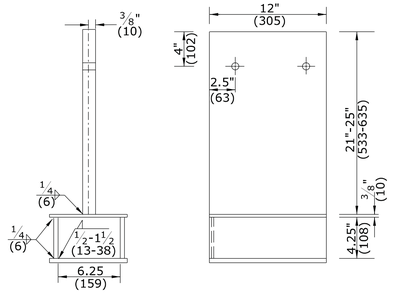
FRONT AND REAR CASTER / JACK ASSEMBLY NOTES:

1. JACK SHALL BE SIDEWIND TYPE HAVING A LIFTING CAPACITY OF 1000LBS AND THROW DISTANCE OF 18" (457)
2. SWIVEL CASTER SHALL BE STEEL
3. THE LOCATION OF THE JACK MOUNT IN RELATION TO THE GROUND SHALL BE DETERMINED BY THE JACK SIZE AND MANUFACTURER'S SPECIFICATIONS.

NOTE: POSITION AND ATTACH SHOES TO CHASSIS AS REQUIRED TO ACCEPT FORWARD END OF 6" x 4" (152x102) BOX BEAMS

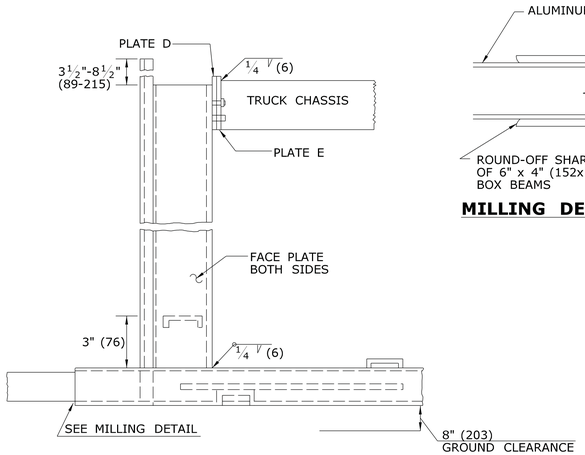


TOP



SIDE **FRONT**

SUPPORT SHOE (2 REQUIRED)
A36 STEEL ⅜" (10)



SIDE VIEW

MILLING DETAIL

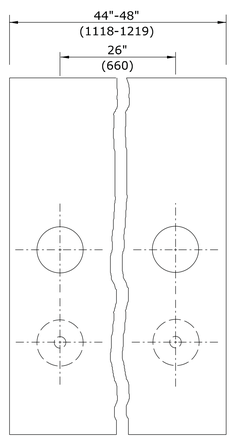


PLATE D

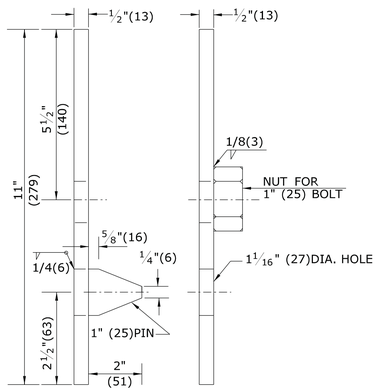
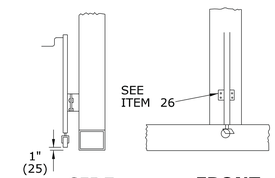
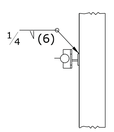


PLATE D

PLATE E



SIDE **FRONT**



TOP

ATTACHMENT TO TRUCK
ALIGNMENT PIN DETAIL

CASTER DETAIL

STEEL GUIDE FRAME DETAILS

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

REV.	DATE	REVISION DESCRIPTION
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 9/30/2010

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_HIGHWAY_STD.dgn Model: 76 - HW-1806_01b

SUBMITTED BY: NAME/DATE/TIME:

APPROVED BY: NAME/DATE/TIME:

CTDOT STANDARD SHEET

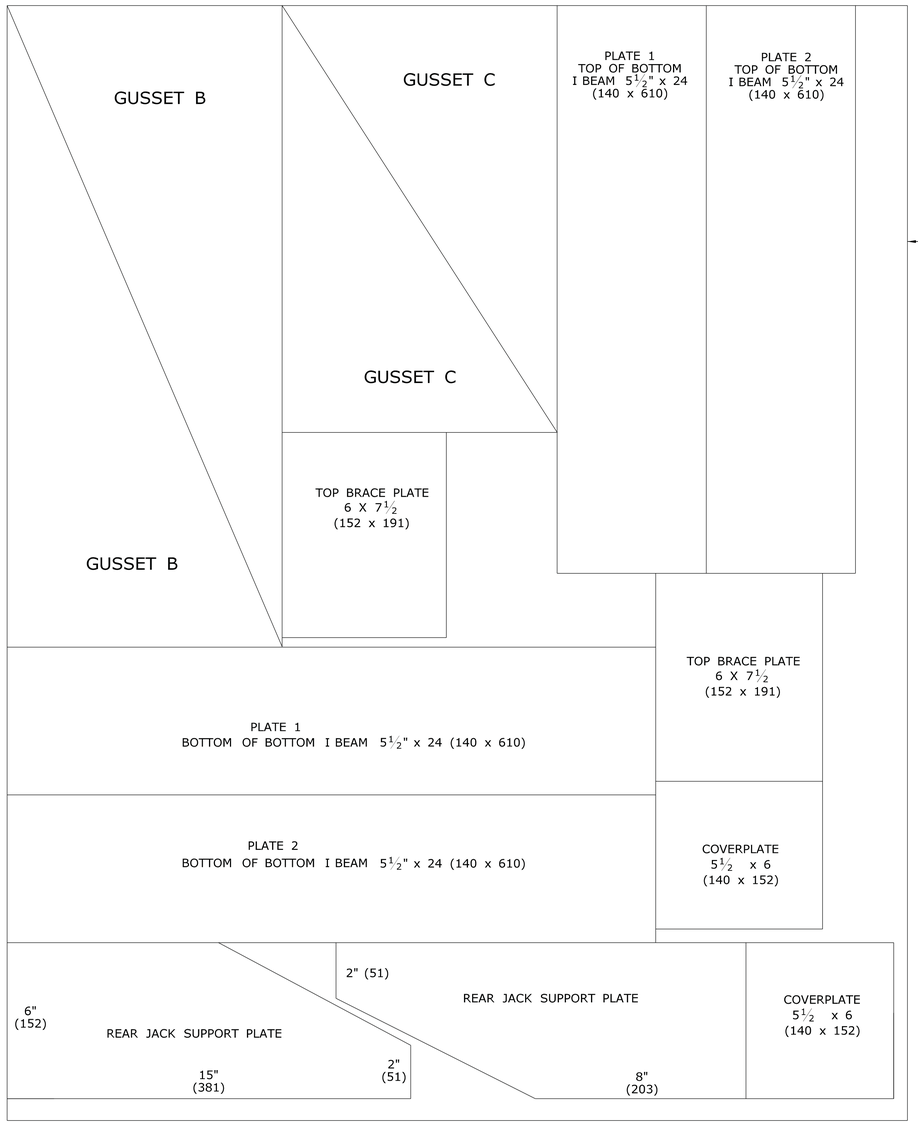
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

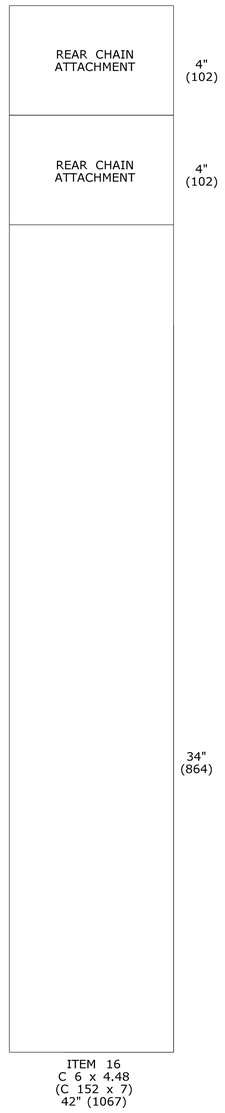
CT TRUCK MOUNTED IMPACT ATTENUATOR SHEET 2

STANDARD SHEET NO.:

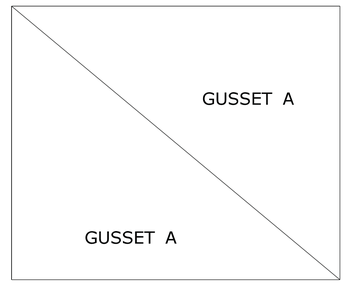
HW-1806_01b



ITEM 17
1/2" (13) ALUMINUM
PLATE 36" x 48"
(914 x 1219)



ITEM 16
C 6 x 4.48
(C 152 x 7)
42" (1067)



ITEM 19
1" (25) ALUMINUM PLATE
10" x 12" (254 x 305)

CUTTING PATTERN

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

REV.	DATE	REVISION DESCRIPTION
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-
-	-	-

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 9/30/2010

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_HIGHWAY_STD.dgn Model: 77 - HW-1806_01c

SUBMITTED BY: NAME/DATE/TIME:

APPROVED BY: NAME/DATE/TIME:

CTDOT
STANDARD SHEET

OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

CT TRUCK MOUNTED IMPACT ATTENUATOR SHEET 3

STANDARD SHEET NO.:

HW-1806_01c

ONLY STANDARD SHEETS MARKED WITH AN "✓" ARE IN THIS PROJECT #

	SHEET NO.	TITLE	APPROVAL DATE
	TR-1000_01	GENERAL CLAUSES (TEST PROCEDURES)	1/2014
✓	TR-1001_01	TRENCHING & BACKFILLING, ELECTRICAL CONDUIT	4/2012
✓	TR-1002_01	TRAFFIC CONTROL FOUNDATIONS	1/2014
✓	TR-1010_01	CONCRETE HANDHOLE	4/2014
✓	TR-1102_01	PEDESTALS, PEDESTRIAN SIGNALS	4/2012
	TR-1105_01	TRAFFIC SIGNALS AND CABLE ASSIGNMENTS	5/2013
✓	TR-1107_01	PEDESTRIAN PUSH BUTTON	4/2014
	TR-1108_01	CONTROLLERS	5/2013
	TR-1111_01	LOOP VEHICLE DETECTOR AND SAWCUT	4/2014
	TR-1113_01	CONTROL CABLE	4/2014
	TR-1114_01	POLE ANCHOR, "Y" CLAMP, SIGN HANGER	4/2014

	SHEET NO.	TITLE	APPROVAL DATE
	TR-1205_01	DELINEATION, DELINEATOR AND OBJECT MARKER DETAILS [7]	2/2011
✓	TR-1208_01	SIGN SUPPORT AND SIGN PLACEMENT DETAILS, GORE EXIT SIGN [8]	2/2011
✓	TR-1208_02	METAL SIGN POSTS AND SIGN MOUNTING DETAILS [9]	2/2011
	TR-1210_01	PAVEMENT MARKINGS (DURABLE MARKINGS) FOR DIVIDED HIGHWAYS [21A]	12/2013
	TR-1210_02	PAVEMENT MARKINGS (DURABLE MARKINGS) FOR DIVIDED HIGHWAYS [22A]	12/2013
✓	TR-1210_03	SPECIAL DETAILS & TYPICAL PAVEMENT MARKINGS FOR TWO-WAY HIGHWAYS [25]	11/2014
✓	TR-1220_01	SIGNS FOR CONSTRUCTION AND PERMIT OPERATIONS [23]	6/2012
✓	TR-1220_02	CONSTRUCTION SIGN SUPPORTS AND CHANNELIZING DEVICES [23A]	2/2011

STANDARD SHEETS SHALL BE USED WITH STANDARD SPECIFICATIONS

REV.	DATE	REVISION DESCRIPTION
3	4-2014	REMOVED TR-1111_02.
2	1-2014	REMOVED TR-1103_01.
1	4-2012	RENUMBERED TR-1107_02 TO TR-1114_01. REMOVED TR-1116_01.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 4/11/2014

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_TRAFFIC_STD.DGN Model: TR-01-STD_INDEX

SUBMITTED BY: NAME/DATE/TIME:

CTDOT
STANDARD SHEET

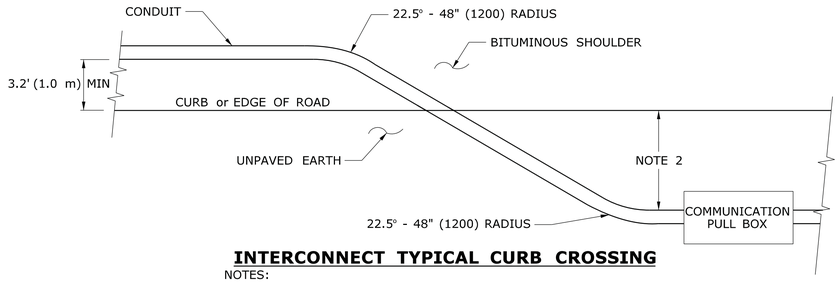
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

TRAFFIC
STANDARD SHEET INDEX

STANDARD SHEET NO.:

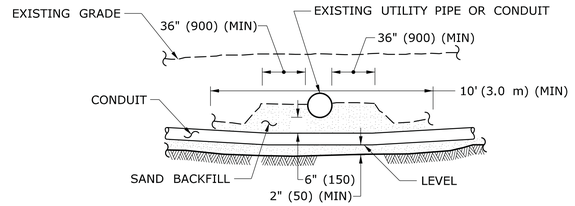
TR-STD_INDEX



INTERCONNECT TYPICAL CURB CROSSING

NOTES:

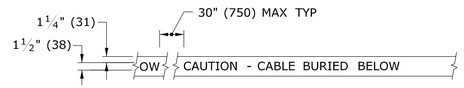
1. RESTORE AREAS DISTURBED BY TRENCH TO ORIGINAL CONDITION.
2. INSTALL PULL BOX A MINIMUM OF 10' (3.0 m) FROM CURB UNLESS OTHERWISE SHOWN ON PLANS OR DIRECTED BY ENGINEER.



CROSSING UNDER EXISTING UTILITY

NOTES:

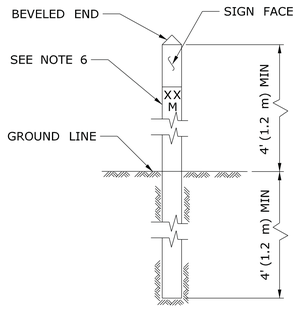
1. WHEN ENCOUNTERED AT APPROXIMATELY THE SAME DEPTH, CROSS BENEATH.
2. PROTECT & SUPPORT EXPOSED EXISTING UTILITY.



DETECTABLE WARNING TAPE

NOTE:
STANDARD SPECIFICATIONS, ARTICLE: 1.05.15

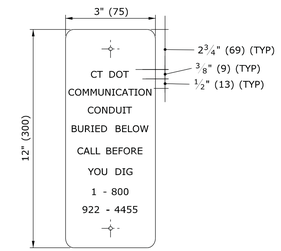
1. TAPE COLORS:
   COMMUNICATION - ORANGE BACKGROUND / BLACK LEGEND
   POWER - RED BACKGROUND / BLACK LEGEND



INTERCONNECT CONDUIT IDENTIFICATION POST

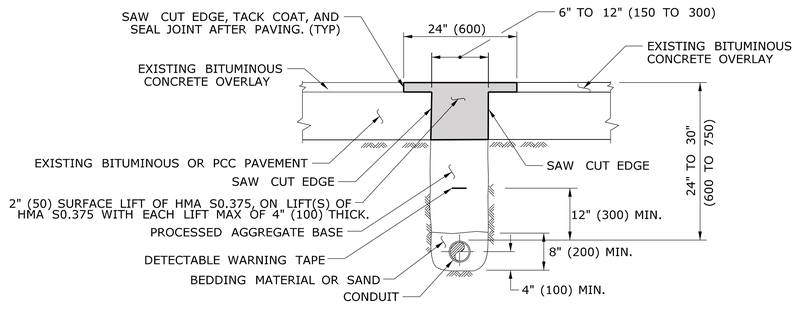
NOTES:

1. 4" x 4" (100 x 100) NOMINAL, PRESSURE TREATED WOOD POST.
2. ATTACH SIGN TO POST WITH ¼" x 1¼" (6 x 31) STAINLESS STEEL LAG SCREW WITH NYLON WASHER ON FACE OF SIGN.
3. SIGN COLORS: BACKGROUND - ORANGE (RETROREFLECTIVE) LEGEND - BLACK (OPAQUE).
4. INSTALL POST APPROX 24" (600) FROM RMC IN VICINITY OF EACH PULL BOX.
5. INSTALL POSTS BETWEEN PULL BOXES, APPROX 10' (3.0 m) OFF CURB. SPACE POSTS 1500'± (460 m±) APART.
6. PERMANENTLY ATTACH STAINLESS STEEL NUMBERS INDICATING DISTANCE TO TRENCH IN FEET (METERS) CONTAINING COMMUNICATION CABLE. ATTACH NUMBERS TO SIDE OF POST FACING CONDUIT. INCLUDE "M" SUFFIX IF METERS.



SIGN FACE DETAIL

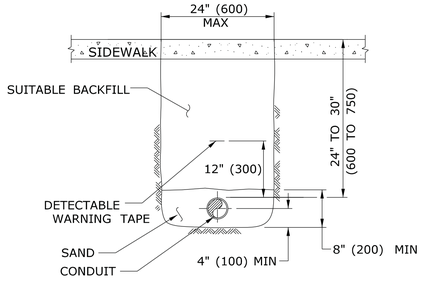
SIGN # 41-4669



PAVEMENT - BITUMINOUS CONCRETE OR OVERLAYED PORTLAND CEMENT CONCRETE

NOTES:
STANDARD SPECIFICATIONS, ARTICLE: 3.04 & 4.06.03

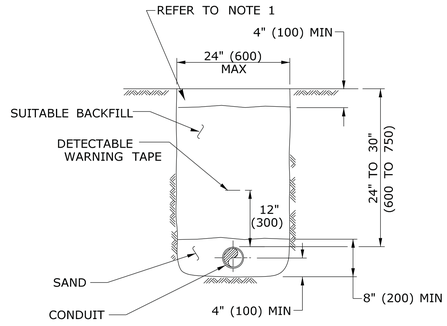
1. TOTAL HOT MIX ASPHALT (HMA) THICKNESS TO MATCH EXISTING BITUMINOUS CONCRETE AND PORTLAND CEMENT CONCRETE (PCC) THICKNESS.
2. WHEN ALLOWED BY ENGINEER, USE CONTROLLED LOW STRENGTH MATERIAL (CLSM) AS BEDDING MATERIAL. TOP OF CLSM AT LEAST 20" (500) BELOW SURFACE.



SIDEWALK

NOTES:
STANDARD SPECIFICATIONS, ARTICLE: 9.21 & 9.22

1. WHERE CONCRETE SIDEWALK DAMAGED OR CUT, REPLACE THE ENTIRE SECTION BETWEEN JOINTS. REPLACEMENT SIDEWALK IS PAID FOR AT THE CONTRACT UNIT PRICE FOR "CONCRETE SIDEWALK".



EARTH

NOTES:
STANDARD SPECIFICATIONS, ARTICLE: 9.50

1. IN MOWED AREAS: PLACE TOPSOIL, FERTILIZER, SEED, & MULCH.

GENERAL NOTES:

1. TOP OF CONDUIT NO LESS THAN 24" (600) DEEP.
2. COMPACT BACKFILL IN ≤ 6" (150) LIFTS. HAND COMPACTION NOT PERMITTED.

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:
-- RMC (RIGID METAL CONDUIT)

REV.	DATE	REVISION DESCRIPTION
1	4-2012	REVISED BITUMINOUS CONRCETE TO HMA, & MINOR REVISIONS.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 4/14/2012

DIMENSIONS ARE IN ENGLISH (') & METRIC UNITS (mm). METRIC DIMENSIONS ARE ROUNDED:
- OVER 1" TO NEAREST 5 mm
- UNDER 1" TO NEAREST 1 mm.

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_TRAFFIC_STD.dgn Model: TR-1001_01

SUBMITTED BY: NAME/DATE/TIME:

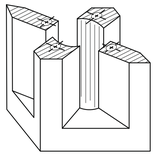
APPROVED BY: NAME/DATE/TIME:

CTDOT STANDARD SHEET

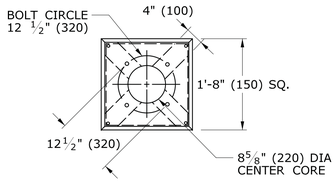
OFFICE OF ENGINEERING

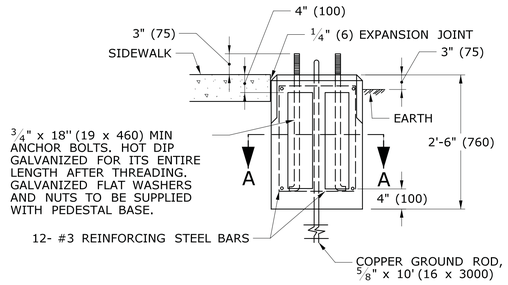
STANDARD SHEET TITLE:
TRENCHING & BACKFILLING, ELECTRICAL CONDUIT

STANDARD SHEET NO.:
TR-1001_01



PICTORIAL
SECTION A-A

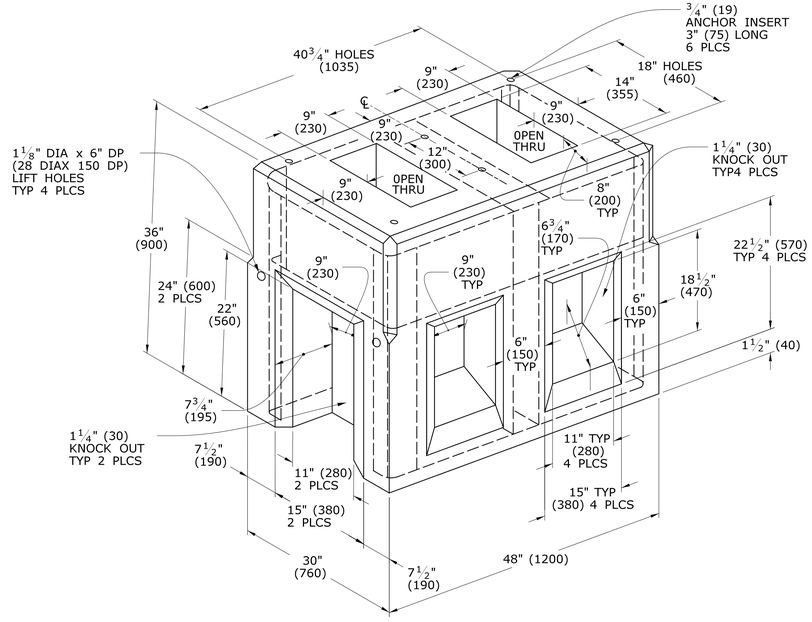




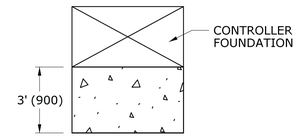
TRAFFIC CONTROL FOUNDATION
PEDESTAL - TYPE I - PRECAST

NOTES:

PLACE NO. 6 CRUSHED STONE IN CENTER OPENING AFTER CONDUITS AND GROUND ROD HAVE BEEN INSTALLED.



TRAFFIC CONTROL FOUNDATION
CONTROLLER - TYPE IV - PRECAST

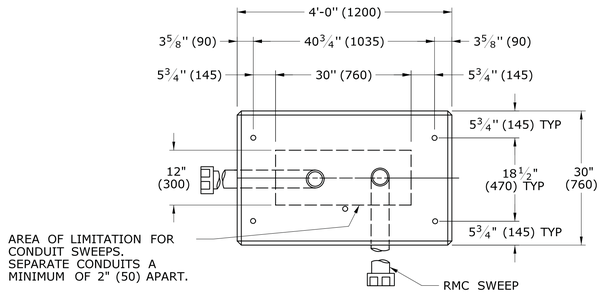


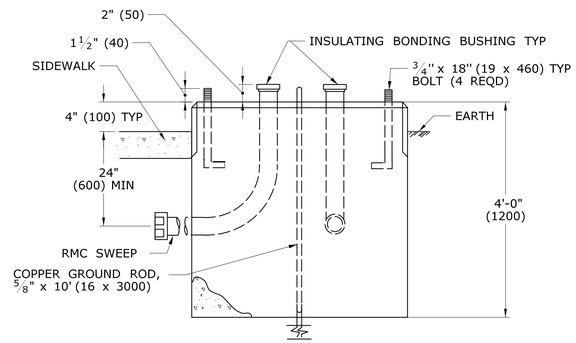
INSTALL PRECAST OR CAST IN PLACE CONCRETE SIDEWALK ON CABINET DOOR SIDE OF CONTROLLER FOUNDATION.

PITCH SIDEWALK ¼" PER FOOT (20 PER METER) AWAY FROM THE CONTROLLER FOUNDATION.

REFER TO HIGHWAY STANDARD SHEET HW-921_01 FOR SIDEWALK CONSTRUCTION.

TYPICAL CONCRETE SIDEWALK
AT CONTROLLER FOUNDATION





TRAFFIC CONTROL FOUNDATION
CONTROLLER - TYPE IV - CAST IN PLACE

NOTES:

INSTALL FOUNDATION ON 6" (150) OF COMPACTED GRAVEL IN ACCORDANCE WITH SECTION 2.14.
LEVEL FOUNDATION WITH A PROJECTION OF 4" (100) ABOVE FINISHED GRADE.
INSTALL COPPER GROUND ROD: ⅝" x 10 (16 x 3000).
PLACE NO. 6 CRUSHED STONE IN THE CENTER OPENINGS AFTER THE CONDUITS AND GROUND ROD HAVE BEEN INSTALLED. THE OPENINGS SHALL BE CAPPED WITH A 2" (50) GROUT LEVEL WITH THE TOP OF THE FOUNDATION AND NEATLY FINISHED. THE GROUT SHALL CONFORM WITH THE REQUIREMENTS OF ARTICLE M.3.01-12.
CONCRETE: CLASS "A" CONFORMING TO ARTICLE M.03.01.
#4 REBAR 2" (50) MIN COVER AROUND ALL OPENINGS, 3-#4 REBARS IN EACH CORNER.
CONDUITS SHALL NOT PROJECT MORE THAN 2" (50) ABOVE FOUNDATION.

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:
- PROPOSED CONTROLLER
- EXISTING CONTROLLER
- PROPOSED STEEL SPAN POLE
- EXISTING STEEL SPAN POLE

REV.	DATE	REVISION DESCRIPTION
2	1-2014	REMOVED SPAN POLE FOUNDATION DETAILS, REVISED TYPICAL CONCRETE SIDEWALK AT CONTROLLER FOUNDATION.
1	4-2012	MINOR REVISIONS.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 1/7/2014

DIMENSIONS ARE IN ENGLISH (.") & METRIC UNITS (mm).
METRIC DIMENSIONS ARE ROUNDED:
- OVER 1" TO NEAREST 5 mm
- UNDER 1" TO NEAREST 1 mm.

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_TRAFFIC_STD.DGN Model: TR-1002_01

SUBMITTED BY: NAME/DATE/TIME:

APPROVED BY: NAME/DATE/TIME:

CTDOT
STANDARD SHEET

OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

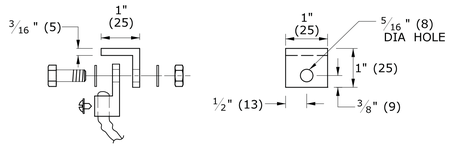
TRAFFIC CONTROL FOUNDATIONS

STANDARD SHEET NO.:

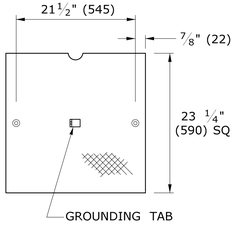
TR-1002_01

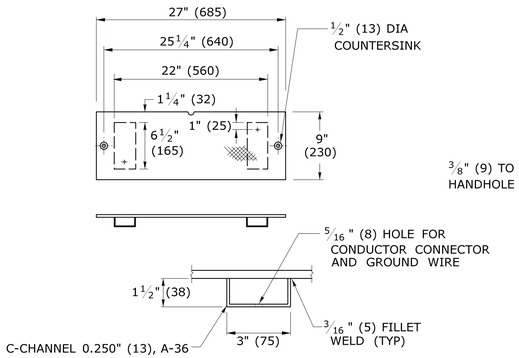
COVER NOTES:

1. GROUNDING TAB WELDED TO BOTTOM CENTER OF COVER WITH 3/16" (5) WELD (3 SIDES).
2. ATTACH 6' (2 m) LENGTH OF NO. 8 GROUND WIRE TO GROUNDING TAB WITH CONDUCTOR CONNECTOR, 1/4" - 20 X 3/4" (M6 X 20) LG SST HEX HEAD BOLT, AND SST FLAT WASHER. ATTACH FREE END OF GROUND WIRE TO CONDUIT BONDING BUSHING IN HANDHOLE.
3. CONDUCTOR CONNECTOR: COPPER ALLOY BODY, BRASS SCREW, BRASS OR COPPER ALLOY PRESSURE PLATE.
4. COVER SCREW INSERT: 3/8"-16 (9-16), 1 1/2"L (37L), STAINLESS STEEL.
5. COVER SCREW: 3/8"-16 (9-16), 1"L (25L), FLAT HEAD, SLOTTED, STAINLESS STEEL.

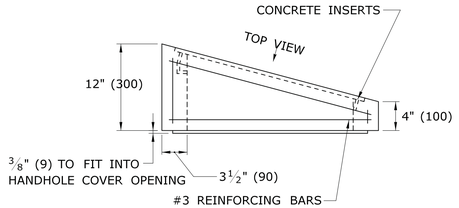


STEEL GROUNDING TAB w/ CONDUCTOR CONNECTOR

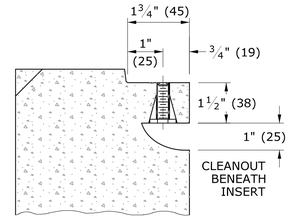




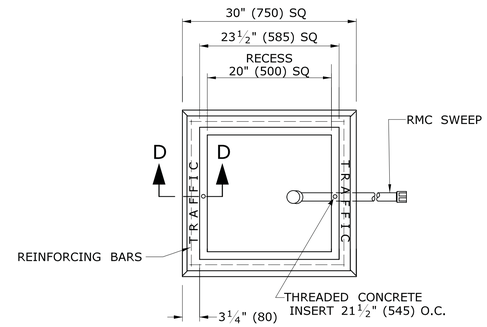
NON SKID FLOOR PLATE GALVANIZED STEEL, 3/8" (10)



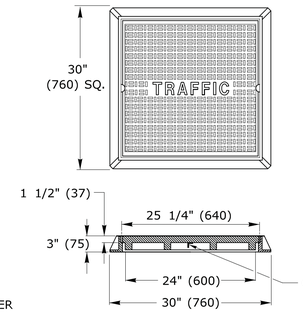
BANK ADAPTER

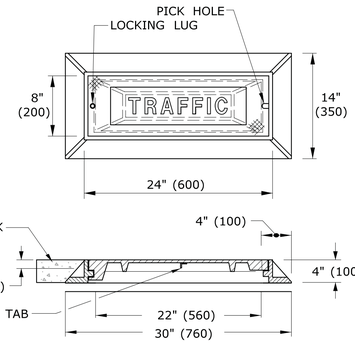


SECTION D-D

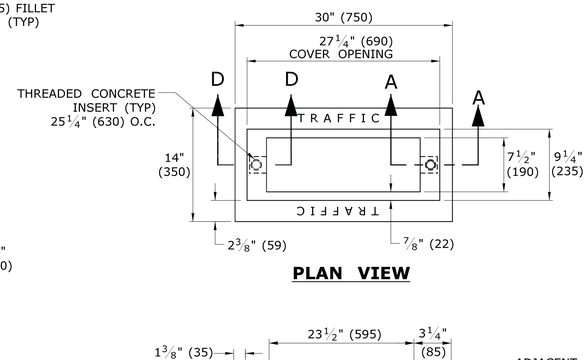


PLAN VIEW

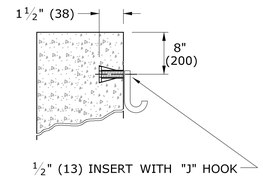




CAST IRON HANDHOLE COVERS

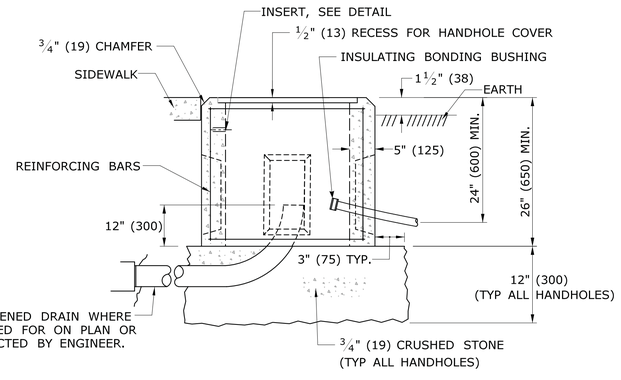


PLAN VIEW

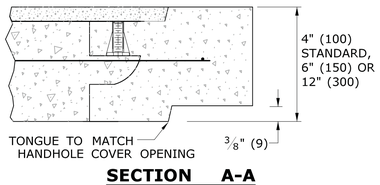


INSERT DETAIL

TYP IN TWO PLACES FOR ALL HANDHOLES

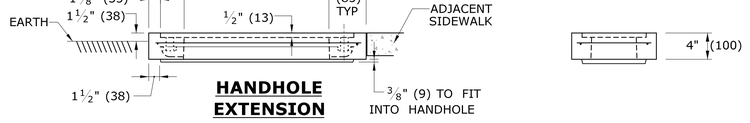


CONCRETE HANDHOLE TYPE I

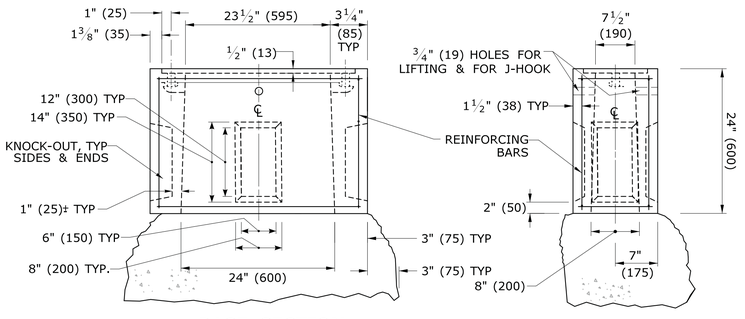


SECTION A-A
HANDHOLE EXTENSIONS

4 - #8 REINFORCING BARS REQ'D



HANDHOLE EXTENSION



BASE SECTION
CONCRETE HANDHOLE TYPE II

HANDHOLE NOTES:

1. MINIMUM CLASS "C" CONCRETE.
2. COMPLETE TYPE II HANDHOLE:
   IN EARTH AREAS, CONSISTS OF A BASE SECTION WITH 4" (100) HANDHOLE EXTENSION,
   IN SIDEWALK AREAS, CONSISTS OF A BASE SECTION WITH 4" (100) CAST IRON COVER.
3. PLAN VIEW DIMENSIONS, SECTION VIEW, & DETAILS, SAME FOR BASE SECTION, EXTENSIONS & BANK ADAPTER.
4. GROUT AROUND ALL CONDUITS.
5. INSTALL 30" (750) SIDE PARALLEL TO ROAD UNLESS OTHERWISE NOTED.
6. INSTALL HANDHOLES APPROX. 12" (300) BEHIND CURB OR IF NO CURB, 24" (600) BEHIND EDGE OF ROAD UNLESS OTHERWISE SPECIFIED.
7. CAST THE WORD "TRAFFIC" INTO TOP EDGE OF HANDHOLE, 1 1/2" (38) LETTERS.
8. WHERE AN EXISTING CONCRETE SIDEWALK SLAB ABUTTING A HANDHOLE IS DAMAGED OR CUT DURING INSTALLATION, REPLACE THE ENTIRE SIDEWALK SECTION.
9. 12-#3 REINFORCING BARS REQUIRED FOR ALL HANDHOLES. (8 HORIZONTAL, 4 VERTICAL)

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:

- PROPOSED HANDHOLE
- EXISTING HANDHOLE

REV.	DATE	REVISION DESCRIPTION
2	4-2014	REVISED HANDHOLES NOTES, ADDED NOTE #6. ADDED "J" HOOK TO INSERT DETAIL.
1	4-2012	CAST IRON COVER: CHANGED BOLT TO PICK HOLE. ADDED EXTENSIONS, C-CHANNEL, CONDUCTOR CONNECTOR & MINOR REVISIONS.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 4/11/2014

DIMENSIONS ARE IN ENGLISH (".") & METRIC UNITS (mm). METRIC DIMENSIONS ARE ROUNDED:
- OVER 1" TO NEAREST 5 mm
- UNDER 1" TO NEAREST 1 mm.

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_TRAFFIC_STD.DGN Model: TR-1010_01

SUBMITTED BY: NAME/DATE/TIME:

APPROVED BY: NAME/DATE/TIME:

CTDOT STANDARD SHEET

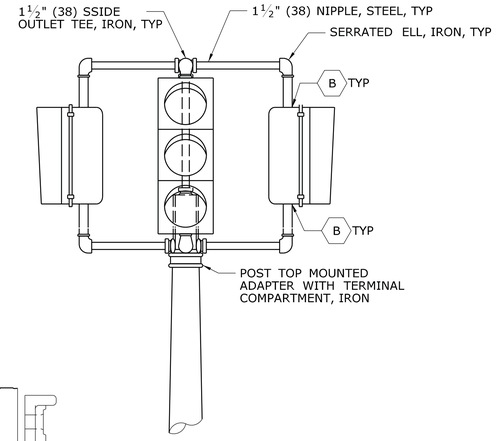
OFFICE OF ENGINEERING

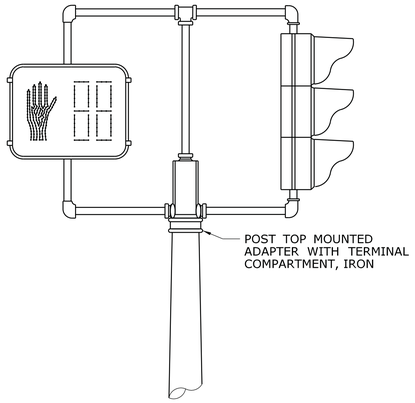
STANDARD SHEET TITLE:

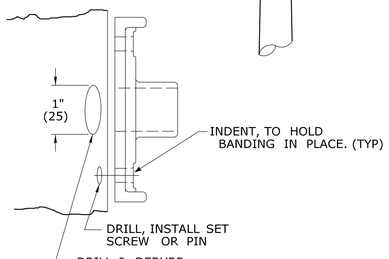
CONCRETE HANDHOLE

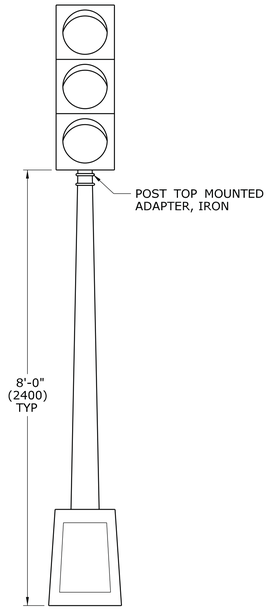
STANDARD SHEET NO.:

TR-1010_01

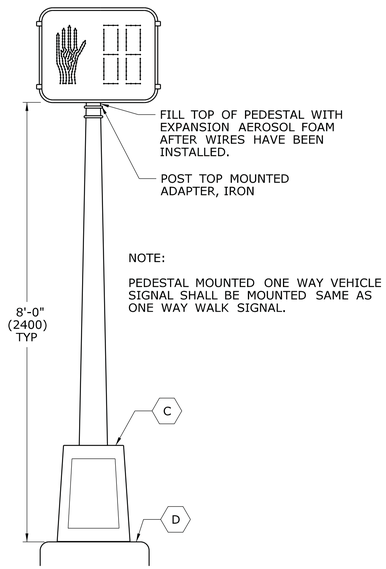




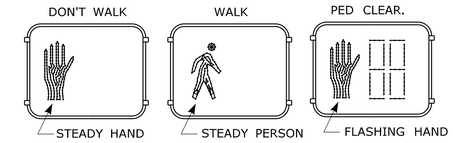




ONE WAY TRAFFIC SIGNAL PEDESTAL MOUNTED

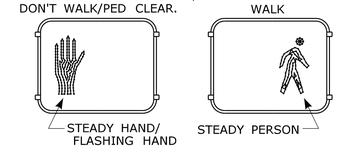


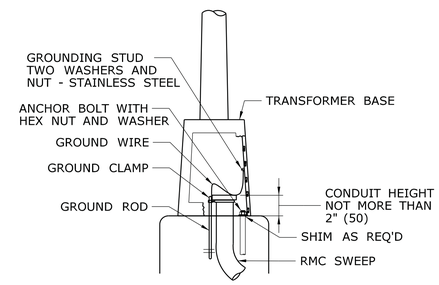
ONE WAY WALK SIGNAL PEDESTAL MOUNTED



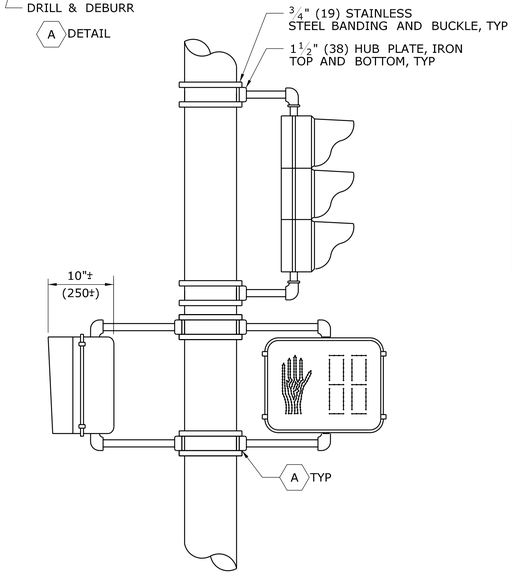
TYPICAL INDICATION WHEN LIT

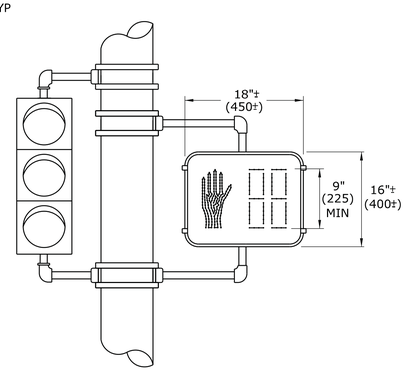
NON-COUNTDOWN DISPLAY, ONLY WHEN SHOWN ON PLAN.

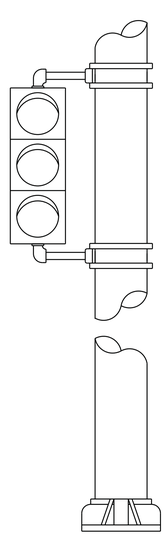




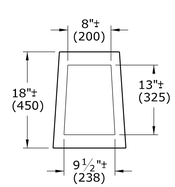
ALUMINUM PEDESTAL INSTALLATION DETAIL



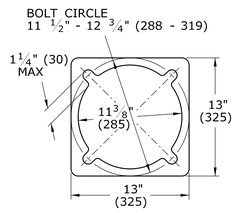




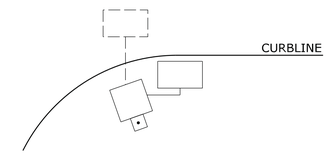
ONE WAY TRAFFIC SIGNAL POLE MOUNTED



ALUMINUM PEDESTAL DOOR OPENING DETAIL



PEDESTAL BASE PLAN



WHEN PEDESTALS OR SPAN POLES ARE INSTALLED CLOSE TO THE CURB, SIDE MOUNT PEDESTRIAN OR TRAFFIC SIGNALS TO AVOID VISOR DAMAGE FROM TURNING VEHICLES.

NOTES:

A SECURE LOWER HUB PLATE WITH STAINLESS STEEL SET SCREW OR PIN PRIOR TO BANDING TO PREVENT MOVEMENT. INSTALL CABLE THROUGH BOTTOM OF HUB PLATE.

B REFER TO CTDOT TRAFFIC STANDARD SHEET, TR-1105_01, TRAFFIC SIGNALS & CABLE ASSIGNMENTS.

C IF THREADED, MIN 1" (25) THREADED INTO BASE, SECURED WITH STAINLESS STEEL SET SCREWS.

D BASE DESIGNED AS BREAK-AWAY.

INCANDESCENT WALK SIGNAL LAMPS ARE 67 WATTS, RATED AT 8000 HOURS LAMP LIFE. LED WALK SIGNAL LAMPS ARE MAXIMUM 15 WATTS, WARRANTEED AT 5 YEAR LIFE.

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:
- STEEL SPAN POLE, MAST ARM ASSEMBLY SHAFT
- ALUMINM PEDESTAL
- TRAFFIC SIGNAL
- PEDESTRIAN SIGNAL
- PEDESTAL MOUNTED, TRAFFIC & PEDESTRIAN SIGNALS
- POLE MOUNTED, TRAFFIC & PEDESTRIAN SIGNALS

REV.	DATE	REVISION DESCRIPTION
2	4-2012	MINOR REVISIONS.
1	1-2010	INCLUDED COUNTDOWN PEDESTRIAN SIGNALS.

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Plotted Date: 4/14/2012

DIMENSIONS ARE IN ENGLISH (' ") & METRIC UNITS (mm).
METRIC DIMENSIONS ARE ROUNDED:
- OVER 1" TO NEAREST 5 mm
- UNDER 1" TO NEAREST 1 mm.

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_TRAFFIC_STD.dgn Model: TR-1102_01

SUBMITTED BY: NAME/DATE/TIME:

APPROVED BY: NAME/DATE/TIME:

CTDOT STANDARD SHEET

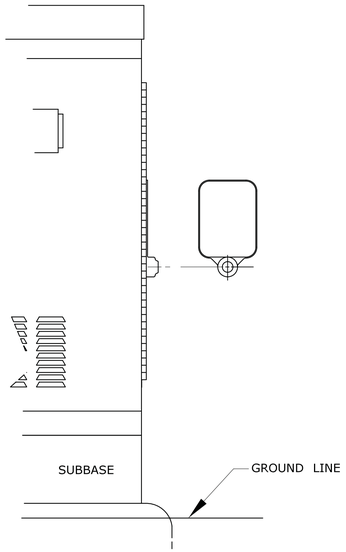
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

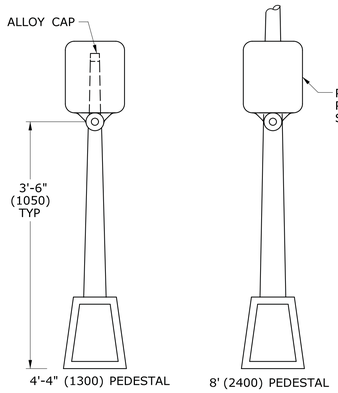
PEDESTALS, PEDESTRIAN SIGNALS

STANDARD SHEET NO.:

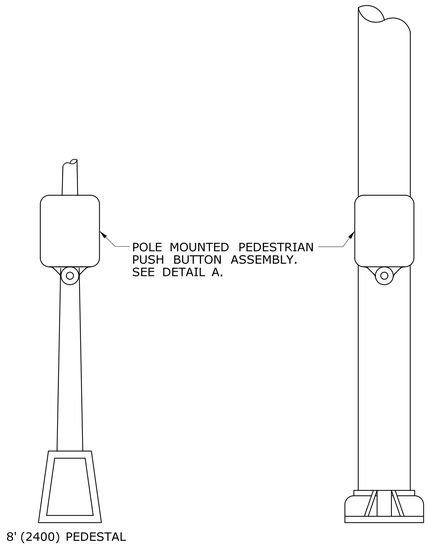
TR-1102_01



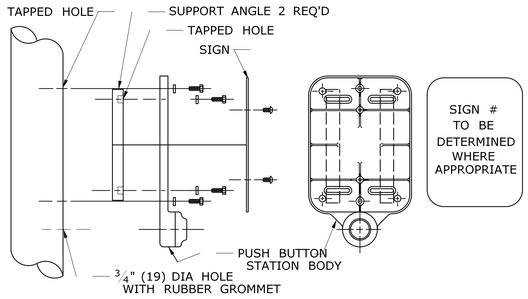
SURFACE MOUNTED



PEDESTAL MOUNTED



SPAN POLE/MAST ARM MOUNTED



DETAIL A

SIGN # 31-0833

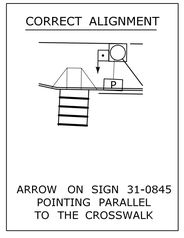
* USE APPROPRIATE ARROW UNLESS OTHERWISE NOTED ON PLAN.

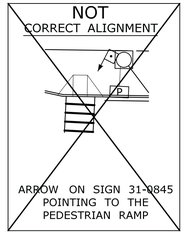
SIGN # 31-0835

FOR CROSSING WITH SIDE STREET GREEN

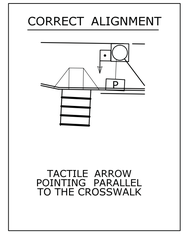
GENERAL NOTES:

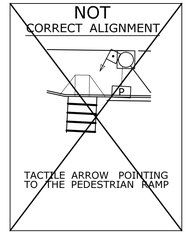
3'-6" (1050) FROM FINISHED GRADE SUCH AS SIDEWALK TO CENTER OF PUSH BUTTON.
PUSH BUTTON INSTALLATIONS SHALL CONFORM TO THE REQUIREMENTS OF THE AMERICANS WITH DISABILITIES ACT (ADA) STANDARDS FOR ACCESSIBLE DESIGN, CURRENT EDITION GOVERNS.
4'-4" (1300) PEDESTAL TO INCLUDE ALLOY CAP SECURED WITH STAINLESS STEEL SET SCREW.



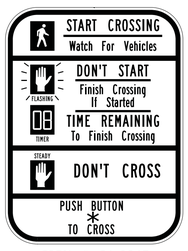


PEDESTRIAN PUSH BUTTON ALIGNMENT





ACCESSIBLE PEDESTRIAN SIGNAL AND DETECTOR



SIGN # 31-0845

* USE APPROPRIATE ARROW UNLESS OTHERWISE NOTED ON PLAN.

EXAMPLE ALIGNMENTS FOR EXCLUSIVE PEDESTRIAN PHASE

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:
- PEDESTRIAN PUSH BUTTON
- PEDESTRIAN PUSH BUTTON, PEDESTAL MOUNTED
- PEDESTRIAN PUSH BUTTON, POLE MOUNTED

REV.	DATE	REVISION DESCRIPTION
2	4-2014	ADDED PEDESTRIAN EXAMPLE ALIGNMENTS
1	4-2012	MINOR REVISIONS & UPDATED SIGN #31-0845.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 4/25/2014

DIMENSIONS ARE IN ENGLISH (".") & METRIC UNITS (mm). METRIC DIMENSIONS ARE ROUNDED:
- OVER 1" TO NEAREST 5 mm
- UNDER 1" TO NEAREST 1 mm.

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_TRAFFIC_STD.DGN Model: TR-1107_01

SUBMITTED BY: NAME/DATE/TIME:

APPROVED BY: NAME/DATE/TIME:

CTDOT STANDARD SHEET

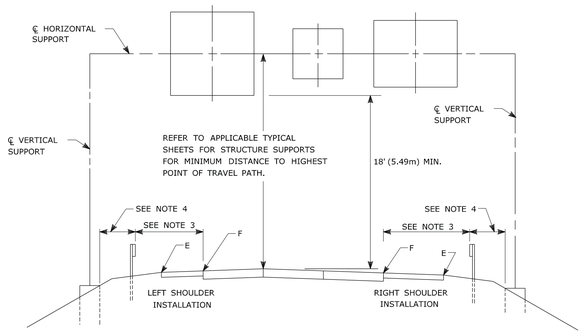
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

PEDESTRIAN PUSH BUTTONS

STANDARD SHEET NO.:

TR-1107_01



GUIDE RAIL PLACEMENT FOR SIGN SUPPORTS

NOTES:

1) FOR PLACEMENT OF CANTILEVER SIGN SUPPORT USE APPLICABLE PORTION OF ABOVE DETAIL.
2) BARRIER SYSTEM IS REQUIRED FOR BOTH SIDES OF MEDIAN SUPPORTS IN NARROW MEDIANS.
3) AT LOCATIONS WHERE IMPACT PROTECTION IS NOT REQUIRED FOR ROADSIDE ELEMENTS, FACE OF GUIDE RAIL SHALL BE PLACED 30' (9.1m) FROM EDGE OF TRAVELWAY.
4) OFFSETS OF FOUNDATIONS FROM BARRIER SYSTEMS SHALL BE AS SHOWN ELSEWHERE ON THE CONTRACT PLANS.
5) ALL SIGNS ARE TO BE HORIZONTAL, REGARDLESS OF CAMBER IN SUPPORT.

FOR MAXIMUM EFFECTIVENESS AND TO ELIMINATE OR MINIMIZE GLARE, POSITION SIDE MOUNTED SIGNS ON STRUCTURAL STEEL BREAKAWAY SIGN SUPPORTS AS FOLLOWS:

ON A TANGENT SECTION, POSITION THE SIGN SO THE VERTICAL AXIS IS PLUMB AND THE HORIZONTAL AXIS IS AT AN ANGLE OF 93° WITH THE TRAFFIC LANE WHICH THE SIGN SERVES:

DIAGRAM "A"

ON A HORIZONTAL CURVE SECTION, POSITION THE SIGN SO THE VERTICAL AXIS IS PLUMB AND THE HORIZONTAL AXIS IS AT AN ANGLE OF 90° WITH A STRAIGHT LINE BETWEEN THE SIGN AND THE POINT AT WHICH THE SIGN SHALL BE READ.

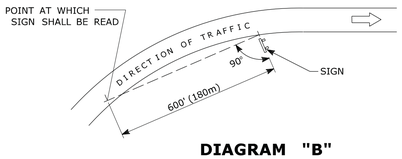
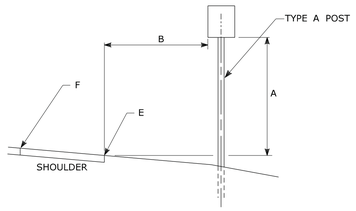
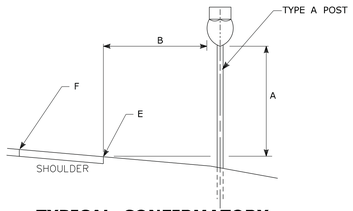


DIAGRAM "B"

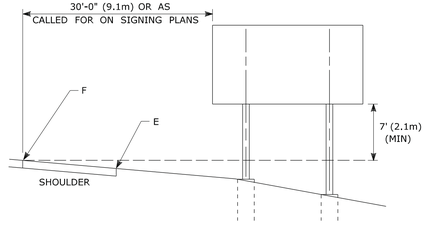
SIGN ORIENTATION DETAILS



TYPICAL REGULATORY & WARNING SIGN PLACEMENT



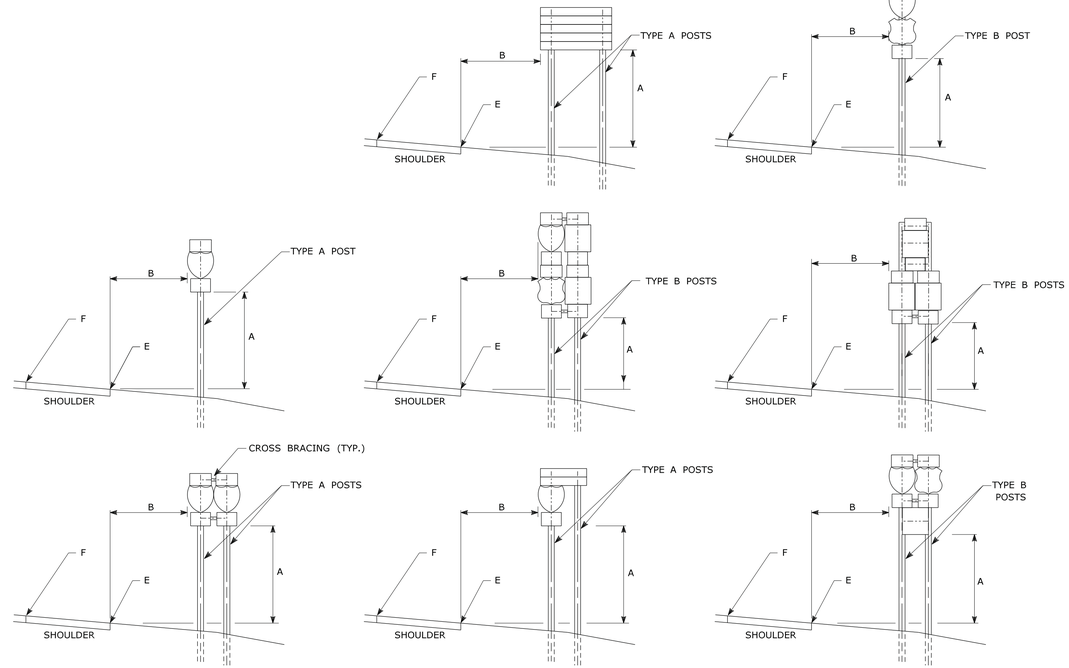
TYPICAL CONFIRMATORY ROUTE MARKER PLACEMENT



TYPICAL PLACEMENT OF SIDE MOUNTED SIGNS ON STRUCTURAL STEEL BREAKAWAY SIGN SUPPORTS

NOTES:

1) MIN. VERTICAL CLEARANCE ABOVE SIDEWALKS SHALL BE 8'-6" (2.6m).
2) WHERE GUIDE RAIL IS USED, THE OFFSET TO THE NEAR EDGE OF SIGN FACE SHALL BE AS SHOWN ELSEWHERE IN THE CONTRACT PLANS.
3) ON INTERSECTING ROADS AT RAMP TERMINI, THE OFFSET TO THE NEAR EDGE OF OF SIGN FACE SHALL BE 6' (1.8m) MIN. FROM POINT "E".
4) IF 30'-0" (9.1m) MIN. CANNOT BE MET, PLEASE CONTACT THE ENGINEER.



TYPICAL SIGN PLACEMENT AND POST SELECTION

NOTES:

ALL SIGNS AND SHIELDS ON DIRECTIONAL ASSEMBLIES SHALL ABUT VERTICALLY

2 POST ASSEMBLIES SHALL BE PROVIDED WITH 3" X ¼" (75 X 6) GALVANIZED STEEL BAR CROSS BRACING.

REFER TO TRAFFIC TYPICAL SHEET "TYPICAL METAL SIGN POSTS AND SIGN MOUNTING DETAILS" FOR SIGN POSTS.

DIM."A"	DIM."B" △1	ASSEMBLY LOCATION
7' (2.1m)	6' (1.8m) 12' (3.6m) △2	RURAL DISTRICTS & EXPRESSWAYS
7' (2.1m)	2' (0.6m)	BUSINESS & RESIDENTIAL DISTRICTS WHERE PARKING OR OTHER OBSTRUCTIONS LIMIT VISIBILITY
8'-6" (2.6m)	1' (0.3m)	SIDEWALKS △3

△1 OR AS DIRECTED BY THE ENGINEER

△2 6' FROM EDGE OF SHOULDER, WHEN SHOULDER IS OVER 6' WIDE
12' FROM EDGE OF TRAVELWAY, WHEN SHOULDER IS LESS THAN 6' WIDE.

△3 A CLEAR PATH OF NOT LESS THAN 3 FT (0.9m) SHALL BE PROVIDED IN SIDEWALK AREAS.

"E" DENOTES EDGE OF SHOULDER OR FACE OF CURB

"F" DENOTES EDGE OF TRAVELWAY

REV.	DATE	REVISION DESCRIPTION
1	2-2011	MINOR REVISIONS.

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Plotted Date: 2/16/2011

DIMENSIONS ARE IN ENGLISH (.") & METRIC UNITS (mm).
METRIC DIMENSIONS ARE ROUNDED:
- OVER 1" TO NEAREST 5 mm
- UNDER 1" TO NEAREST 1 mm.

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_TRAFFIC_STD.dgn Model: TR-1208_01

SUBMITTED BY:	NAME/DATE/TIME:
Charles S. Harlow	Charles S. Harlow 2011.02.22 11:09:17 -05'00'
APPROVED BY:	NAME/DATE/TIME:
John F. Carey	John F. Carey 2011.03.02 09:19:01 -05'00'

CTDOT STANDARD SHEET

OFFICE OF ENGINEERING

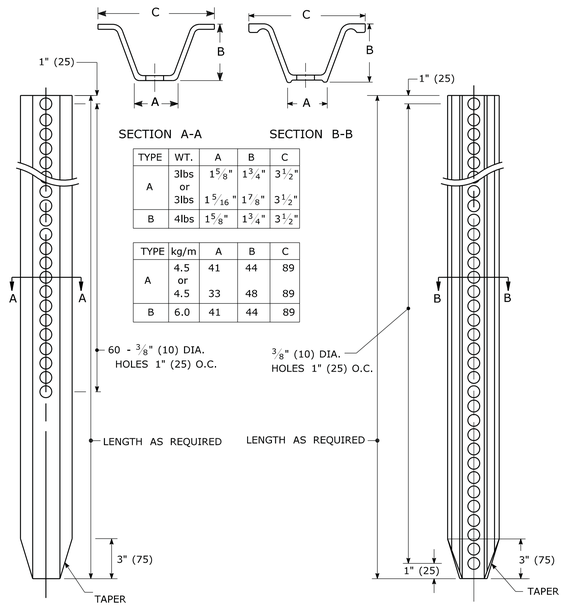
STANDARD SHEET TITLE:

SIGN SUPPORT & SIGN PLACEMENT DETAILS, GORE EXIT SIGN

STANDARD SHEET NO.:

TR-1208_01

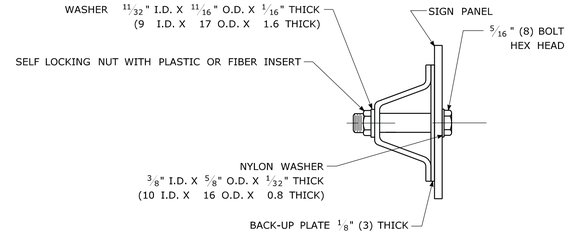
TYPICAL METAL SIGN POSTS



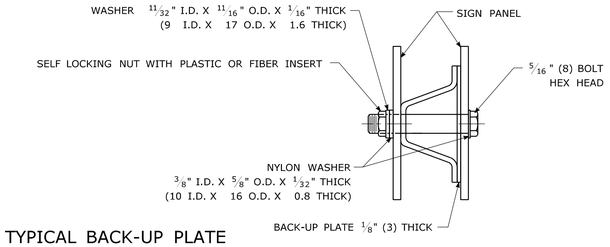
TYPE	WT.	A	B	C
A	3lbs or 3lbs	1 5/8"	1 3/4"	3 1/2"
		1 5/16"	1 7/8"	3 1/2"
B	4lbs	1 5/8"	1 3/4"	3 1/2"

TYPE	kg/m	A	B	C
A	4.5 or 4.5	41	44	89
		33	48	89
B	6.0	41	44	89

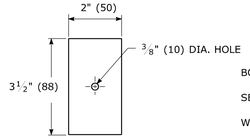
TYPICAL SIGN PANEL ATTACHMENT



TYPICAL BACK TO BACK SIGN PANEL ATTACHMENT



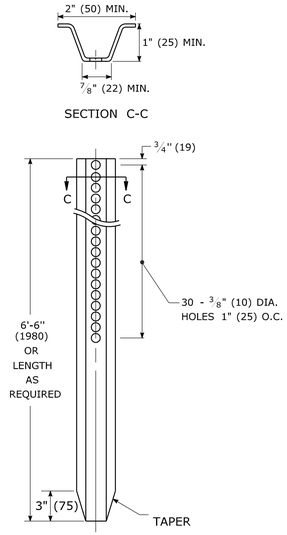
TYPICAL BACK-UP PLATE



BOLTS - STAINLESS STEEL CONFORMING TO ASTM F593, ALLOY GROUP 1 OR 2 (ALLOY TYPES 304 OR 316).
SELF LOCKING NUTS - STAINLESS STEEL CONFORMING TO ASTM F594, ALLOY GROUP 1 OR 2 (ALLOY TYPES 304 OR 316).
WASHERS - STAINLESS STEEL CONFORMING TO ASTM A240, (ALLOY TYPES 304 OR 316).

METAL DELINEATOR POST

WT./FT. = 1.12 LBS. MIN.
(MASS/m = 1.67 kg/m MIN.)



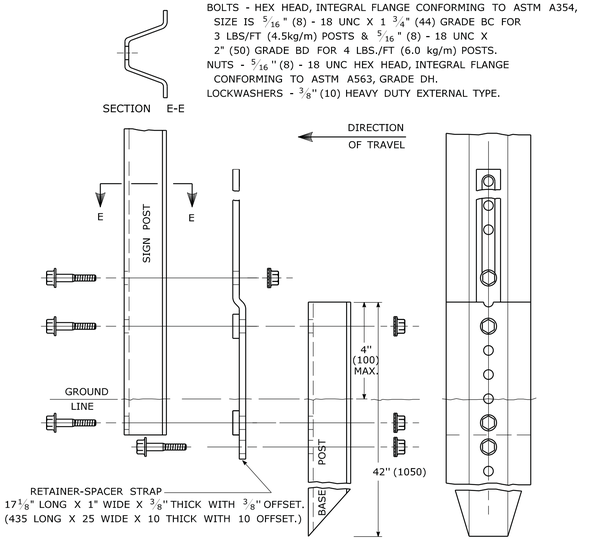
GENERAL NOTES:

1. STEEL FOR DELINEATOR POSTS SHALL BE ASTM A36/A36(m) STEEL. STEEL FOR ALL OTHER POSTS SHALL CONFORM TO THE MECHANICAL REQUIREMENTS OF ASTM A 499 GRADE 60 AND TO THE CHEMICAL REQUIREMENTS OF ASTM A1 CARBON STEEL TEE RAIL HAVING NOMINAL WEIGHT (MASS) OF 91lbs. (45 kg.) OR GREATER PER LINEAR YARD (METER).
2. AFTER FABRICATION, ALL STEEL POSTS, STRAPS AND PLATES SHALL BE GALVANIZED TO MEET THE REQUIREMENTS OF ASTM A123/A123(m).
3. WASHERS FOR BREAKAWAY INSTALLATIONS SHALL MEET ASTM F436, TYPE 1.
4. ALL BOLTS, NUTS, AND WASHERS FOR BREAKAWAY INSTALLATIONS SHALL BE GALVANIZED TO MEET THE REQUIREMENTS OF ASTM A153/A153(m).
5. ALL SIGN POSTS SHALL HAVE BREAKAWAY FEATURES THAT MEET AASHTO REQUIREMENTS CONTAINED IN THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS." THE BREAKAWAY FEATURES SHALL BE STRUCTURALLY ADEQUATE TO CARRY THE SIGNS SHOWN IN THE PLANS AT 60 mph (97 km/h) WIND LOADINGS. INSTALLATIONS SHALL BE IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS.
6. TYPE A POSTS - 3 lbs/ft (4.5 kg/m) TYPE B POSTS - 4 lbs/ft (6 kg/m).

BREAKAWAY TYPE I INSTALLATION

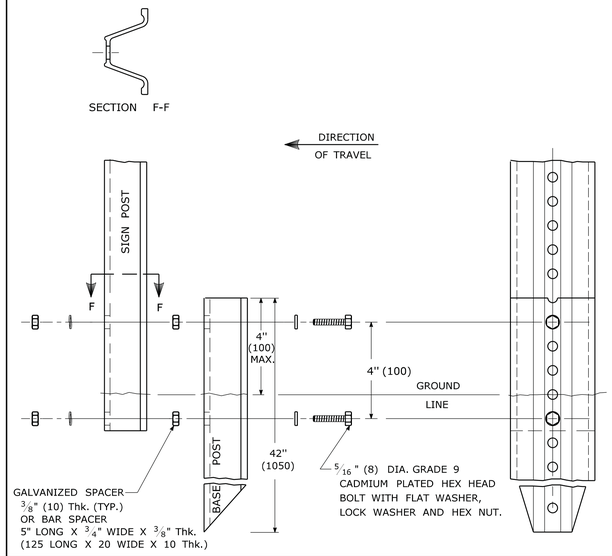
FOR 3 & 4 LB. POSTS
(FOR 4.5 & 6.0 kg/m POSTS)

BOLTS - HEX HEAD, INTEGRAL FLANGE CONFORMING TO ASTM A354, SIZE IS 5/16" (8) - 18 UNC X 1 3/4" (44) GRADE BC FOR 3 LBS/FT (4.5kg/m) POSTS & 5/16" (8) - 18 UNC X 2" (50) GRADE BD FOR 4 LBS./FT (6.0 kg/m) POSTS.
NUTS - 5/16" (8) - 18 UNC HEX HEAD, INTEGRAL FLANGE CONFORMING TO ASTM A563, GRADE DH.
LOCKWASHERS - 3/8" (10) HEAVY DUTY EXTERNAL TYPE.

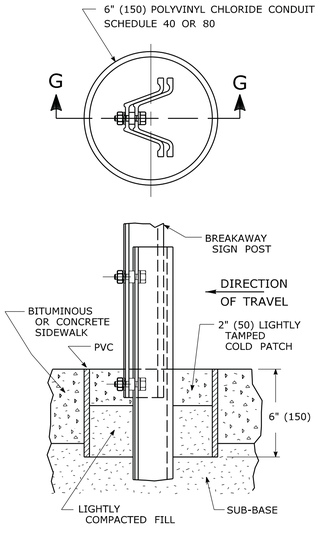


BREAKAWAY TYPE II INSTALLATION

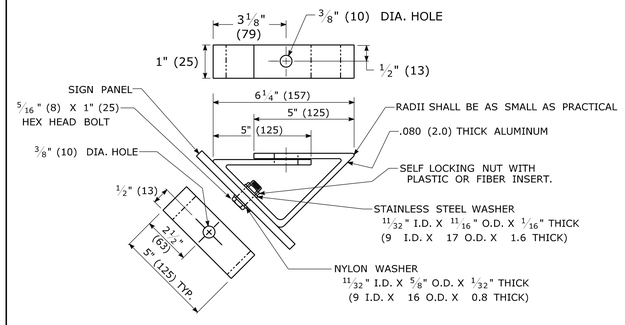
FOR 3 & 4 LB. POSTS
(FOR 4.5 & 6.0 kg/m POSTS)



TYPICAL SLEEVE FOR PAVED AREAS



45° SUBMOUNTING BRACKET



REV.	DATE	REVISION DESCRIPTION
1	2-2011	MINOR REVISIONS.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 3/22/2011

DIMENSIONS ARE IN ENGLISH (.") & METRIC UNITS (mm).
METRIC DIMENSIONS ARE ROUNDED:
- OVER 1" TO NEAREST 5 mm
- UNDER 1" TO NEAREST 1 mm.

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_TRAFFIC_STD.dgn Model: TR-1208_02

SUBMITTED BY: NAME/DATE/TIME:

APPROVED BY: NAME/DATE/TIME:

CTDOT STANDARD SHEET

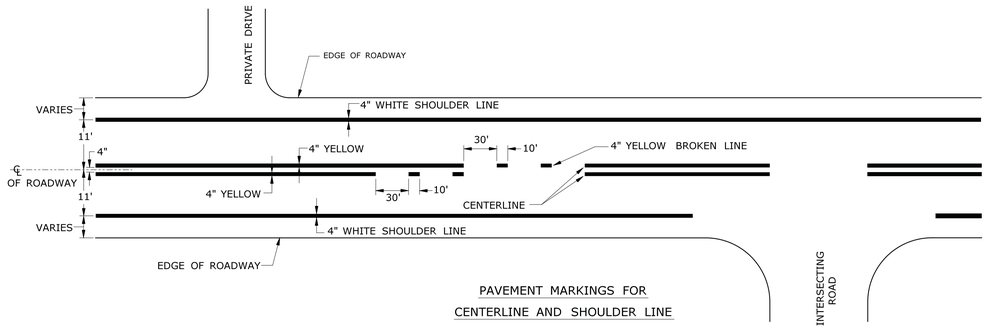
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

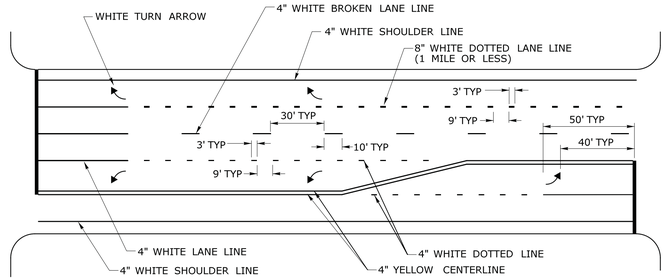
METAL SIGN POSTS AND SIGN MOUNTING DETAILS

STANDARD SHEET NO.:

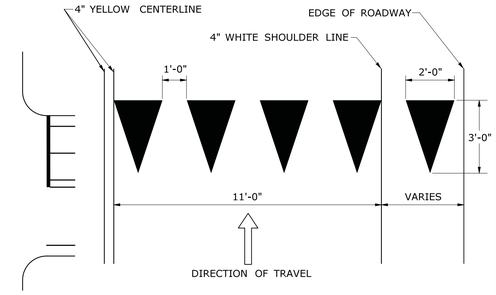
TR-1208_02



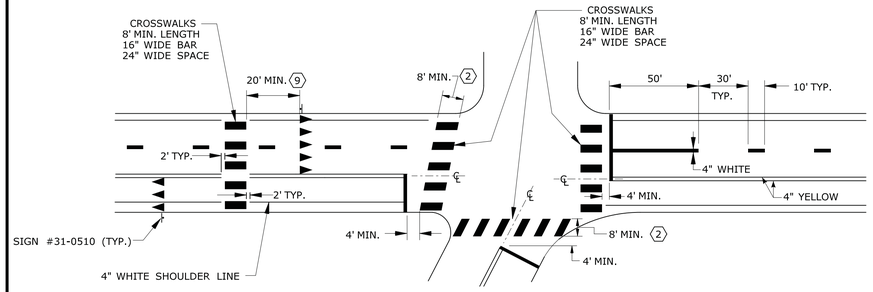
PAVEMENT MARKINGS FOR
CENTERLINE AND SHOULDER LINE



PAVEMENT MARKINGS FOR
TURNING LANES



YIELD LINE DETAIL
(WHITE)



PAVEMENT MARKINGS FOR
STOP BARS, YIELD LINES, AND CROSSWALKS

NOTES:

STOP BARS AND YIELD LINES

1. STOP BARS AND YIELD LINES SHALL BE WHITE.
2. STOP BARS SHALL BE 12" MIN. UNLESS OTHERWISE NOTED ON PLANS.
3. STOP BARS TO BE MARKED A MINIMUM OF 4' IN ADVANCE OF THE NEAREST EDGE OF CROSSWALK AND SHOULD BE PLACED 90° TO THE CENTERLINE OF THE ROADWAY.
4. IN THE ABSENCE OF A MARKED CROSSWALK THE STOP BAR SHOULD BE PLACED 90° TO THE CENTERLINE OF THE ROADWAY, AT THE DESIRED STOPPING POINT AT LEAST 5' AND NO MORE THAN 30' FROM THE NEAREST EDGE OF THE INTERSECTING ROADWAY.
5. THE STOP BAR SHOULD BE PLACED IN LINE WITH THE STOP SIGN. HOWEVER, IF THE STOP SIGN CANNOT BE LOCATED EXACTLY WHERE VEHICLES ARE EXPECTED TO STOP, THE STOP BAR SHOULD BE PLACED AT THE STOPPING POINT.
6. FOR STOP BARS AT RAMPS SEE DETAILS "O" & "P" AND NOTES ON TRAFFIC STANDARD SHEET TR-1210_02 "PAVEMENT MARKINGS FOR DIVIDED HIGHWAYS".
7. YIELD LINES SHOULD BE INSTALLED FROM THE CENTERLINE TO THE CURB LINE/EDGE OF ROAD AND SHOULD BE PLACED 90° TO THE CENTERLINE OF THE ROADWAY.
8. FOR YIELD LINE INSTALLATIONS, ONLY FULL TRIANGLES ARE TO BE INSTALLED.

⑨ AT MID-BLOCK CROSSWALKS ONLY, YIELD LINES SHOULD BE INSTALLED 20 TO 50 FEET IN ADVANCE OF THE NEAREST CROSSWALK LINE, OR AS DIRECTED BY THE ENGINEER.

10. THE YIELD LINE SHOULD BE PLACED IN LINE WITH A YIELD SIGN. HOWEVER, IF THE YIELD SIGN CANNOT BE LOCATED EXACTLY WHERE VEHICLES ARE EXPECTED TO YIELD, THE YIELD LINE SHOULD BE PLACED AT THE YIELDING POINT.

CROSSWALKS

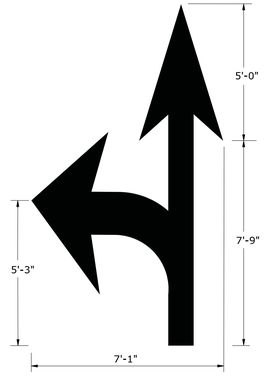
1. CROSSWALK MARKINGS SHALL BE WHITE.

② AT LOCATIONS WHERE THE CROSSWALK IS SKEWED, BARS TO BE PARALLEL TO ℄ AND ENDS OF BARS TO BE PARALLEL. THE LENGTH OF THE BARS WILL VARY DEPENDING ON THE ANGLE OF SKEW.

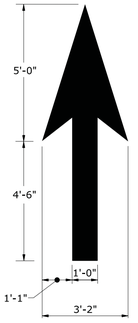
3. BARS SHOULD BE NO CLOSER THAN 1' FROM CURB LINE/EDGE OF ROAD.
4. ONLY FULL LENGTH BARS ARE TO BE INSTALLED.
5. DECORATIVE CROSSWALKS SHALL BE BANDED FROM CURB TO CURB WITH A MINIMUM 12" WIDE WHITE TRANSVERSE LINE ALONG EACH EDGE.
6. 24" WIDE SPACE TO BE CENTERED ON YELLOW CENTERLINE.

RAILROAD GRADE CROSSINGS

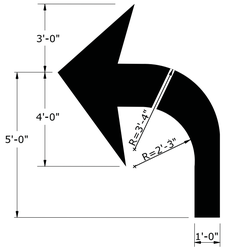
1. RAILROAD MARKINGS SHALL BE WHITE.
2. ON MULTI-LANE ROADS THE TRANSVERSE BANDS SHOULD EXTEND ACROSS THE APPROACH LANES AND INDIVIDUAL R X R SYMBOLS SHOULD BE USED IN EACH APPROACH LANE.



REFER TO ADJACENT VIEWS FOR DIMENSIONING
27.0 S.F.



12.5 S.F.

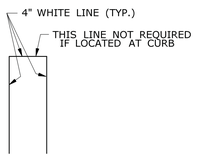


15.5 S.F.

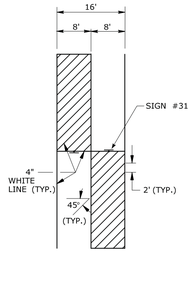
27.0 S.F.
LEFT & RIGHT
ARROW COMBO

38.5 S.F.
3 WAY ARROW
COMBO

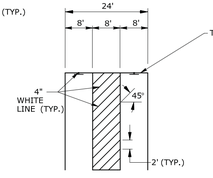
PAVEMENT ARROW DETAILS
(WHITE)
ARROWS SHALL BE CENTERED IN TRAVEL LANE



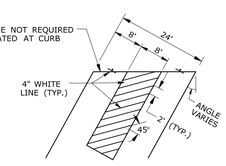
STANDARD
PARKING STALL



(PIGGY BACK STYLE)



(SIDE BY SIDE STYLE)
(SHARED AISLE)

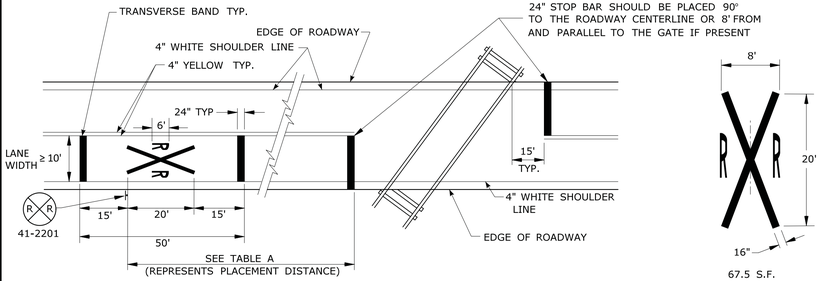


(ANGLED SIDE BY SIDE STYLE)
(SHARED AISLE)

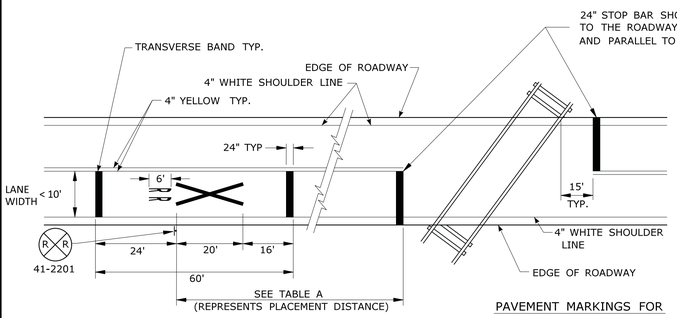
PARKING STALLS FOR HANDICAPPED

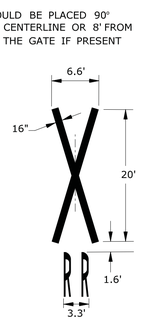
NOTES:
PAVEMENT MARKING

1. FOR PAVEMENT MARKINGS ON A CLIMBING LANE SEE DETAIL "L" ON TRAFFIC STANDARD SHEET TR-1210_02 "PAVEMENT MARKINGS FOR DIVIDED HIGHWAYS".
2. AREA OF PAVEMENT MARKINGS AS INDICATED IS APPROXIMATE.
3. FOR PAVEMENT MARKING LINES, STOP BARS AND ARROWS AT RAMPS SEE DETAILS "O" & "P" AND NOTES ON TRAFFIC STANDARD SHEET TR-1210_02 "PAVEMENT MARKINGS FOR DIVIDED HIGHWAYS".
4. RIGHT TURN PAVEMENT MARKINGS ARROWS ARE MIRROR IMAGE OF LEFT TURN PAVEMENT MARKING ARROWS.
5. SHARED AISLES MAY NOT BE USED WHERE CONNECTICUT BUILDING CODE GOVERNS.
6. LANE WIDTHS TO BE 11' UNLESS OTHERWISE NOTED.



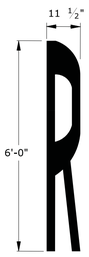
67.5 S.F.





67.5 S.F.

PAVEMENT MARKINGS FOR
RAILROAD GRADE CROSSINGS



5.75 S.F.

TABLE A	
POSTED OR 85 PERCENTILE SPEED M.P.H.	MINIMUM DISTANCE FT.
20	100
25	100
30	100
35	100
40	125
45	175
50	250
55	325
60	400
65	475

REV.	DATE	REVISION DESCRIPTION
3	10-2014	ADDED YIELD LINE DETAIL AND NOTES. REVISED NOTES FOR STOP BARS AND CROSSWALKS. REVISED MARKINGS FOR STOP BARS AND CROSSWALKS. REVISED STANDARD LANE WIDTH. REVISED MARKINGS FOR GRADE CROSSINGS AND TABLE A.
2	12-2013	REMOVED PAVEMENT MARKING ARROWS ON RAMPS NOTES AND ADDED NEW NOTE #3 REFERRING TO SHEET TR-1210_02.
1	2-2011	ADDED PAVEMENT MARKINGS FOR TURNING LANES.

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Plotted Date: 10/9/2014

DIMENSIONS ARE IN ENGLISH (' ")

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_TRAFFIC_STD.DGN Model: TR-1210_03

SUBMITTED BY: NAME/DATE/TIME:

APPROVED BY: NAME/DATE/TIME:

CTDOT
STANDARD SHEET

OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

SPECIAL DETAILS AND PAVEMENT MARKINGS FOR TWO-WAY HIGHWAYS

STANDARD SHEET NO.:

TR-1210_03

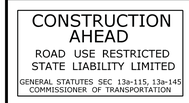
E5 - SERIES

ES-1

COPY & BORDER - WHITE
BACKGROUND - GREEN

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	16.0	48	51-6147	2

16 - SERIES

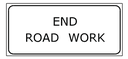


	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
16-M	5.0	30X24	80-1613	1
16-H	17.5	60X42	80-1608	2
16-E	35.0	84X60	80-1605	2

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
16-S	10.0	48X30	80-1619	2

G20 - SERIES

G20-2a



	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	8.0	48X24	80-9612	2

VARIABLE MILEAGE

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	90.0	120X108	80-9728	

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	9.0	36	80-9711	1
	16.0	48	80-9712	2

VARIABLE MILEAGE

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	7.0	72X14	80-9720	

VARIABLE ARROW
COPY & BORDER - WHITE
BACKGROUND - BLUE

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	9.0	36	50-5934	1

M4 - SERIES

M4-8

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	2.0	24X12	80-9707	1

M4-9b

VARIABLE ARROW

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	5.0	30X24	80-9703	1

M4-8a

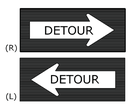
	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	3.0	24X18	80-9708	1

M4-9b

VARIABLE ARROW

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	6.25	30X24	80-9706	1

M4-10



	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	6.0	48X18	80-9701R	2
	6.0	48X18	80-9702L	2

VARIABLE ARROW

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	5.0	30X24	80-9710	2

R1 - SERIES

R1-1

*
COPY & BORDER - WHITE
BACKGROUND - RED

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	5.19	30	31-0552	1
	13.30	48	31-0557	2

R1-2

*
COPY & BORDER - RED
BACKGROUND - WHITE

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	3.90	36	31-0523	1
	10.83	60	31-0528	2

R4 - SERIES

R4-7

COPY & BORDER - BLACK
BACKGROUND - WHITE

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	5.0	24X30	31-1526	1

R9 & R11 - SERIES

R9-9

COPY & BORDER - BLACK
BACKGROUND - WHITE

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	3.75	30X18	80-9076	1

R9-11

VARIABLE ARROW
COPY & BORDER - BLACK
BACKGROUND - WHITE

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	2.0	24X12	80-9074	1

R9-11a

VARIABLE ARROW
COPY & BORDER - BLACK
BACKGROUND - WHITE

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	2.0	24X12	80-9075	1

R11-2

COPY & BORDER - BLACK
BACKGROUND - WHITE

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	10.0	48X30	80-9080	2

R11-3a

VARIABLE MILEAGE
COPY & BORDER - BLACK
BACKGROUND - WHITE

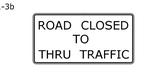
	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	12.5	60X30	80-9077	2

R11-3b

VARIABLE MILEAGE
COPY & BORDER - BLACK
BACKGROUND - WHITE

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	12.5	60X30	80-9078	2

R11-3b



COPY & BORDER - BLACK
BACKGROUND - WHITE

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	12.5	60X30	80-9081	2

R11-3b

COPY & BORDER - BLACK
BACKGROUND - WHITE

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	14.0	48X42	31-1906	2
	22.5	60X54	31-1907	2
	30.0	72X60	31-1908	2

W1 - SERIES

W1-4

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	9.0	36	80-9432L	1
	9.0	36	80-9431R	1
	16.0	48	80-9452L	2
	16.0	48	80-9451R	2

W1-6

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	4.5	36X18	80-9422	1
	8.0	48X24	80-9424	2

W1-8

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	1.5	12X18	80-9402	1
	5.0	24X30	80-9403	1
	7.5	30X36	80-9404	1

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	16.0	48	80-9433L	2
	16.0	48	80-9435R	2

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	16.0	48	80-9434L	2
	16.0	48	80-9436R	2

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	25.0	60	80-9443L	2
	25.0	60	80-9445R	2

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	25.0	60	80-9444L	2
	25.0	60	80-9446R	2

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	25.0	60	80-9483L	2
	25.0	60	80-9485R	2

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	25.0	60	80-9484L	2
	25.0	60	80-9486R	2

W3 - SERIES

W3-1

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	9.0	36	80-9808	1

W3-1a

OCTAGON - RED W/ WHITE BORDER
ARROW & BORDER - BLACK
BACKGROUND - ORANGE

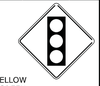
	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	9.0	36	80-9050	1
	16.0	48	80-9051	2

W3-2a

TRIANGLE - RED W/ WHITE BORDER
ARROW & BORDER - BLACK
BACKGROUND - ORANGE

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	9.0	36	80-9054	1
	16.0	48	80-9055	2

W3-3

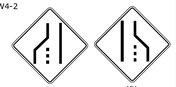


TOP CIRCLE - RED
MIDDLE CIRCLE - YELLOW
BOTTOM CIRCLE - GREEN
COPY & BORDER - BLACK
BACKGROUND - ORANGE

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	9.0	36	80-9052	1
	16.0	48	80-9053	2

W4-W6 - SERIES

W4-2



	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	16.0	48	80-9918L	2
	16.0	48	80-9917R	2

W6-3

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	16.0	48	80-9945	2

W8-W9 - SERIES

W8-1

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	9.0	36	80-9901	1
	16.0	48	80-9902	2

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	9.0	36	80-9801	1
	16.0	48	80-9802	2

W9-2

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	16.0	48	80-9910L	2
	16.0	48	80-9911R	2

W13 - SERIES

W13-1

SUBPLATE
VARIABLE SPEED

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	2.25	18	80-9568	1
	4.0	24	80-9569	1

VARIABLE SPEED

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	9.0	36	80-9506	1
	16.0	48	80-9508	2

VARIABLE SPEED

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	12.0	72X24	80-9519	2

W20 - SERIES

W20-1

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	6.25	30	80-9602	1
	9.0	36	80-9603	1
	16.0	48	80-9604	2

VARIABLE DISTANCE

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	9.0	36	80-9614	1
	16.0	48	80-9615	2

W20-2

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	9.0	36	80-9805	1
	16.0	48	80-9806	2

W20-4

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	9.0	36	80-9834	1
	16.0	48	80-9835	2

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
(L)	16.0	48	80-9836	2
(R)	16.0	48	80-9839	2

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
(L)	16.0	48	80-9837	2
(R)	16.0	48	80-9838	2

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
(L)	16.0	48	80-9847	2
(R)	16.0	48	80-9848	2

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
(L)	16.0	48	80-9846	2
(R)	16.0	48	80-9849	2

W20-7a

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	9.0	36	80-9803	1
	16.0	48	80-9804	2

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	32.0	96X48	80-9815	

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	1.66	30X8	80-9829	

W21 - SERIES

W21-6

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	9.0	36	80-9607	1

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	2.0	24X12	80-9912	1

W22 - SERIES

W22-1

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	9.0	36	80-9620	1
	16.0	48	80-9625	2

W22-2

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	10.5	42X36	80-9623	2

W22-3

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	7.5	36X30	80-9622	1
	10.5	42X36	80-9621	2

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	4.17	60X10	80-9913	2
	12.0	96X18	80-9914	2
	3.33	48X10	80-9916	2

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	16.0	48	80-9951	2

BLANK OR
VARIABLE LEGEND

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	9.0	36	80-9933	1
	16.0	48	80-9934	2

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	16.0	48	80-9956	2

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	12.5	60X30	80-9928	2
	24.0	72X48	80-9929	2

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
(1)	16.0	48	80-9957	2
(2)	9.0	36	80-9958	1
(2)	16.0	48	80-9959	2

STOP-SLOW PADDLE

SIDE A
BACKGROUND - RED
COPY & BORDER - WHITE
SIDE B
BACKGROUND - ORANGE
COPY & BORDER - BLACK PLAIN

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	2.51	19	80-9950	PADDLE

COPY & BORDER - BLACK
BACKGROUND - YELLOW

	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS
	2.0	12X24	41-0815	

METRIC CONVERSION CHART
(1'' = 25mm)

ENGLISH	METRIC	ENGLISH	METRIC
12''	300	60''	1500
18''	450	66''	1650
24''	600	72''	1800
30''	750	78''	1950
36''	900	84''	2100
42''	1050	90''	2250
48''	1200	96''	2400
54''	1350		

NOTES:

FOR METRIC SEE CONVERSION CHART.
1. R1-SERIES SIGN THE LEGEND "O.S.T.A." SHALL APPEAR.
2. POSTS - SEE STANDARD SHEET TR-1208_02 - "METAL SIGN POSTS AND SIGN MOUNTING DETAILS".
3. POSTS - TYPE A (EXCEPT WHERE NOTED WITH A "B" FOR TYPE B)
4. ALL POSTS NOTED ARE FOR LONG TERM INSTALLATION. SEE STANDARD SHEET TR-1208_02.
5. FOR TEMPORARY SUPPORTS SEE STANDARD SHEET TR-1220_02 - "CONSTRUCTION SIGN SUPPORTS AND CHANNELIZING DEVICES".
6. FOR SPECIFIC SIGN DESIGN, CONTACT CONN. D.O.T., DIVISION OF TRAFFIC ENGINEERING. FOR BOLT HOLE PATTERN REFER TO FHWA PUBLICATION "STANDARD HIGHWAY SIGNS". SIGNS OF DIFFERENT DIMENSIONS TO BE ERECTED ON THE SAME POSTS, OR SPAN/MAST ARM MOUNTED, MAY REQUIRE SPECIAL BOLT HOLE PATTERNS.
7. ALL CONSTRUCTION SIGNS TO BE PAID FOR UNDER THE CONSTRUCTION SIGNS ITEM IN THE CONTRACT.
8. MATERIALS & COLORS SHALL CONFORM TO STATE SPECIFICATIONS

MATERIALS:
SIGNS AND THEIR PORTABLE SUPPORTS SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3). ALUMINUM THICKNESS FOR POST MOUNTED SIGNS SHALL BE .100 EXCEPT SIGN #s. 80-9815, 80-9728, 80-9519, & 51-6147 (L OR R) WHICH SHALL BE .125, PLYWOOD THICKNESS FOR POST MOUNTED SIGNS SHALL BE 1/2 " EXTERIOR GRADE A-C OR BETTER. SIGN BLANKS SHALL HAVE ONE COAT OF PRIMER PAINT PRIOR TO APPLICATION OF RETROREFLECTIVE SHEETING & COPY.

ALL COLORS SHALL BE RETROREFLECTIVE WITH THE EXCEPTION OF BLACK WHICH SHALL BE OPAQUE.

COLORS: (UNLESS OTHERWISE SPECIFIED)
LEGEND - BLACK PLAIN
BACKGROUND - ORANGE RETROREFLECTIVE

* SIGNS TO BE "BRIGHT WIDE ANGLE RETROREFLECTIVE SHEETING.

REV.	DATE	REVISION DESCRIPTION
4	6-2012	REVISED NOTE #1 TO REFERENCE "O.S.T.A."
3	4-2012	REVISED NEW SIGNAL SIGN(S) TO CONFORM TO 2009 MUTCD.
2	2-2011	MINOR REVISIONS.
1	3-2010	REMOVED OBSOLETE SIGNS (50-5925, 50-5935).

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 7/20/2012

DIMENSIONS ARE IN ENGLISH (.") & METRIC UNITS (mm).
METRIC DIMENSIONS ARE ROUNDED:
- OVER 1" TO NEAREST 5 mm
- UNDER 1" TO NEAREST 1 mm.

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_TRAFFIC_STD.dgn Model: TR-1220_01

SUBMITTED BY: NAME/DATE/TIME:

APPROVED BY: NAME/DATE/TIME:

CTDOT
STANDARD SHEET

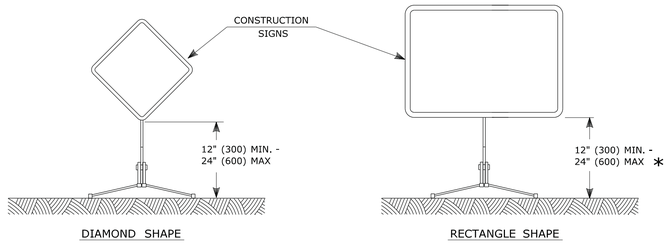
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

SIGNS FOR CONSTRUCTION AND PERMIT OPERATIONS

STANDARD SHEET NO.:

TR-1220_01

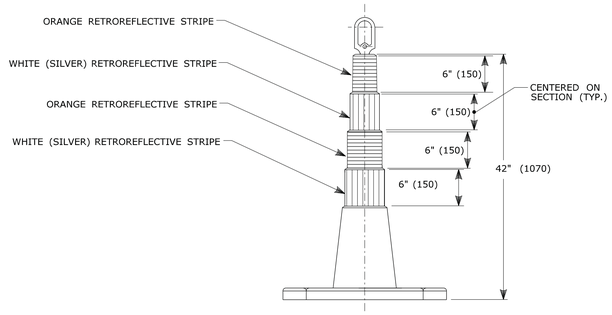


CONSTRUCTION SIGNS

NOTES FOR PORTABLE SIGN SUPPORTS:

1. SIGNS AND THEIR PORTABLE SUPPORTS SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) AND THE LATEST EDITION OF THE MUTCD.
2. MOUNTING HEIGHT OF SIGNS SHALL BE A MINIMUM OF 12" (300) AND A MAXIMUM OF 24" (600). SIGNS SHALL BE MOUNTED HIGHER AS NEEDED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
3. THE ENGINEER RESERVES THE RIGHT TO REJECT ANY SUPPORT DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
4. PORTABLE SIGN SUPPORTS SHALL BE STABILIZED IN A MANNER THAT WILL NOT AFFECT THEIR COMPLIANCE WITH NCHRP REPORT 350 (TL-3).

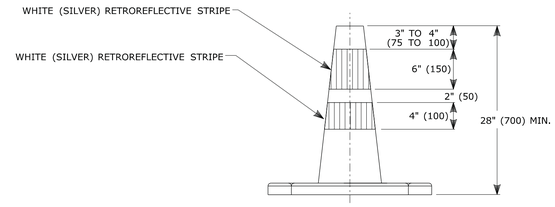
* FOR EXIT SIGNS, USE MIN. 72" (1800).



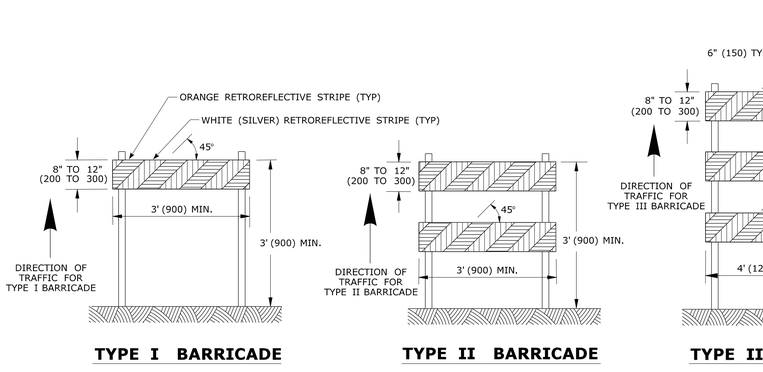
42" (1m) TRAFFIC CONE

NOTES:

1. TRAFFIC CONES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) AND THE LATEST EDITION OF THE MUTCD.
2. IF RUBBER CONES ARE USED, THEY SHALL HAVE INTERIOR RIBS FOR RIGIDITY.
3. IF PLASTIC CONES ARE USED, THEY SHALL BE COLOR IMPREGNATED.
4. THE ENGINEER RESERVES THE RIGHT TO REJECT ANY CONE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.



TRAFFIC CONE



TYPE I BARRICADE

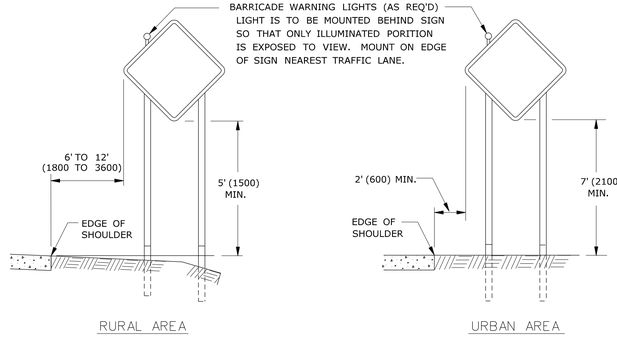
TYPE II BARRICADE

TYPE III BARRICADE

CONSTRUCTION BARRICADES

NOTES:

1. CONSTRUCTION BARRICADES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) AND THE LATEST EDITION OF THE MUTCD.
2. MARKINGS FOR BARRICADE RAILS SHALL BE ALTERNATE ORANGE AND WHITE STRIPES SLOPING DOWNWARD IN THE DIRECTION TRAFFIC IS TO PASS. 6" (150) WIDE STRIPES SHALL BE USED.
3. THE ENTIRE AREA OF ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS. RAILS FOR TYPE I AND TYPE II BARRICADES SHALL BE RETROREFLECTIVE ON BOTH SIDES. WHERE TRAFFIC PASSES ONLY IN ONE DIRECTION OF TRAVEL, ONLY THE SIDE FACING TRAFFIC SHALL BE RETROREFLECTIVE.
4. THE ENGINEER RESERVES THE RIGHT TO REJECT ANY BARRICADE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
5. CORNERS OF BARRICADE RAILS SHALL BE ROUNDED.
6. SIGNS MAY ONLY BE INSTALLED ON TYPE III BARRICADES AND SHALL BE PLACED SO AS TO COVER NO MORE THAN ONE BARRICADE RAIL.

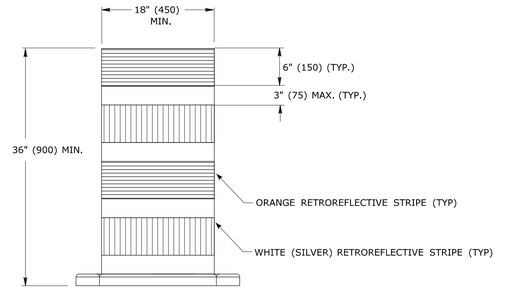


PLACEMENT OF CONSTRUCTION SIGNS TYPICAL LONG TERM INSTALLATION

NOTES:

SUPPORTS SHALL BE METAL SIGN POSTS AND HAVE BREAK-AWAY FEATURES.

SEE TYPICAL SHEETS:
"TYPICAL SIGN SUPPORT AND SIGN PLACEMENT DETAILS-GORE EXIT SIGN"
"TYPICAL METAL SIGN POSTS AND SIGN MOUNTING DETAILS"



TRAFFIC DRUM FRONT VIEW

NOTES:

1. TRAFFIC DRUM SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) AND THE LATEST EDITION OF THE MUTCD.
2. THE ENGINEER RESERVES THE RIGHT TO REJECT ANY DRUM DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
3. THE ENTIRE AREA OF ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS.
4. THE SECTIONS OF DRUMS NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.

REV.	DATE	REVISION DESCRIPTION
1	2-2011	MINOR REVISIONS.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 2/16/2011

DIMENSIONS ARE IN ENGLISH (.") & METRIC UNITS (mm).
METRIC DIMENSIONS ARE ROUNDED:
- OVER 1" TO NEAREST 5 mm
- UNDER 1" TO NEAREST 1 mm.

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_TRAFFIC_STD.dgn Model: TR-1220_02

SUBMITTED BY: NAME/DATE/TIME:

APPROVED BY: NAME/DATE/TIME:

CTDOT STANDARD SHEET

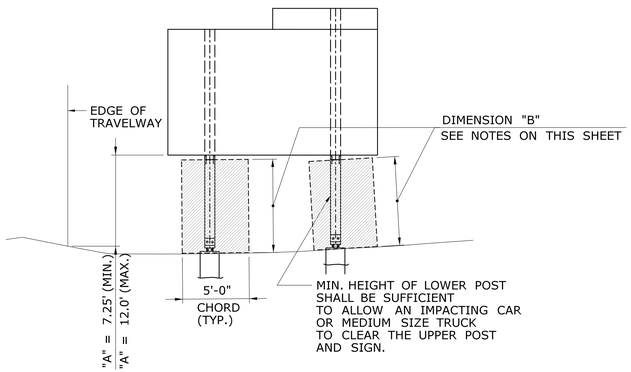
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

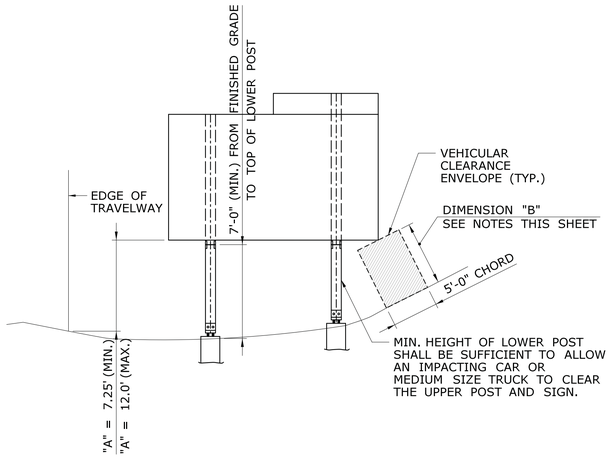
CONSTRUCTION SIGN SUPPORTS AND CHANNELIZING DEVICES

STANDARD SHEET NO.:

TR-1220_02

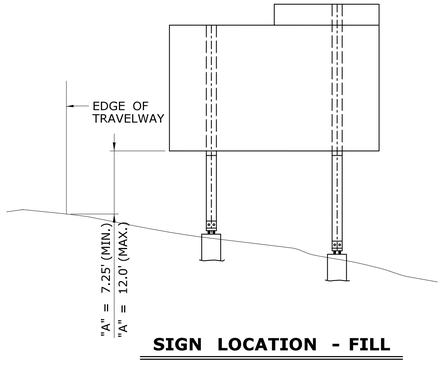


LEVEL TO SHALLOW SLOPES

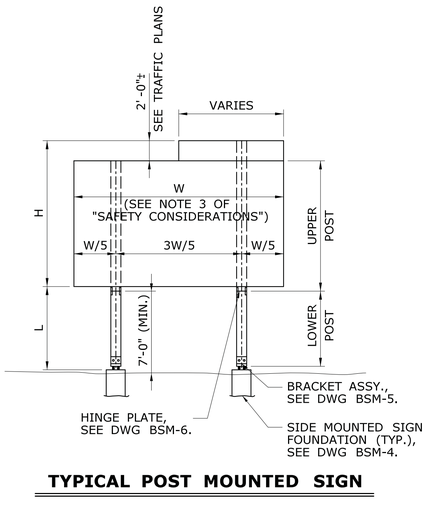


STEEPER SLOPES

SIGN LOCATION - CUT



SIGN LOCATION - FILL



TYPICAL POST MOUNTED SIGN

NOTES FOR DETERMINING DIMENSION "B"

1. DIMENSION "B" IS THE SMALLER OF:
   A. THE CLEAR DISTANCE BETWEEN THE BOTTOM OF SIGN AND THE FINISHED GRADE.
   B. THE CLEAR DISTANCE BETWEEN THE BOTTOM OF UPPER POST AND THE FINISHED GRADE.
2. DIMENSION "B" SHALL TYPICALLY BE A MINIMUM OF 7'-0" TO CLEAR AN IMPACTING CAR OR MEDIUM SIZE TRUCK.
3. WHEN DIMENSION "A" WOULD EXCEED 12'-0", CONSIDERATION MAY BE GIVEN TO REDUCING DIMENSION "B" IN ACCORDANCE WITH PROVISIONS OF NOTE 4.
4. DIMENSION "B" MAY BE LESS THAN 7'-0":
   A. IF THE POST IS OUT OF THE CLEAR ZONE.
   B. IF THE POST IS WITHIN THE CLEAR ZONE BUT SHIELDED BY AN APPROPRIATE BARRIER SYSTEM.
   C. IN NO CASE SHALL DIMENSION "B" BE LESS THAN 2'-6".
5. IF FIELD CONDITIONS EXCEED THESE REQUIREMENTS, CONTACT THE ENGINEER FOR DIRECTION.

SAFETY CONSIDERATIONS

1. THE HINGE BETWEEN THE UPPER AND LOWER POSTS SHALL BE AT LEAST 7 FT. ABOVE THE GROUND.
2. NO SUPPLEMENTARY SIGNS SHALL BE ATTACHED BELOW THE HINGES.
3. THE POST SPACING SHALL BE 3/5 W EXCEPT AS NOTED BELOW:

UNIT WEIGHT OF POST	POST SPACING REQUIREMENTS
LESS THAN 18 PLF	NO RESTRICTIONS ON POST SPACING **
FROM 18 PLF TO 45 PLF	PROVIDE AT LEAST 7 FT. CLEAR DISTANCE BETWEEN POSTS ***
EXCEEDS 45 PLF	RELOCATE SIGN OUTSIDE OF CLEAR ZONE OR SHIELD SIGN FROM VEHICULAR IMPACT AS DIRECTED BY THE ENGINEER

** IF THE TOTAL COMBINED WEIGHT OF ONE LOWER POST AND TWO BRACKETS EXCEEDS 600 LBS OR THE COMBINED WEIGHT OF TWO POSTS AND FOUR BRACKETS LOCATED WITHIN A CLEAR DISTANCE OF 7 FT OF EACH OTHER EXCEEDS 600 LBS, THE SIGN SHALL BE RELOCATED OUTSIDE OF THE CLEAR ZONE OR SHALL BE PROPERLY SHIELDED FROM VEHICULAR IMPACT AS DIRECTED BY THE ENGINEER. SEE "TABLE 1 - BRACKET DATA" ON BSM-5 FOR BRACKET WEIGHT.

*** IF THE REQUIRED CLEAR DISTANCE CANNOT BE ATTAINED, THE ENGINEER MAY DIRECT THAT THE SIGN BE RELOCATED OUTSIDE THE CLEAR ZONE OR THAT IT BE PROPERLY SHIELDED FROM VEHICULAR IMPACT.

SELECTING A POST SIZE, BRACKET NUMBER, AND HINGE TYPE

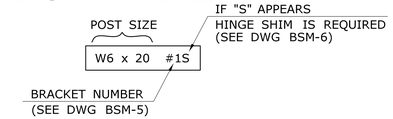
1. DETERMINE THE REQUIRED SIGN DIMENSIONS AND POST HEIGHTS (SEE "TYPICAL POST MOUNTED SIGN" DETAIL, THIS SHEET).

   W = SIGN WIDTH (HORIZONTAL DIMENSION)
   H = SIGN HEIGHT (VERTICAL DIMENSION) (ADD CROWN HEIGHT WHEN APPLICABLE)
   L = POST HEIGHT (THE DISTANCE BETWEEN THE TOP OF THE FOUNDATION AND THE BOTTOM OF THE SIGN MEASURED AT THE TALLER POST)

2. ENTER "POST SELECTION TABLE 1 AND 2" ON DWG BSM-2 AND BSM-3 WITH THE DESIRED VALUES OF W, H, AND L. ROUND UP TO THE NEAREST VALUES IN THE TABLE. READ THE CORRESPONDING POST SIZE AND BRACKET NUMBER. REFER TO DWG BSM-5 FOR BRACKET TYPE AND BSM-6 FOR TYPICAL HINGE REQUIREMENTS.

   EXAMPLE: W = 8', L = 10', H = 14'

   ENTER "POST SELECTION TABLE 1" ON DWG BSM-2 SINCE TABLE 1 IS APPLICABLE FOR SIGN WIDTH ≤ 15'. LOCATE THE FOLLOWING CELL:



NOTES ON TOTAL HEIGHT OF SIGN POSTS

1. UPPER SIGN POSTS SHALL EXTEND TO THE TOP OF FULL WIDTH SIGN PANEL OR THE TOP OF CROWN, WHICHEVER IS HIGHER.
2. FOR SIGN OR CROWN PANEL RETROFIT, THE EXISTING SIGN POSTS SHALL BE REPLACED WITH NEW POSTS OR EXTENDED WITH ADDITIONAL SECTIONS USING HINGE ASSEMBLIES. REFER TO TRAFFIC TYPICAL SHEETS "EXTRUDED SIGN PANEL - RETROFIT DETAIL".

BREAKAWAY SIGN SUPPORT TYPICAL SHEETS ARE IN US CUSTOMARY UNITS

FOR METRIC PROJECTS:
1. DETERMINE US CUSTOMARY POST SIZE FROM THE POST SELECTION TABLE.
2. CALCULATE THE WEIGHT OF POSTS IN US CUSTOMARY UNITS (CWT) THEN USE THE FOLLOWING CONVERSION FACTOR TO CONVERT CWT TO KILOGRAMS.

1 CWT = 45.36 KG

EXAMPLE: 120 CWT x 45.36 KG/CWT = 5443 KG

TABLE OF CONTENT

DWG. NO.	DESCRIPTION
BSM-1	GENERAL NOTES
BSM-2	POST SELECTION TABLE 1 (W ≤ 15 FT.)
BSM-3	POST SELECTION TABLE 2 (W > 15 FT.)
BSM-4	FOUNDATION DETAILS
BSM-5	BRACKET DETAILS
BSM-6	HINGE DETAILS

REV.	DATE	REVISION DESCRIPTION	SHEET NO.
-	-	-	-
-	-	-	-
-	-	-	-
-	-	-	-
-	-	-	-
-	-	-	-

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 12/4/2013

DESIGNER/DRAFTER: BKC
CHECKED BY: JRH
SCALE AS NOTED

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\SB..Breakaway..Signpost..BSM1..General Note..REV..1.dgn

SIGNATURE/ BLOCK:
OFFICE OF ENGINEERING
APPROVED BY: DATE:

PROJECT TITLE:

TOWN:

PROJECT NO.

DRAWING NO. BSM-1

DRAWING TITLE: BREAKAWAY SIGN SUPPORTS GENERAL NOTES

SHEET NO.

POST SELECTION TABLE 1

W	L	H (Sign Height + Crown Height)																	
		4 ft	5 ft	6 ft	7 ft	8 ft	9 ft	10 ft	11 ft	12 ft	13 ft	14 ft	15 ft	16 ft	17 ft	18 ft	19 ft	20 ft	21 ft	22 ft
8 ft	7 ft	W6 x 9 #3	W6 x 9 #3	W6 x 9 #2	W6 x 9 #2	W6 x 9 #2	W6 x 12 #2	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #1	W6 x 15 #1S	W6 x 16 #1S	W8 x 18 #2S	W8 x 21 #1S	W10 x 22 #2	W10 x 22 #2	W10 x 26 #2	W10 x 26 #1S	W10 x 26 #1S
	8 ft	W6 x 9 #2	W6 x 9 #2	W6 x 9 #2	W6 x 9 #2	W6 x 12 #2	W6 x 12 #2	W6 x 12 #2	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 16 #1S	W8 x 18 #1	W8 x 21 #1S	W8 x 21 #1S	W10 x 22 #2	W10 x 26 #1	W10 x 26 #1	W10 x 26 #1S	W10 x 30 #1S
	9 ft	W6 x 9 #2	W6 x 9 #2	W6 x 9 #2	W6 x 12 #2	W6 x 12 #2	W6 x 12 #2	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 16 #1	W8 x 18 #1	W6 x 20 #1S	W8 x 21 #1S	W8 x 21 #1S	W10 x 26 #1	W10 x 26 #1	W10 x 26 #1	W10 x 30 #1S	W12 x 30 #1
	10 ft	W6 x 9 #2	W6 x 9 #2	W6 x 12 #2	W6 x 12 #2	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W8 x 18 #1	W6 x 20 #1S	W6 x 20 #1S	W8 x 21 #1S	W10 x 26 #1	W10 x 26 #1	W10 x 26 #1	-	-	-
	11 ft	W6 x 9 #2	W6 x 9 #2	W6 x 12 #1	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W8 x 18 #1	W6 x 20 #1	W6 x 20 #1S	W8 x 21 #1	-	-	-	-	-	-
	12 ft	W6 x 9 #2	W6 x 12 #1	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W8 x 18 #1	W6 x 20 #1	W6 x 20 #1	W6 x 20 #1S	-	-	-	-	-	-	-	-
	13 ft	W6 x 12 #1	W6 x 12 #1	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W8 x 18 #1	W8 x 18 #1	W6 x 20 #1	W6 x 20 #1	-	-	-	-	-	-	-	-	-
	14 ft	W6 x 12 #1	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 16 #1	W8 x 18 #1	W6 x 20 #1	W6 x 20 #1	-	-	-	-	-	-	-	-	-	-
	15 ft	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W8 x 18 #1	-	-	-	-	-	-	-	-	-	-	-	-	-
	16 ft	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
9 ft	7 ft	W6 x 9 #3	W6 x 9 #3	W6 x 9 #2	W6 x 9 #2	W8 x 10 #2	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #1S	W6 x 15 #1S	W8 x 18 #2S	W8 x 21 #2S	W10 x 22 #2	W10 x 22 #2	W10 x 26 #2	W10 x 26 #2S	W10 x 26 #1S	W12 x 30 #2S
	8 ft	W6 x 9 #2	W6 x 9 #2	W6 x 9 #2	W8 x 10 #2	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1S	W8 x 18 #2	W8 x 21 #2S	W8 x 21 #1S	W10 x 22 #2	W10 x 26 #2	W10 x 26 #1	W10 x 26 #1	W12 x 30 #2	W12 x 30 #2S
	9 ft	W6 x 9 #2	W6 x 9 #2	W6 x 9 #2	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W8 x 18 #2	W6 x 20 #1S	W8 x 21 #1S	W8 x 21 #1S	W10 x 26 #2	W10 x 26 #1	W10 x 26 #1	W10 x 30 #1S	W12 x 30 #1	-
	10 ft	W6 x 9 #2	W6 x 9 #2	W6 x 12 #2	W6 x 12 #2	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W8 x 18 #1	W6 x 20 #1S	W6 x 20 #1S	W8 x 21 #1S	W10 x 26 #1	W10 x 26 #1	W10 x 26 #1	-	-	-	-
	11 ft	W6 x 9 #2	W6 x 12 #2	W6 x 12 #1	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W8 x 18 #1	W6 x 20 #1	W6 x 20 #1S	W8 x 21 #1	-	-	-	-	-	-	-
	12 ft	W6 x 9 #2	W6 x 12 #1	W6 x 12 #1	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W8 x 18 #1	W6 x 20 #1	W6 x 20 #1S	W8 x 21 #1	-	-	-	-	-	-	-
	13 ft	W6 x 12 #1	W6 x 12 #1	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W8 x 18 #1	W8 x 18 #1	W6 x 20 #1	W6 x 20 #1	-	-	-	-	-	-	-	-
	14 ft	W6 x 12 #1	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 16 #1	W8 x 18 #1	W6 x 20 #1	W6 x 20 #1	W6 x 20 #1	-	-	-	-	-	-	-	-	-
	15 ft	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W8 x 18 #1	-	-	-	-	-	-	-	-	-	-	-	-	-	-
	16 ft	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
10 ft	7 ft	W6 x 9 #3	W6 x 9 #3	W6 x 9 #2	W6 x 9 #2	W6 x 12 #2	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #1S	W8 x 18 #2	W8 x 18 #2S	W10 x 22 #2	W10 x 22 #2	W10 x 26 #2	W10 x 26 #2S	W10 x 26 #2S	W12 x 30 #2S	W14 x 30 #2S
	8 ft	W6 x 9 #2	W6 x 9 #2	W6 x 9 #2	W6 x 12 #2	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #1	W6 x 15 #1	W8 x 18 #2	W8 x 18 #2S	W8 x 21 #1S	W10 x 22 #2	W10 x 26 #2	W10 x 26 #2	W10 x 26 #1S	W12 x 30 #2	W14 x 30 #2	-
	9 ft	W6 x 9 #2	W6 x 9 #2	W6 x 12 #2	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W8 x 18 #2	W8 x 18 #1	W8 x 21 #1S	W10 x 26 #2	W10 x 26 #1	W10 x 26 #1	W10 x 30 #1S	W12 x 30 #2	-	-
	10 ft	W6 x 9 #2	W6 x 12 #2	W6 x 12 #2	W6 x 12 #2	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 16 #1	W8 x 18 #1	W6 x 20 #1S	W8 x 21 #1S	W10 x 26 #2	W10 x 26 #1	W10 x 26 #1	-	-	-	-	-
	11 ft	W6 x 9 #2	W6 x 12 #2	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 16 #1	W8 x 18 #1	W6 x 20 #1	W8 x 24 #1S	-	-	-	-	-	-	-	-
	12 ft	W6 x 12 #2	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W8 x 18 #1	W6 x 20 #1	W6 x 20 #1	W10 x 22 #1	-	-	-	-	-	-	-	-	-
	13 ft	W6 x 12 #1	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W8 x 18 #1	W6 x 20 #1	W8 x 21 #1	-	-	-	-	-	-	-	-	-	-	-
	14 ft	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W8 x 18 #1	W6 x 20 #1	W6 x 20 #1	W6 x 20 #1	-	-	-	-	-	-	-	-	-	-	-
	15 ft	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 16 #1	W8 x 18 #1	-	-	-	-	-	-	-	-	-	-	-	-	-	-
	16 ft	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
11 ft	7 ft	W6 x 9 #3	W6 x 9 #3	W6 x 9 #2	W8 x 10 #3	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #2S	W6 x 15 #1S	W8 x 18 #2S	W8 x 21 #2S	W10 x 22 #2	W10 x 22 #2	W10 x 26 #2	W10 x 26 #2S	W12 x 26 #2S	W14 x 30 #2S	W16 x 36 #2S
	8 ft	W6 x 9 #2	W6 x 9 #2	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #1	W6 x 15 #1S	W8 x 18 #2	W8 x 21 #2S	W8 x 21 #1S	W10 x 22 #2	W10 x 26 #2	W10 x 26 #2S	W12 x 30 #2	W14 x 30 #2	-	-
	9 ft	W6 x 9 #2	W6 x 9 #2	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #1	W6 x 15 #1	W8 x 18 #2	W6 x 20 #1S	W8 x 21 #1S	W10 x 22 #2	W10 x 26 #2	W10 x 26 #1	W10 x 26 #1S	W12 x 30 #2	-	-	-
	10 ft	W6 x 9 #2	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W8 x 18 #2	W6 x 20 #1S	W8 x 21 #1S	W10 x 26 #2	W10 x 26 #1	W10 x 26 #1	-	-	-	-	-	-
	11 ft	W6 x 9 #2	W6 x 12 #2	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W8 x 18 #1	W6 x 20 #1	W6 x 20 #1S	W8 x 21 #1	-	-	-	-	-	-	-	-
	12 ft	W6 x 12 #2	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W8 x 18 #1	W6 x 20 #1	W6 x 20 #1	W8 x 21 #1	-	-	-	-	-	-	-	-	-	-
	13 ft	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W8 x 18 #1	W6 x 20 #1	W6 x 20 #1	W6 x 20 #1	-	-	-	-	-	-	-	-	-	-	-
	14 ft	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1
	15 ft	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1
	16 ft	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12 ft	7 ft	W6 x 9 #3	W6 x 9 #3	W6 x 9 #2	W6 x 12 #2	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #2S	W8 x 18 #2S	W8 x 18 #2S	W10 x 22 #2	W10 x 22 #2	W10 x 26 #2	W10 x 26 #2S	W12 x 26 #2S	W14 x 30 #2S	W18 x 35 #2	W18 x 40 #2
	8 ft	W6 x 9 #2	W6 x 9 #2	W6 x 12 #2	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #1	W8 x 18 #2	W8 x 18 #2S	W8 x 21 #2S	W10 x 22 #2	W10 x 26 #2	W10 x 26 #2	W12 x 26 #2	W14 x 30 #2	W18 x 35 #2	-	-
	9 ft	W6 x 9 #2	W8 x 10 #2	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #1	W8 x 18 #2	W8 x 18 #2	W8 x 21 #1S	W8 x 21 #1S	W10 x 26 #2	W10 x 26 #2	W10 x 26 #1	W12 x 30 #2	-	-	-	-
	10 ft	W6 x 9 #2	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #1	W6 x 15 #1	W8 x 18 #2	W8 x 18 #1	W6 x 20 #1S	W8 x 21 #1S	W10 x 26 #2	W10 x 26 #1	W10 x 26 #1	-	-	-	-	-	-
	11 ft	W6 x 12 #2	W6 x 12 #2	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 16 #1	W8 x 18 #1	W6 x 20 #1	W8 x 21 #1	-	-	-	-	-	-	-	-	-	-
	12 ft	W6 x 12 #2	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1
	13 ft	W6 x 12 #1	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1
	14 ft	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1
	15 ft	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1
	16 ft	W6 x 15 #1	W6 x 15 #1	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
13 ft	7 ft	W6 x 9 #3	W6 x 9 #2	W6 x 9 #2	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #2S	W6 x 15 #2S	W8 x 18 #2S	W8 x 18 #2S	W8 x 21 #2S	W10 x 22 #2	W10 x 26 #2	W10 x 26 #2S	W12 x 30 #2	W14 x 34 #2	W18 x 40 #2
	8 ft	W6 x 9 #2	W6 x 9 #2	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #1	W6 x 15 #1	W8 x 18 #2	W6 x 20 #1S	W8 x 21 #1S	W10 x 22 #2	W10 x 26 #2	W10 x 26 #2	W10 x 26 #2	W12 x 30 #2	W14 x 34 #2	W18 x 40 #2	W21 x 44 #3
	9 ft	W6 x 9 #2	W6 x 12 #2	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #1	W8 x 18 #2	W8 x 18 #2	W8 x 21 #1S	W8 x 21 #1S	W10 x 26 #2	W10 x 26 #2	W10 x 26 #2	W12 x 30 #2	W14 x 30 #2	W16 x 36 #2	W18 x 40 #2	W21 x 44 #2
	10 ft	W6 x 9 #2	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #1	W6 x 15 #1	W8 x 18 #1	W8 x 18 #1	W6 x 20 #1S	W8 x 21 #1S	W10 x 26 #2	W10 x 26 #1	W10 x 26 #1	-	-	-	-	-	-
	11 ft	W6 x 12 #2	W6 x 12 #2	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 16 #1	W8 x 18 #1	W6 x 20 #1	W8 x 21 #1	-	-	-	-	-	-	-	-	-	-
	12 ft	W6 x 12 #2	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1
	13 ft	W6 x 12 #1	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1
	14 ft	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1
	15 ft	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1
	16 ft	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
14 ft	7 ft	W6 x 9 #3	W6 x 9 #2	W8 x 10 #3	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #2S	W8 x 18 #2	W8 x 18 #2S	W10 x 22 #2	W10 x 22 #2	W10 x 26 #2	W10 x 26 #2S	W12 x 26 #2S	W14 x 30 #2S	W18 x 35 #3	W21 x 44 #3	-
	8 ft	W6 x 9 #2	W6 x 9 #2	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #2	W8 x 18 #2	W8 x 18 #2S	W8 x 21 #2S	W10 x 22 #2	W10 x 26 #2	W10 x 26 #2	W12 x 26 #2	W14 x 30 #2	W18 x 35 #2	W18 x 40 #2	W21 x 44 #3
	9 ft	W6 x 9 #2	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #1	W8 x 18 #2	W8 x 18 #2	W8 x 18 #2S	W8 x 21 #2S	W10 x 22 #2	W10 x 26 #2	W10 x 26 #2	W12 x 26 #2	W14 x 30 #2	W16 x 36 #2	W18 x 40 #2	W21 x 44 #3	-
	10 ft	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #1	W6 x 15 #1	W8 x 18 #2	W8 x 18 #1	W6 x 20 #1S	W8 x 21 #1S	W10 x 26 #2	W10 x 26 #2	W10 x 26 #2	W12 x 30 #2	W16 x 36 #2	W18 x 40 #2	W21 x 44 #3	-	-
	11 ft	W6 x 12 #2	W6 x 12 #2	W6 x 12 #2	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W8 x 18 #1	W6 x 20 #1	W8 x 21 #1	W10 x 26 #2	W10 x 26 #2	W10 x 26 #2	W10 x 26 #2	W12 x 30 #2	W14 x 34 #2	W16 x 36 #2	W18 x 40 #2	-	-
	12 ft	W6 x 12 #1	W6 x 12 #1	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W8 x 18 #1	W6 x 20 #1	W6 x 20 #1	W8 x 21 #1	W10 x 26 #2	W10 x 26 #2	W10 x 26 #2	W12 x 30 #2	W14 x 34 #2	W16 x 36 #2	W18 x 40 #2	-	-
	13 ft	W6 x 12 #1	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W8 x 18 #1	W6 x 20 #1	W6 x 20 #1	W8 x 21 #1	W10 x 26 #2	W10 x 26 #2	W10 x 26 #2	W12 x 30 #2	W14 x 34 #2	W16 x 36 #2	W18 x 40 #2	-	-
	14 ft	W6 x 12 #1	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W8 x 18 #1	W6 x 20 #1	W6 x 20 #1	W8 x 21 #1	W10 x 26 #2	W10 x 26 #2	W10 x 26 #2	W12 x 30 #2	W14 x 34 #2	W16 x 36 #2	W18 x 40 #2	-	-
	15 ft	W6 x 12 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	W8 x 18 #1	W6 x 20 #1	W6 x 20 #1	W8 x 21 #1	W10 x 26 #2	W10 x 26 #2	W10 x 26 #2	W12 x 30 #2	W14 x 34 #2	W16 x 36 #2	W18 x 40 #2	-	-
	16 ft	W6 x 15 #1	W6 x 15 #1	W6 x 15 #1	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
15 ft	7 ft	-	W6 x 9 #2	W8 x 10 #3	W6 x 12 #2	W12 x 14 #3	W6 x 15 #2	W6 x 15 #2S	W6 x 15 #2S	W8 x 18 #2S	W8 x 21 #2S	W10 x 22 #2	W10 x 22 #2	W10 x 26 #2S	W12 x 26 #2	W14 x 30 #2	W18 x 35 #3	W18 x 40 #3	W21 x 44 #3	-
	8 ft	-	W8 x 10 #3	W6 x 12 #2	W12 x 14 #3	W6 x 15 #2	W6 x 15 #2	W6 x 15 #2S	W8 x 18 #2	W8 x 21 #2S	W10 x 22 #2	W10 x 22 #2	W10 x 26 #2	W12 x 26 #2	W14 x 30 #2	W18 x 35 #3	W18 x 40 #2	W21 x 44 #3	-	-
	9 ft	-	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #1	W8 x 18 #2	W6 x 20 #1S	W8 x 21 #2S	W10 x 22 #2	W10 x 26 #2	W10 x 26 #2	W12 x 30 #2	W14 x 34 #2	W18 x 40 #2	W21 x 44 #3	-	-	-
	10 ft	-	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #1	W8 x 18 #2	W6 x 20 #1S	W8 x 21 #1	W10 x 22 #2	W10 x 26 #2	W10 x 26 #2	W12 x 30 #2	W14 x 30 #2	W16 x 36 #2	W18 x 40 #2	W21 x 44 #2	-	-	-
	11 ft	-	W6 x 15 #2	W6 x 15 #1	W6 x 15 #1	W8 x 18 #2	W6 x 20 #1	W8 x 21 #1	W10 x 22 #2	W10 x 26 #2	W10 x 26 #1	W10 x 30 #1	W14 x 30 #2	W16 x 36 #2	W18 x 40 #2	W21 x 44 #2	-	-	-	-
	12 ft	-	W6 x 15 #1	W6 x 15 #1	W6 x 16 #1	W8 x 18 #1	W6 x 20 #1	W8 x 21 #1	W10 x 26 #2	W10 x 26 #1	W10 x 30 #1	W12 x 30 #2	W14 x 34 #2	W18 x 40 #2	W21 x 44 #2	-	-	-	-	-
	13 ft	-	W6 x 15 #1	W6 x 15 #1	W8 x 18 #1	W6 x 20 #1	W8 x 21 #1	W10 x 26 #1	W10 x 26 #1	W10 x 26 #1	W12 x 30 #2	W14 x 34 #2	W16 x 36 #2	W18 x 40 #2	-	-	-	-	-	-
	14 ft	-	W6 x 15 #1	W8 x 18 #1	W6 x 20 #1	W6 x 20 #1	W8 x 24 #1	W10 x 26 #1	W10 x 26 #1	W10 x 30 #1	W12 x 30 #1	W14 x 34 #1	W16 x 36 #2	W18 x 40 #2	-	-	-	-	-	-
	15 ft	-	W6 x 16 #1	W8 x 18 #1	W6 x 20 #1	W8 x 24 #1	W10 x 26 #1	W10 x 30 #1	W12 x 30 #1	W14 x 34 #1	-	-	-	-	-	-	-	-	-	-
	16 ft	-	W8 x 18 #1	W6 x 20 #1	W6 x 20 #1	W8 x 24 #1	W10 x 26 #1	W10 x 30 #1	W10 x 30 #1	W14 x 34 #1	-	-	-	-	-	-	-	-	-	-

REV.	DATE	REVISION DESCRIPTION	SHEET NO.
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-	-	-	-
-	-	-	-
-	-	-	-
-	-	-	-
-	-	-	-

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 12/4/2013

DESIGNER/DRAFTER: BKC

CHECKED BY: JRH

SCALE AS NOTED

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\SB_Breakaway_Signpost_BSM2_PostSelectionTable_1.dgn

SIGNATURE/BLOCK: OFFICE OF ENGINEERING

APPROVED BY: DATE:

PROJECT TITLE:

TOWN:

DRAWING TITLE: BREAKAWAY SIGN SUPPORTS POST SELECTION TABLE 1

PROJECT NO.

DRAWING NO. BSM-2

SHEET NO.

POST SELECTION TABLE 2

W	L	H (Sign Height + Crown Height)																		
		4 ft	5 ft	6 ft	7 ft	8 ft	9 ft	10 ft	11 ft	12 ft	13 ft	14 ft	15 ft	16 ft	17 ft	18 ft	19 ft	20 ft	21 ft	22 ft
16 ft	7 ft	-	W6 x 9 #2	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #2S	W8 x 18 #2	W8 x 18 #2S	W10 x 22 #2	W10 x 22 #2	W10 x 26 #2	W10 x 26 #2S	W12 x 26 #2S	W18 x 35 #3	W18 x 35 #3	W21 x 44 #3	-	-
	8 ft	-	W8 x 10 #3	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #2	W6 x 16 #2S	W8 x 18 #2	W8 x 21 #2S	W10 x 22 #2	W10 x 26 #2	W10 x 26 #2	W12 x 26 #2	W14 x 30 #2	W18 x 35 #3	W21 x 44 #3	-	-	-
	9 ft	-	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #2	W6 x 16 #1	W8 x 18 #2	W8 x 21 #2	W10 x 22 #2	W10 x 26 #2	W10 x 26 #2	W12 x 26 #2	W14 x 30 #2	W18 x 35 #2	W18 x 40 #2	W21 x 44 #3	-	-	-
	10 ft	-	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 16 #1	W8 x 18 #2	W8 x 21 #2	W8 x 21 #1	W10 x 26 #2	W10 x 26 #2	W12 x 26 #2	W14 x 30 #2	W16 x 36 #2	W18 x 40 #2	W21 x 44 #3	-	-	-	-
	11 ft	-	W6 x 15 #2	W6 x 15 #1	W6 x 15 #1	W8 x 18 #2	W6 x 20 #1	W8 x 21 #1	W10 x 26 #2	W10 x 26 #2	W10 x 26 #1	W12 x 30 #2	W14 x 34 #2	W18 x 40 #2	W21 x 44 #3	-	-	-	-	-
	12 ft	-	W6 x 15 #1	W6 x 15 #1	W8 x 18 #2	W6 x 20 #1	W8 x 21 #1	W10 x 26 #2	W10 x 26 #2	W10 x 26 #1	W12 x 30 #2	W14 x 34 #2	W16 x 36 #2	W18 x 40 #2	-	-	-	-	-	-
	13 ft	-	W6 x 15 #1	W6 x 16 #1	W6 x 20 #1	W6 x 20 #1	W10 x 22 #2	W10 x 26 #1	W10 x 26 #1	W10 x 30 #1	W14 x 34 #2	W16 x 36 #2	W18 x 40 #2	-	-	-	-	-	-	-
	14 ft	-	W6 x 15 #1	W8 x 18 #1	W6 x 20 #1	W8 x 21 #1	W10 x 26 #1	W10 x 26 #1	W10 x 30 #1	W12 x 30 #1	W14 x 34 #2	W18 x 40 #2	-	-	-	-	-	-	-	-
	15 ft	-	W8 x 18 #1	W6 x 20 #1	W6 x 20 #1	W8 x 24 #1	W10 x 26 #1	W10 x 30 #1	W10 x 30 #1	W14 x 34 #2	W16 x 36 #2	-	-	-	-	-	-	-	-	-
	16 ft	-	W8 x 18 #1	W6 x 20 #1	W8 x 24 #1	W10 x 26 #1	W10 x 26 #1	W10 x 30 #1	W12 x 30 #1	W14 x 34 #1	-	-	-	-	-	-	-	-	-	-
17 ft	7 ft	-	W6 x 9 #2	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #2S	W8 x 18 #2S	W8 x 21 #2S	W10 x 22 #2	W10 x 22 #2	W10 x 26 #2S	W12 x 26 #2	W14 x 30 #2	W18 x 35 #3	W21 x 44 #3	-	-	-
	8 ft	-	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #2	W8 x 18 #2	W8 x 21 #2S	W10 x 22 #2	W10 x 22 #2	W10 x 26 #2	W12 x 26 #2	W14 x 30 #2	W18 x 35 #3	W18 x 40 #3	W21 x 44 #3	-	-	-
	9 ft	-	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #2	W8 x 18 #2	W6 x 20 #1S	W8 x 21 #2S	W10 x 22 #2	W10 x 26 #2	W10 x 26 #2	W14 x 30 #2	W18 x 35 #3	W18 x 40 #2	W21 x 44 #3	-	-	-	-
	10 ft	-	W6 x 12 #2	W6 x 15 #2	W6 x 15 #1	W8 x 18 #2	W6 x 20 #1	W8 x 21 #2	W10 x 22 #2	W10 x 26 #2	W10 x 26 #2	W12 x 30 #2	W14 x 30 #2	W16 x 36 #2	W18 x 40 #2	W21 x 44 #3	-	-	-	-
	11 ft	-	W6 x 15 #2	W6 x 15 #1	W6 x 16 #1	W8 x 18 #2	W8 x 21 #2	W10 x 22 #2	W10 x 26 #2	W10 x 26 #2	W12 x 30 #2	W14 x 30 #2	W16 x 36 #2	W18 x 40 #2	W21 x 44 #3	-	-	-	-	-
	12 ft	-	W6 x 15 #1	W6 x 15 #1	W8 x 18 #2	W6 x 20 #1	W8 x 21 #1	W10 x 26 #2	W10 x 26 #2	W10 x 30 #1	W14 x 34 #2	W16 x 36 #2	W18 x 40 #2	W21 x 44 #2	-	-	-	-	-	-
	13 ft	-	W6 x 15 #1	W8 x 18 #1	W6 x 20 #1	W8 x 21 #1	W10 x 26 #1	W10 x 26 #1	W12 x 30 #2	W14 x 34 #2	W16 x 36 #2	W18 x 40 #2	W21 x 44 #2	-	-	-	-	-	-	-
	14 ft	-	W6 x 15 #1	W8 x 18 #1	W6 x 20 #1	W8 x 24 #1	W10 x 26 #1	W10 x 26 #1	W12 x 30 #2	W14 x 34 #2	W16 x 36 #2	W18 x 40 #2	-	-	-	-	-	-	-	-
	15 ft	-	W8 x 18 #1	W6 x 20 #1	W8 x 21 #1	W10 x 26 #1	W10 x 26 #1	W10 x 30 #1	W12 x 30 #1	W16 x 36 #2	-	-	-	-	-	-	-	-	-	-
	16 ft	-	W6 x 20 #1	W6 x 20 #1	W8 x 24 #1	W10 x 26 #1	W10 x 30 #1	W12 x 30 #1	W14 x 34 #1	-	-	-	-	-	-	-	-	-	-	-
18 ft	7 ft	-	W8 x 10 #3	W6 x 12 #2	W12 x 14 #3	W6 x 15 #2	W6 x 15 #2S	W6 x 15 #2S	W8 x 18 #2S	W8 x 21 #2S	W10 x 22 #2	W10 x 26 #2	W12 x 26 #2	W14 x 30 #2	W18 x 35 #3	W18 x 35 #3	W21 x 44 #3	-	-	-
	8 ft	-	W6 x 12 #2	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #2S	W8 x 18 #2	W8 x 21 #2S	W10 x 22 #2	W10 x 26 #2	W10 x 26 #2	W12 x 26 #2	W18 x 35 #3	W18 x 35 #3	W21 x 44 #3	-	-	-	-
	9 ft	-	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #2	W8 x 18 #2	W8 x 21 #2	W10 x 22 #2	W10 x 26 #2	W10 x 26 #2	W12 x 26 #2	W14 x 30 #2	W18 x 35 #3	W21 x 44 #3	-	-	-	-	-
	10 ft	-	W6 x 15 #2	W6 x 15 #1	W8 x 18 #2	W8 x 21 #2	W8 x 21 #1	W10 x 22 #2	W10 x 26 #2	W10 x 26 #2	W12 x 26 #2	W14 x 30 #2	W18 x 35 #2	W18 x 40 #2	W21 x 44 #3	-	-	-	-	-
	11 ft	-	W6 x 15 #2	W6 x 15 #1	W8 x 18 #2	W6 x 20 #1	W8 x 21 #1	W10 x 22 #2	W10 x 26 #2	W10 x 26 #2	W12 x 30 #2	W14 x 30 #2	W16 x 36 #2	W18 x 40 #2	W21 x 44 #3	-	-	-	-	-
	12 ft	-	W6 x 15 #1	W6 x 15 #1	W6 x 16 #1	W8 x 18 #2	W8 x 21 #2	W10 x 22 #2	W10 x 26 #2	W10 x 30 #1	W14 x 34 #2	W16 x 36 #2	W18 x 40 #2	W21 x 44 #2	-	-	-	-	-	-
	13 ft	-	W6 x 15 #1	W8 x 18 #1	W6 x 20 #1	W8 x 21 #1	W10 x 26 #2	W10 x 26 #2	W12 x 30 #2	W14 x 34 #2	W16 x 36 #2	W18 x 40 #2	W21 x 44 #2	-	-	-	-	-	-	-
	14 ft	-	W6 x 15 #1	W8 x 18 #1	W6 x 20 #1	W8 x 24 #1	W10 x 26 #1	W10 x 26 #1	W12 x 30 #2	W14 x 34 #2	W16 x 36 #2	W18 x 40 #2	-	-	-	-	-	-	-	-
	15 ft	-	W6 x 16 #1	W6 x 20 #1	W6 x 20 #1	W10 x 26 #1	W10 x 26 #1	W10 x 30 #1	W12 x 30 #2	W14 x 34 #2	W16 x 36 #2	W18 x 40 #2	-	-	-	-	-	-	-	-
	16 ft	-	W8 x 18 #1	W6 x 20 #1	W8 x 24 #1	W10 x 26 #1	W10 x 26 #1	W10 x 30 #1	W12 x 30 #1	W14 x 34 #2	W16 x 36 #2	-	-	-	-	-	-	-	-	-
19 ft	7 ft	-	-	W6 x 12 #2	W12 x 14 #3	W6 x 15 #2	W6 x 15 #2S	W8 x 18 #2	W8 x 18 #2S	W10 x 22 #2	W10 x 22 #2	W10 x 26 #2	W12 x 26 #2	W14 x 30 #2	W18 x 35 #3	W21 x 44 #3	-	-	-	-
	8 ft	-	-	W12 x 14 #3	W6 x 15 #2	W6 x 15 #2	W6 x 16 #2S	W8 x 18 #2	W10 x 22 #2	W10 x 22 #2	W10 x 26 #2	W12 x 26 #2	W14 x 30 #2	W18 x 35 #3	W21 x 44 #3	W21 x 44 #3	-	-	-	-
	9 ft	-	-	W6 x 15 #2	W6 x 15 #2	W6 x 16 #2	W8 x 18 #2	W8 x 21 #2	W10 x 22 #2	W10 x 26 #2	W12 x 26 #2	W14 x 30 #2	W18 x 35 #3	W18 x 35 #3	W21 x 44 #3	-	-	-	-	-
	10 ft	-	-	W6 x 15 #2	W6 x 15 #1	W8 x 18 #2	W8 x 21 #2	W10 x 22 #2	W10 x 26 #2	W10 x 26 #2	W12 x 26 #2	W18 x 35 #3	W18 x 40 #3	W21 x 44 #3	-	-	-	-	-	-
	11 ft	-	-	W6 x 15 #1	W8 x 18 #2	W6 x 20 #1	W8 x 21 #1	W10 x 26 #2	W10 x 26 #2	W12 x 26 #2	W14 x 30 #2	W16 x 36 #2	W21 x 44 #3	-	-	-	-	-	-	-
	12 ft	-	-	W8 x 18 #2	W6 x 20 #1	W8 x 21 #1	W10 x 26 #2	W10 x 26 #2	W10 x 26 #1	W14 x 30 #2	W16 x 36 #2	W18 x 40 #2	W21 x 44 #3	-	-	-	-	-	-	-
	13 ft	-	-	W8 x 18 #1	W6 x 20 #1	W10 x 22 #2	W10 x 26 #2	W10 x 26 #1	W12 x 30 #2	W16 x 36 #2	W18 x 40 #2	W21 x 44 #2	-	-	-	-	-	-	-	-
	14 ft	-	-	W6 x 20 #1	W8 x 21 #1	W10 x 26 #1	W10 x 26 #1	W12 x 30 #2	W14 x 34 #2	W16 x 36 #2	-	-	-	-	-	-	-	-	-	-
	15 ft	-	-	W6 x 20 #1	W8 x 24 #1	W10 x 26 #1	W10 x 30 #1	W12 x 30 #2	W16 x 36 #2	-	-	-	-	-	-	-	-	-	-	-
	16 ft	-	-	W8 x 24 #1	W10 x 26 #1	W10 x 30 #1	W12 x 30 #1	W16 x 36 #2	-	-	-	-	-	-	-	-	-	-	-	-
20 ft	7 ft	-	-	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 15 #2S	W8 x 18 #2S	W8 x 21 #2S	W10 x 22 #2	W10 x 22 #2	W12 x 26 #2	W14 x 30 #3	W16 x 31 #3	W18 x 35 #3	W21 x 44 #3	-	-	-	-
	8 ft	-	-	W6 x 15 #2	W6 x 15 #2	W6 x 15 #2	W8 x 18 #2	W8 x 21 #2S	W10 x 22 #2	W10 x 22 #2	W10 x 26 #2	W12 x 26 #2	W18 x 35 #3	W18 x 35 #3	W21 x 44 #3	-	-	-	-	-
	9 ft	-	-	W6 x 15 #2	W6 x 15 #2	W8 x 18 #2	W8 x 21 #2	W10 x 22 #2	W10 x 22 #2	W10 x 26 #2	W12 x 26 #2	W14 x 34 #2	W18 x 35 #3	W21 x 44 #3	-	-	-	-	-	-
	10 ft	-	-	W6 x 15 #2	W6 x 16 #1	W8 x 18 #2	W8 x 21 #2	W10 x 22 #2	W10 x 26 #2	W12 x 26 #2	W14 x 30 #2	W18 x 35 #3	W21 x 44 #3	W21 x 44 #3	-	-	-	-	-	-
	11 ft	-	-	W6 x 15 #1	W8 x 18 #2	W8 x 21 #2	W10 x 22 #2	W10 x 26 #2	W10 x 26 #2	W12 x 30 #2	W18 x 35 #2	W18 x 40 #2	W21 x 44 #3	-	-	-	-	-	-	-
	12 ft	-	-	W8 x 18 #2	W6 x 20 #1	W8 x 21 #1	W10 x 26 #2	W10 x 26 #2	W12 x 30 #2	W14 x 30 #2	W18 x 40 #2	W21 x 44 #3	-	-	-	-	-	-	-	-
	13 ft	-	-	W6 x 20 #1	W8 x 21 #1	W10 x 26 #2	W10 x 26 #2	W10 x 30 #1	W14 x 30 #2	W16 x 36 #2	W18 x 40 #2	W21 x 44 #2	-	-	-	-	-	-	-	-
	14 ft	-	-	W6 x 20 #1	W8 x 21 #1	W10 x 26 #1	W10 x 26 #1	W12 x 30 #2	W16 x 36 #2	W18 x 40 #2	-	-	-	-	-	-	-	-	-	-
	15 ft	-	-	W6 x 20 #1	W10 x 26 #1	W10 x 26 #1	W10 x 30 #1	W14 x 34 #2	W16 x 36 #2	-	-	-	-	-	-	-	-	-	-	-
	16 ft	-	-	W8 x 24 #1	W10 x 26 #1	W10 x 30 #1	W12 x 30 #1	W16 x 36 #2	-	-	-	-	-	-	-	-	-	-	-	-
21 ft	7 ft	-	-	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W6 x 16 #2S	W8 x 18 #2S	W10 x 22 #2	W10 x 22 #2	W10 x 26 #2	W12 x 26 #2	W16 x 31 #3	W18 x 35 #3	W21 x 44 #3	-	-	-	-	-
	8 ft	-	-	W6 x 15 #2	W6 x 15 #2	W6 x 15 #2	W8 x 18 #2	W8 x 21 #2S	W10 x 22 #2	W10 x 26 #2	W12 x 26 #2	W14 x 30 #2	W18 x 35 #3	W21 x 44 #3	-	-	-	-	-	-
	9 ft	-	-	W6 x 15 #2	W6 x 15 #2	W8 x 18 #2	W8 x 21 #2	W10 x 22 #2	W10 x 26 #2	W12 x 26 #2	W14 x 30 #2	W18 x 35 #3	W18 x 35 #3	W21 x 44 #3	-	-	-	-	-	-
	10 ft	-	-	W6 x 15 #2	W8 x 18 #2	W6 x 20 #1	W10 x 22 #2	W10 x 26 #2	W10 x 26 #2	W12 x 26 #2	W18 x 35 #3	W18 x 35 #3	W21 x 44 #3	-	-	-	-	-	-	-
	11 ft	-	-	W6 x 15 #1	W8 x 18 #2	W8 x 21 #2	W10 x 22 #2	W10 x 26 #2	W12 x 26 #2	W14 x 30 #2	W16 x 40 #2	W21 x 44 #3	-	-	-	-	-	-	-	-
	12 ft	-	-	W8 x 18 #2	W6 x 20 #1	W8 x 21 #1	W10 x 26 #2	W10 x 26 #2	W12 x 30 #2	W14 x 30 #2	W16 x 36 #2	W18 x 40 #2	W21 x 44 #3	-	-	-	-	-	-	-
	13 ft	-	-	W6 x 20 #1	W8 x 21 #1	W10 x 26 #2	W10 x 26 #2	W12 x 30 #2	W14 x 30 #2	W16 x 36 #2	W18 x 40 #2	W21 x 44 #2	-	-	-	-	-	-	-	-
	14 ft	-	-	W6 x 20 #1	W8 x 21 #1	W10 x 26 #2	W10 x 26 #2	W12 x 30 #2	W14 x 34 #2	W16 x 36 #2	W18 x 40 #2	-	-	-	-	-	-	-	-	-
	15 ft	-	-	W6 x 20 #1	W10 x 26 #1	W10 x 26 #1	W10 x 30 #1	W14 x 34 #2	W16 x 36 #2	-	-	-	-	-	-	-	-	-	-	-
	16 ft	-	-	W8 x 24 #1	W10 x 26 #1	W10 x 30 #1	W12 x 30 #1	W16 x 36 #2	-	-	-	-	-	-	-	-	-	-	-	-
22 ft	7 ft	-	-	W6 x 12 #2	W6 x 15 #2	W6 x 15 #2	W8 x 18 #2	W8 x 18 #2S	W10 x 22 #2	W10 x 22 #2	W10 x 26 #2	W12 x 26 #3	W16 x 26 #3	W16 x 31 #3	W18 x 35 #3	W21 x 44 #3	-	-	-	-
	8 ft	-	-	W6 x 15 #2	W6 x 15 #2	W6 x 15 #2	W8 x 18 #2	W10 x 22 #2	W10 x 22 #2	W10 x 26 #2	W12 x 26 #2	W16 x 31 #3	W18 x 35 #3	W21 x 44 #3	-	-	-	-	-	-
	9 ft	-	-	W6 x 15 #2	W6 x 15 #2	W8 x 18 #2	W8 x 21 #2	W10 x 22 #2	W10 x 26 #2	W12 x 26 #2	W14 x 30 #2	W18 x 35 #3	W21 x 44 #3	-	-	-	-	-	-	-
	10 ft	-	-	W6 x 15 #2	W8 x 18 #2	W8 x 21 #2	W10 x 22 #2	W10 x 26 #2	W12 x 26 #2	W14 x 30 #2	W18 x 35 #3	W21 x 44 #3	-	-	-	-	-	-	-	-
	11 ft	-	-	W6 x 16 #1	W6 x 20 #1	W8 x 21 #2	W10 x 26 #2	W12 x 26 #2	W18 x 35 #3	W18 x 40 #3	W21 x 44 #3	-	-	-	-	-	-	-	-	-
	12 ft	-	-	W8 x 18 #2	W8 x 21 #2	W10 x 22 #2	W10 x 26 #2	W12 x 26 #2	W14 x 30 #2	W18 x 40 #2	W21 x 44 #3	-	-	-	-	-	-	-	-	-
	13 ft	-	-	W6 x 20 #1	W8 x 21 #1	W10 x 26 #2	W10 x 26 #1	W12 x 30 #2	W16 x 36 #2	W18 x 40 #2	-	-	-	-	-	-	-	-	-	-
	14 ft	-	-	W6 x 20 #1	W10 x 26 #2	W10 x 26 #1	W12 x 30 #2	W14 x 34 #2	W18 x 40 #2	-	-	-	-	-	-	-	-	-	-	-
	15 ft	-	-	W10 x 22 #1	W10 x 26 #1	W10 x 30 #1	W12 x 30 #2	W16 x 36 #2	-	-	-	-	-	-	-	-	-	-	-	-
	16 ft	-	-	W10 x 26 #1	W10 x 26 #1	W10 x 30 #1	W14 x 34 #2	-	-	-	-	-	-	-	-	-	-	-	-	-

REV.	DATE	REVISION DESCRIPTION	SHEET NO.
-	-	-	-
-	-	-	-
-	-	-	-
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-	-	-	-
-	-	-	-

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 12/4/2013

DESIGNER/DRAFTER: BKC
CHECKED BY: JRH
SCALE AS NOTED

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\SB_Breakaway_Signpost_BSM3_PostSelectionTable_2.dgn

SIGNATURE/ BLOCK:
OFFICE OF ENGINEERING
APPROVED BY: DATE:

PROJECT TITLE:

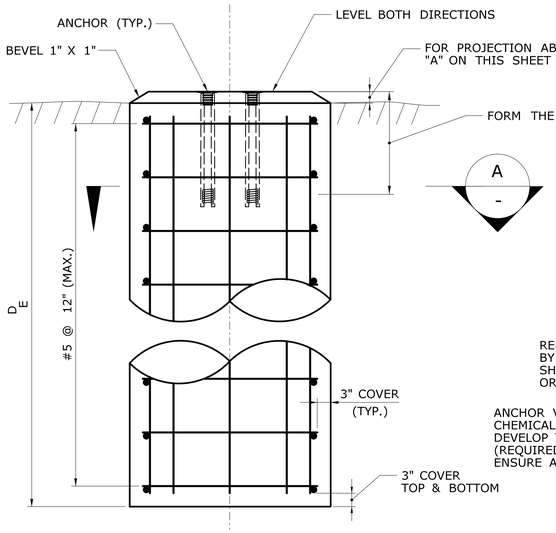
TOWN:

DRAWING TITLE:
BREAKAWAY SIGN SUPPORTS
POST SELECTION TABLE 2

PROJECT NO.

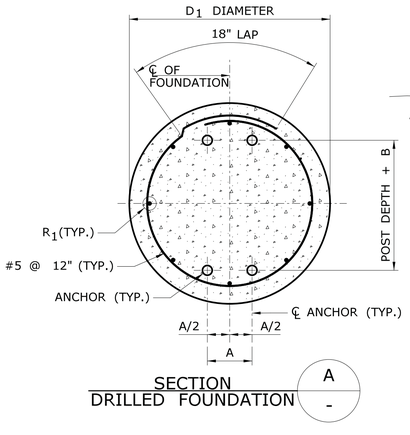
DRAWING NO.
BSM-3

SHEET NO.

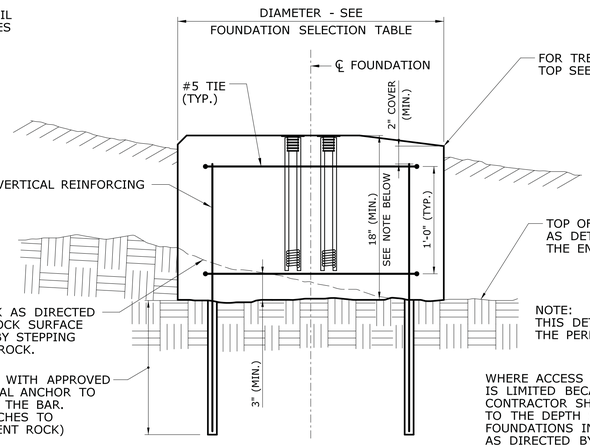


TYPICAL SECTION SIGN SUPPORT FOUNDATION IN EARTH

SCALE: $\frac{3}{4}$" = 1'-0"



SECTION DRILLED FOUNDATION

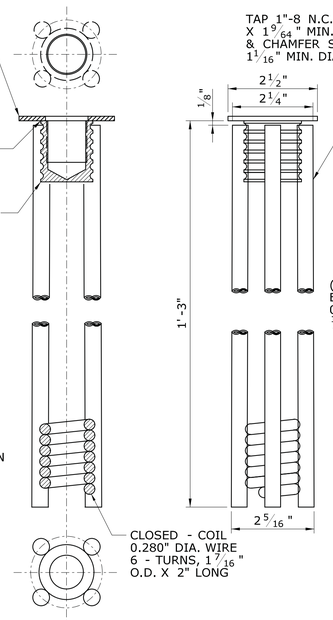


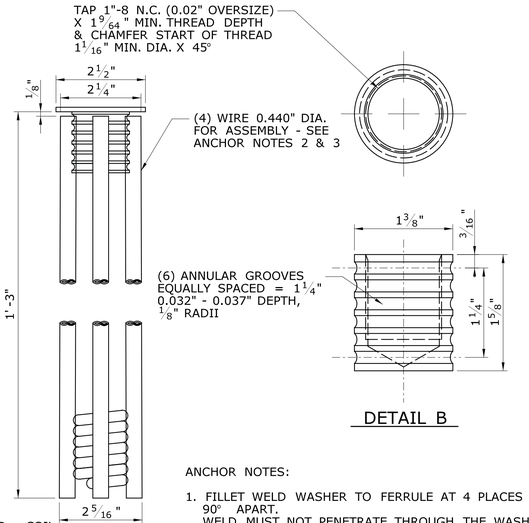
TYPICAL SECTION SIGN SUPPORT FOUNDATION IN ROCK

SCALE: $1\frac{1}{2}$" = 1'-0"

NOTE:
THIS DETAIL MAY BE USED ONLY WITH THE PERMISSION OF THE ENGINEER.

WHERE ACCESS TO THE BOTTOM OF THE HOLE IS LIMITED BECAUSE OF DEPTH TO ROCK, THE CONTRACTOR SHALL CREATE A ROCK SOCKET TO THE DEPTH SPECIFIED ON THE PLANS FOR FOUNDATIONS IN EARTH OR RELOCATE THE SIGN AS DIRECTED BY THE ENGINEER

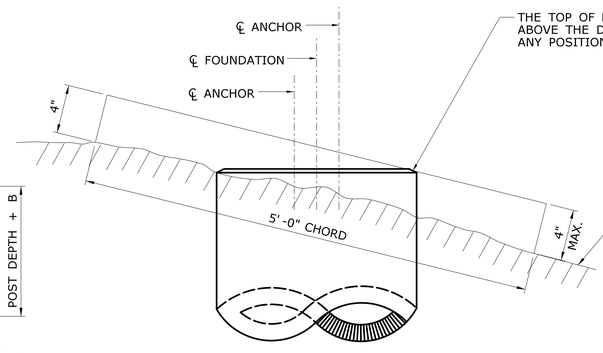




DETAIL B

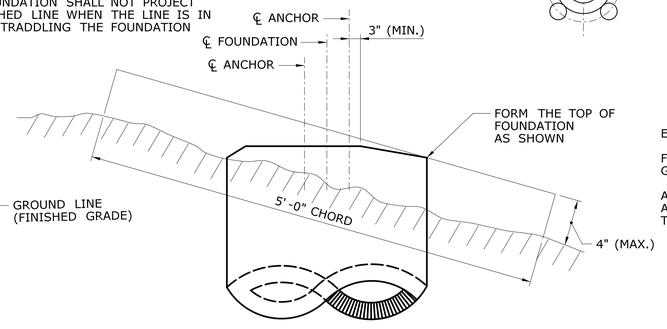
ANCHOR NOTES:

1. FILLET WELD WASHER TO FERRULE AT 4 PLACES 90° APART. WELD MUST NOT PENETRATE THROUGH THE WASHER. WASHER MUST BE PERPENDICULAR TO THE FERRULE CENTER LINE.
2. WIRES (4) TO BE PERCUSSION WELDED TO FERRULE AND TO THE CLOSED COIL.
3. WELD MUST NOT PENETRATE TO FERRULE INTERIOR THREADS.
4. WIRE TO BE DRAWN PER ASTM A510.
5. CHEMICAL & PHYSICAL CERTIFICATION SHOULD ACCOMPANY THE MATERIAL.
6. CERTIFICATION SHOULD EXPLICITLY INDICATE THE MATERIAL TO BE DOMESTIC.
7. TOLERANCES ON DECIMAL DIMENSIONS SHALL BE ± 0.004". ALL OTHER TOLERANCES SHALL BE ± 0.04", EXCEPT AS NOTED.



STANDARD FOUNDATION TOP

THE TOP OF FOUNDATION SHALL NOT PROJECT ABOVE THE DASHED LINE WHEN THE LINE IS IN ANY POSITION STRADDLING THE FOUNDATION



MODIFIED FOUNDATION TOP

(SEE FOUNDATION NOTE 3)

DETAIL A - PROJECTION OF FOUNDATION ABOVE GROUND

SELECTING A FOUNDATION:

ENTER THE "FOUNDATION SELECTION TABLE" WITH THE POST SIZE AND BRACKET NO. SELECTED FROM THE "POST SELECTION TABLE 1 OR 2" ON DWG BSM-2 OR DWG BSM-3. READ HORIZONTALLY ACROSS THE TABLE THE CORRESPONDING VALUES OF FOUNDATION DIAMETER, EMBEDMENT DEPTH, REINFORCING BAR SIZE, ANCHOR SPACING AND DIMENSION "B".

FOUNDATION SELECTION TABLE FOR BREAKAWAY SIGNS

POST SIZE	DIAMETER D_I (FT.)	DEPTH D_E (FT.)	REINF. STEEL R_I	ANCHOR SPACING A (IN.)	B (IN.) BRACKET NO. 1	2	3
W6 W8	2.5	8	8 - #5	3	7 15/16	8 1/16	8 1/8
W10 W12	2.5	8	8 - #5	4	7 15/16	8 1/16	8 1/8
W14 W16 W18 W21	3.25	8	8 - #6	4	7 15/16	8 1/16	8 1/8

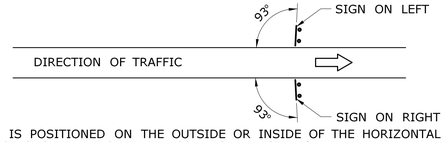
FOUNDATION NOTES

1. DETAIL A ILLUSTRATES THE METHOD USED TO MEASURE THE PROJECTION OF THE FOUNDATION ABOVE FINISHED GRADE. IT IS IMPORTANT THAT THE TOP OF THE FOUNDATION BE PLACED IN ACCORDANCE WITH THIS DETAIL.
2. THE TOP OF FOUNDATION SHALL BE CONSTRUCTED AS CLOSE TO THE FINISHED GRADE AS POSSIBLE, BUT SHOULD NOT BE COVERED BY SOIL.
3. USE A MODIFIED TOP WHERE PROJECTION LIMITS CANNOT BE MET WITH THE STANDARD TOP.
4. FOUNDATIONS SHALL BE PLACED AGAINST UNDISTURBED SOIL. WHERE ROCK IS ENCOUNTERED, THE CONTRACTOR MAY USE THE "SIGN SUPPORT FOUNDATION IN ROCK" DETAIL SHOWN ON THIS SHEET WITH THE PERMISSION OF THE ENGINEER.
5. IF UNSUITABLE SOIL IS ENCOUNTERED DURING EXCAVATION, THE ENGINEER SHALL BE NOTIFIED. AN ALTERNATE FOUNDATION DESIGN MAY BE SUPPLIED BY THE ENGINEER, OR THE SIGN MAY BE RELOCATED.
6. PLACEMENT OF FOUNDATIONS SHALL BE IN ACCORDANCE WITH "SIGN SUPPORT PLACEMENT DETAILS" ON THIS SHEET.
7. WHERE FOUNDATIONS ARE PLACED ON SLOPES STEEPER THAN 1V : 6H, GRADE AROUND THE FOUNDATIONS IN CONFORMANCE WITH DETAIL A.

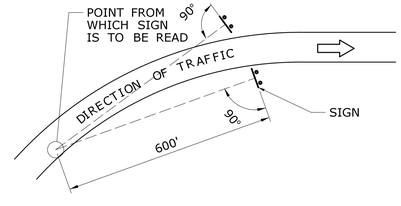
ERECTION NOTE:

FOR MAXIMUM EFFECTIVENESS AND TO ELIMINATE OR MINIMIZE SPECULAR GLARE, POSITION SIDE MOUNTED SIGNS AS FOLLOWS:

A. ON TANGENT SECTION, POSITION THE SIGN SUCH THAT THE VERTICAL AXIS IS PLUMB AND THE HORIZONTAL AXIS IS AT AN ANGLE OF 93° WITH THE TRAFFIC LANE WHICH THE SIGN SERVES (SEE DIAGRAM).



B. WHERE THE SIGN IS POSITIONED ON THE OUTSIDE OR INSIDE OF THE HORIZONTAL CURVE, THE SIGN FACE SHOULD BE ORIENTED 90° TO THE STRAIGHT LINE BETWEEN THE SIGN AND THE POINT FROM WHICH THE SIGN IS TO BE READ AT THE DISTANCE SHOWN.



SIGN SUPPORT PLACEMENT DETAILS

REV.	DATE	REVISION DESCRIPTION	SHEET NO.
-	-	-	-
-	-	-	-
-	-	-	-
-	-	-	-
-	-	-	-
-	-	-	-

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Plotted Date: 12/4/2013

DESIGNER/DRAFTER: **BKC**
CHECKED BY: **JRH**
SCALE AS NOTED

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\SB_Breakaway_Signpost_BSM4_FoundationDet.dgn

SIGNATURE/ BLOCK:
OFFICE OF ENGINEERING
APPROVED BY: DATE:

PROJECT TITLE:

TOWN:

DRAWING TITLE:
BREAKAWAY SIGN SUPPORTS FOUNDATION DETAILS

PROJECT NO.

DRAWING NO.
BSM-4

SHEET NO.

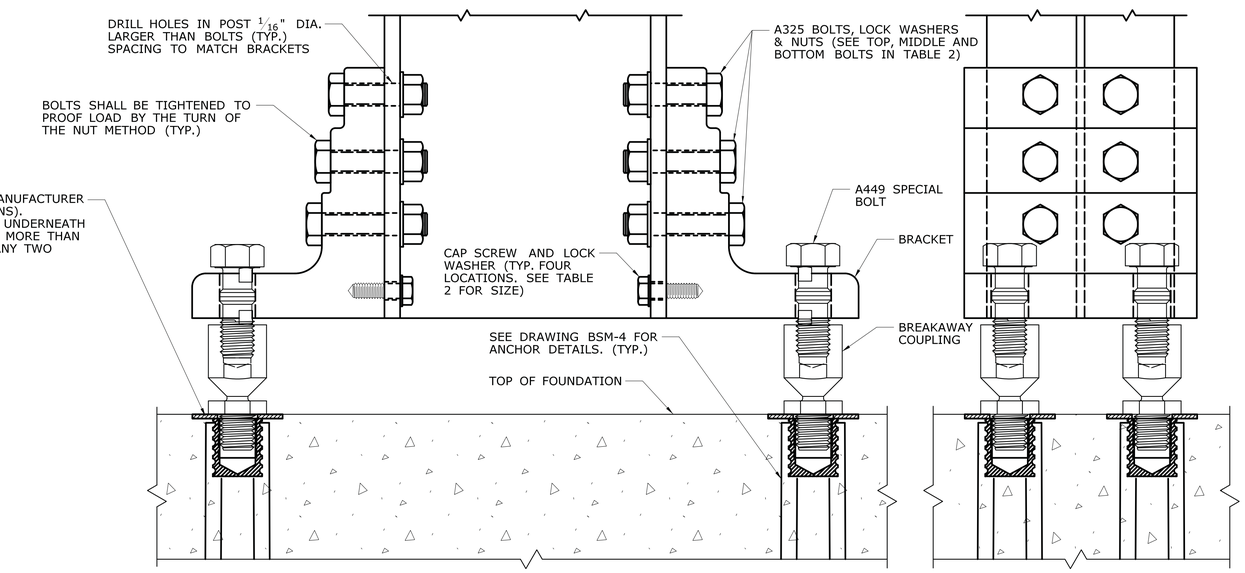
TABLE 1 - BRACKET DATA

POST SIZE	BRACKET TYPE	BRACKET WEIGHT (LBS)	DIMENSIONS (IN.)			HOLE DIAMETERS (IN.)			DIMENSIONS (IN.) E BRACKET NO.			F	G
			A	B	C	D1	D2	D3	1	2	3		
W6, W8	B525	$7\frac{5}{8}$	$5\frac{1}{4}$	$1\frac{1}{8}$	$1\frac{1}{2}$	$\frac{17}{32}$	$\frac{7}{16}$	$\frac{1}{2}$" -13 UNC 1A	0.100	0.150	0.200	$1\frac{7}{8}$	$\frac{1}{2}$
ALL OTHERS	B650	$9\frac{1}{2}$	$6\frac{1}{2}$	$1\frac{1}{8}$	2	$\frac{21}{32}$	$\frac{17}{32}$	$\frac{5}{8}$"-11 UNC 1A	0.100	0.150	0.200	$2\frac{7}{8}$	$\frac{1}{2}$

TABLE 2 - BRACKET BOLTS

POST SIZE	BRACKET TYPE	BOLT AND CAP SCREW DIAMETER	BOLT LENTGH			CAP SCREW LENGTH	THREAD DESIGNATION (U.S. CUSTOMARY UNITS)	
			TOP	MIDDLE	BOTTOM		BOLT	CAP SCREW
W6, W8	B525	$\frac{1}{2}$	$2\frac{1}{2}$	$2\frac{3}{4}$	3	$1\frac{1}{4}$	13 UNC	13 UNC
ALL OTHERS	B650	$\frac{5}{8}$	$2\frac{3}{4}$	3	$3\frac{1}{4}$	$1\frac{1}{4}$	11 UNC	11 UNC

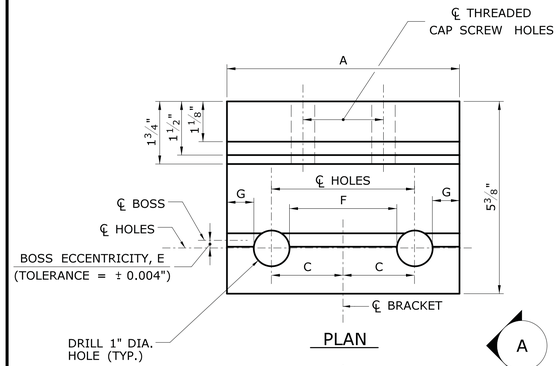
SHIM AS REQUIRED WITH MANUFACTURER SUPPLIED SHIMS (4 LOCATIONS). NO MORE THAN TWO SHIMS UNDERNEATH ANY ONE COUPLING AND NO MORE THAN THREE SHIMS UNDERNEATH ANY TWO COUPLINGS.



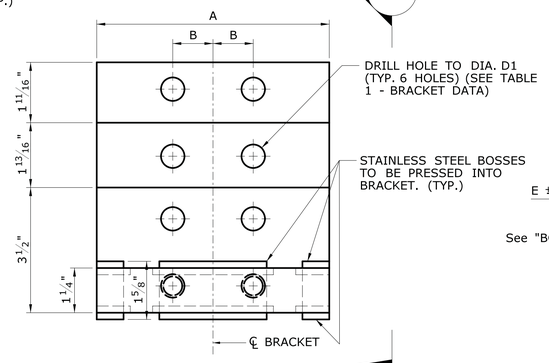
SIDE ELEVATION

FRONT ELEVATION

BRACKET ASSEMBLY DETAILS

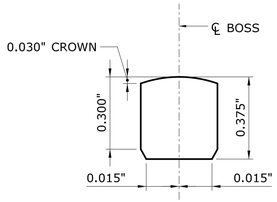


PLAN

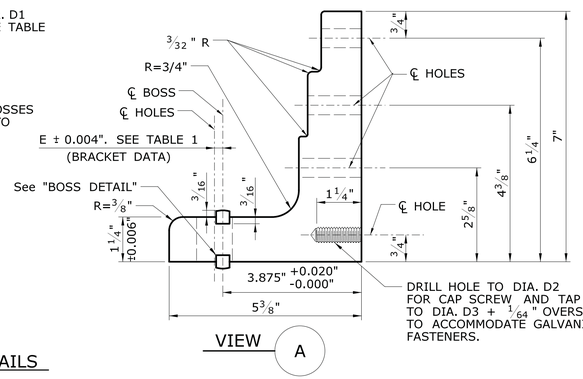


ELEVATION

A



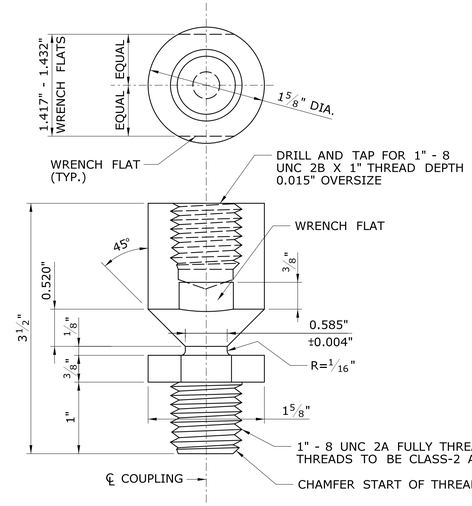
BOSS DETAIL



VIEW A

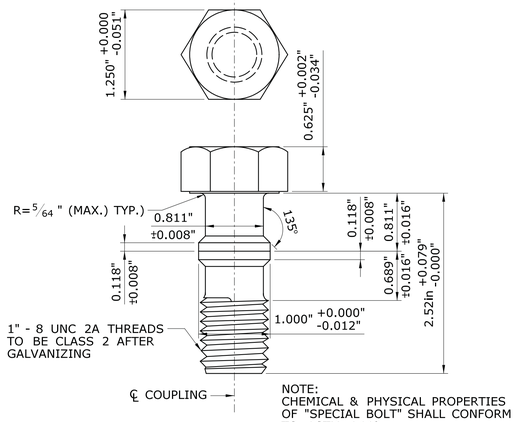
BRACKET DETAILS

HALF SCALE



NOTE:
TOLERANCES TO $\frac{1}{32}$" EXCEPT AS NOTED

BREAKAWAY COUPLING



NOTE:
CHEMICAL & PHYSICAL PROPERTIES OF "SPECIAL BOLT" SHALL CONFORM TO ASTM A449.

SPECIAL BOLT

REV.	DATE	REVISION DESCRIPTION	SHEET NO.
-	-	-	-
-	-	-	-
-	-	-	-
-	-	-	-
-	-	-	-
-	-	-	-

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Plotted Date: 12/4/2013

DESIGNER/DRAFTER: **BKC**

CHECKED BY: **JRH**

SCALE AS NOTED

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\SB_Breakaway_Signpost_BSMS_BracketDet.dgn

SIGNATURE/ BLOCK:

OFFICE OF ENGINEERING

APPROVED BY: DATE:

PROJECT TITLE:

TOWN:

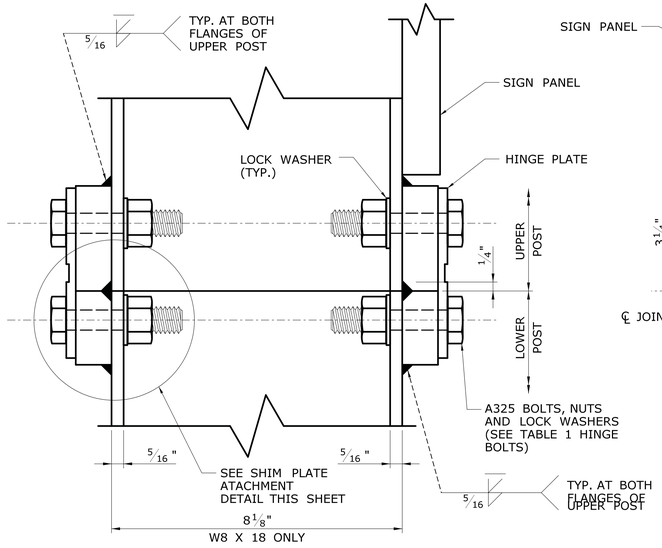
DRAWING TITLE:

BREAKAWAY SIGN SUPPORTS BRACKET DETAILS

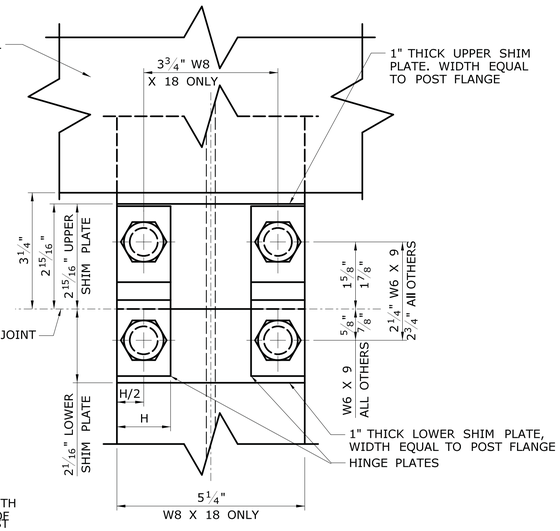
PROJECT NO.

DRAWING NO. **BSM-5**

SHEET NO.



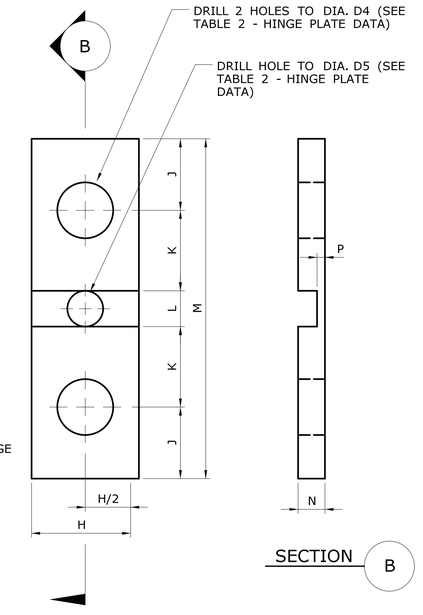
SIDE ELEVATION



FRONT ELEVATION

WITH SHIM PLATE

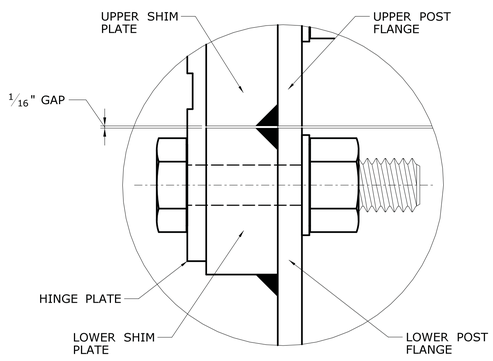
SCALE: 1/2 (W8 X 18)



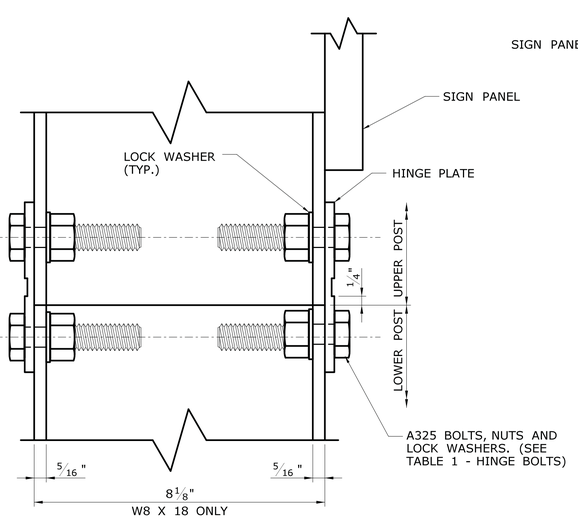
SECTION B

HINGE PLATE DETAILS

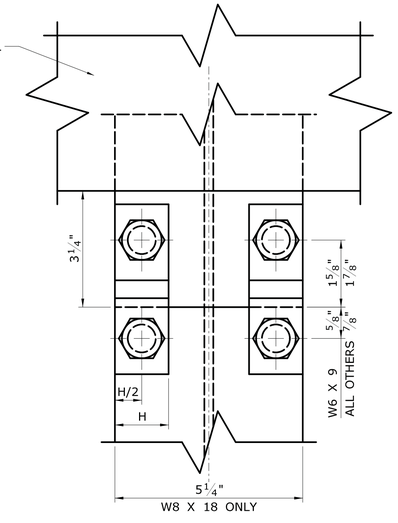
SCALE: FULL



SHIM PLATE ATTACHMENT DETAILS



SIDE ELEVATION



FRONT ELEVATION

WITHOUT SHIM PLATE

SCALE: 1/2 (W8 X 18)

TABLE 2 - HINGE PLATE DATA

POST SIZE	PLATE NO.	DIMENSIONS (IN.)							HOLE DIA. (IN.)	
		H	J	K	L	M	N	P	D4	D5
W6 X 9	1	1	3/4	7/8	1/2	3 3/4	15/64	0.071 ± 0.004	17/32	NONE
W6* AND W8	2	1 1/2	1	1 1/8	1/2	4 3/4	3/8	0.113 ± 0.004	25/32	1/2
ALL OTHERS	3	1 1/2	1	1 1/8	1/2	4 3/4	3/8	0.113 ± 0.004	25/32	NONE

* EXCLUDING W6 X 9

GENERAL NOTES

SPECIFICATIONS: CONNECTICUT DEPARTMENT OF TRANSPORTATION FORM 816 (2004), SUPPLEMENTAL SPECIFICATION DATED JANUARY 2011, AND SPECIAL PROVISIONS.
DESIGN SPECIFICATIONS: AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES 17TH EDITION DATED 2002, AND AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS (2009) WITH THE 2011 INTERIM SPECIFICATIONS.
DESIGN LOADS: THE DESIGN WIND SPEED IS 100 MPH, BASED ON A 10-YEAR MEAN RECURRENCE INTERVAL.
MATERIALS:

FOUNDATIONS: CONCRETE FOR FOUNDATIONS SHALL BE CLASS "A" CONCRETE.

REINFORCEMENT: REINFORCING STEEL SHALL CONFORM TO THE REQUIREMENTS OF ASTM A615, GRADE 60.
SIGN POSTS: STEEL FOR SIGN POSTS SHALL CONFORM TO THE REQUIREMENTS OF ASTM A709, GRADE 36, AND SHALL BE HOT DIP GALVANIZED AFTER FABRICATION IN ACCORDANCE WITH ASTM A123. THE POST SHALL BE PERMANENTLY LABELED WITH THE POST SIZE ON THE WEB AT THE BOTTOM OF THE LOWER POST.
ANCHORS: THREADED FERRULES SHALL BE FABRICATED FROM TYPE 304 STAINLESS STEEL. RODS SHALL BE FABRICATED FROM STEEL CONFORMING TO AISI 1038. STEEL COILS SHALL CONFORM TO THE REQUIREMENTS OF AISI 1008. MINIMUM TENSILE STRENGTH OF 60,000 LBS.
SHIMS: 1" HORSESHOE SHIMS SHALL BE FABRICATED FROM 14 OR 18 GAUGE SHEET STEEL.

BREAKAWAY COUPLINGS: BREAKAWAY COUPLINGS SHALL BE MADE FROM ALLOY STEEL CONFORMING TO AMS 6378D WITH EXCEPTIONS TO DECARBURIZATION AND MACROSTRUCTURE CLAUSES OR AN EQUIVALENT MATERIAL, AND SHALL HAVE A MINIMUM TENSILE YIELD STRENGTH OF 130,000 PSI. THE COUPLING SHALL HAVE A MINIMUM TENSILE ULTIMATE STRENGTH OF 40,400 LBS. THE ROCKWELL HARDNESS SHALL BE C32 MINIMUM. COUPLINGS SHALL BE HOT DIP GALVANIZED IN ACCORDANCE WITH ASTM A153, CLEANED AND PHOSPHATED PER FEDERAL SPECIFICATION TT-C-490C, COATED, 0.002" - 0.004" THICK, USING MORTON POWDER COATINGS' 20-7037 BLACK POLYESTER POWDER OR EQUIVALENT.
CHIPPED AREAS OF THE COATED SURFACE SHALL BE REPAIRED. ALL THREADED SURFACES, AFTER COATING, SHALL BE CLEANED TO ALLOW THEM TO FUNCTION PROPERLY.

BRACKETS: BRACKETS SHALL BE MADE FROM ALUMINUM ALLOY 6061-T6 OR AN EQUIVALENT MATERIAL. THE LOAD CONCENTRATING MEMBER (BOSS) SHALL BE MADE FROM STAINLESS STEEL CONFORMING TO ASTM A582, TYPE 416 WITH ROCKWELL HARDNESS OF C33 - C45. LOCATION HOLES FOR THE BREAKAWAY COUPLING SHALL BE ACCURATELY POSITIONED RELATIVE TO THE LOAD CONCENTRATING MEMBER AND BRACKETS SHALL BE PERMANENTLY LABELED WITH THE BRACKET NUMBER TO REFLECT THE HOLE POSITIONING. SEE DWG. NO. BSM-5 FOR IDENTIFICATION OF BRACKETS BY NUMBER.

HINGE PLATES: HINGE PLATES SHALL BE MADE FROM ALLOY STEEL CONFORMING TO AISI 4130 OR AN EQUIVALENT MATERIAL AND SHALL BE GALVANIZED AFTER FABRICATION IN ACCORDANCE WITH ASTM A123. THE HINGE PLATE SHALL HAVE A MINIMUM TENSILE YIELD STRENGTH OF 90,000 PSI AND MINIMUM TENSILE ULTIMATE STRENGTH AS FOLLOWS:

HI-1 7,100 LBS
HI-2 11,300 LBS
HI-3 17,000 LBS

BOLTS, NUTS AND WASHERS: UNLESS NOTED OTHERWISE, ALL BOLTS SHALL CONFORM TO THE REQUIREMENTS OF ASTM A325. SPECIAL BOLTS SHALL CONFORM TO ASTM A449. NUTS SHALL CONFORM TO THE REQUIREMENTS OF ASTM A563, GRADE DH. LOCKWASHERS SHALL CONFORM TO THE REQUIREMENTS OF ANSI B18-21-1. BOLTS, NUTS AND WASHERS SHALL BE GALVANIZED IN ACCORDANCE WITH THE REQUIREMENTS OF ASTM A153. SPECIAL BOLTS MAY BE MECHANICALLY GALVANIZED IN ACCORDANCE WITH THE REQUIREMENTS OF ASTM B695, CLASS 50.

CAP SCREWS: CAP SCREWS ATTACHING BRACKETS TO POSTS SHALL CONFORM TO THE REQUIREMENTS OF ASTM A307 AND SHALL BE HOT DIP GALVANIZED IN ACCORDANCE WITH ASTM A153.

BREAKAWAY HARDWARE: BREAKAWAY HARDWARE SHALL BE SUPPLIED AS COMPONENTS OF A CRASH-TESTED SYSTEM COMPLYING WITH THE GUIDELINES OF NCHRP REPORT 350 (RECOMMENDED PROCEDURES FOR THE SAFETY PERFORMANCE EVALUATION OF HIGHWAY FEATURES). THE MANUFACTURER SHALL SUBMIT TEST REPORTS TO FHWA FOR APPROVAL.

CERTIFICATION: THE CONTRACTOR SHALL PROVIDE A MATERIALS CERTIFICATE TO CERTIFY THAT THE MATERIAL AND COMPONENTS CONFORM TO THOSE SHOWN ON THE PLANS AND SPECIFICATIONS.

CHANGES: NO CHANGE IN DESIGN MATERIALS OR DETAIL ALTERATIONS WILL BE PERMITTED WITHOUT PRIOR APPROVAL BY THE ENGINEER.

INSTALLATION: INSTALLATION OF THE BREAKAWAY ASSEMBLY SHALL BE IN ACCORDANCE WITH THE RECOMMENDED PRACTICES OF THE SUPPLIER.

BASIS OF PAYMENT: THE COST OF FURNISHING AND INSTALLING THE BREAKAWAY HINGE PLATE ASSEMBLY WILL BE INCLUDED IN THE PAY ITEM "STRUCTURAL STEEL SIGN SUPPORTS." THE COST OF FURNISHING AND INSTALLING THE BREAKAWAY COUPLING SYSTEM, CONSISTING OF BRACKET, BREAKAWAY COUPLINGS, SPECIAL BOLTS, AND SHIMS WILL BE INCLUDED IN THE PAY ITEM "SIDE MOUNTED SIGN FOUNDATION." THE COST OF FURNISHING AND INSTALLING FOUNDATIONS, INCLUDING EXCAVATING, CLASS "A" CONCRETE, REINFORCING STEEL AND ANCHOR FERRULES, WILL BE INCLUDED IN THE PAY ITEM "SIDE MOUNTED SIGN FOUNDATION."

TABLE 1 - HINGE BOLTS

POST SIZE	HINGE ASSEMBLY		THREAD DESIGNATION (U.S. CUSTOMARY UNITS)
	BOLT DIAMETER	BOLT LENGTH	
W6 x 9	1/2	1 1/2	13 UNC
ALL OTHERS	3/4	2 1/4	10 UNC

REV.	DATE	REVISION DESCRIPTION	SHEET NO.
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-	-	-	-
-	-	-	-
-	-	-	-
-	-	-	-
-	-	-	-

Plotted Date: 12/4/2013

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

DESIGNER/DRAFTER: BKC
CHECKED BY: JRH
SCALE AS NOTED

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\SB_Breakaway_Signpost_BSM6_HingeDet.dgn

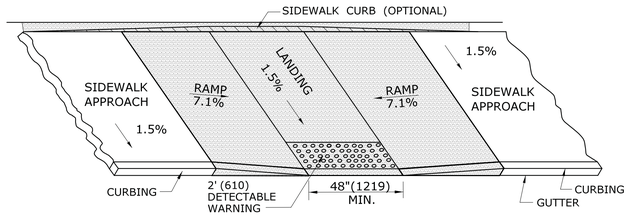
SIGNATURE/BLOCK:
OFFICE OF ENGINEERING
APPROVED BY: DATE:

PROJECT TITLE:

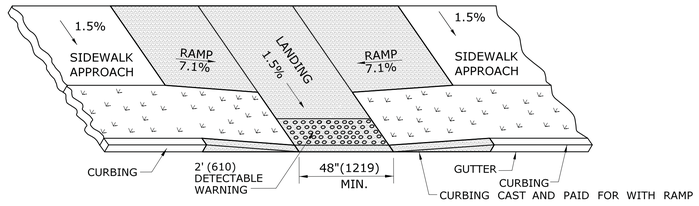
TOWN:

DRAWING TITLE:
BREAKAWAY SIGN SUPPORTS HINGE DETAILS

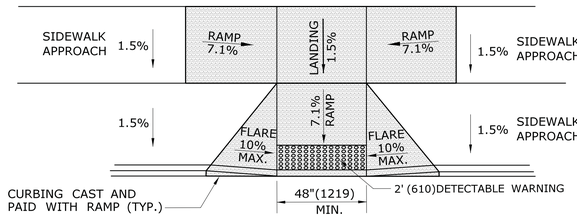
PROJECT NO.
DRAWING NO. BSM-6
SHEET NO.



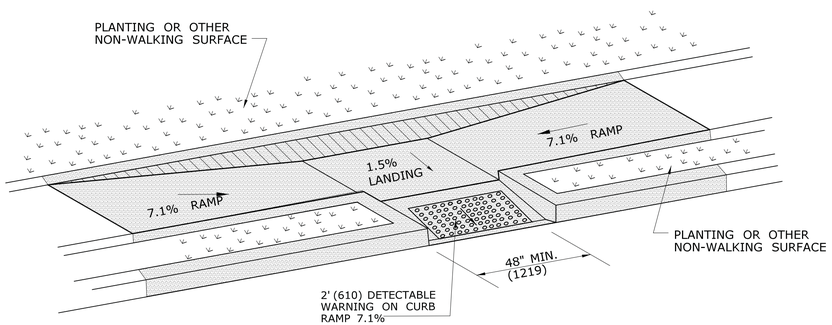
PARALLEL SIDEWALK RAMP (TYPE 1) NO UTILITY STRIP



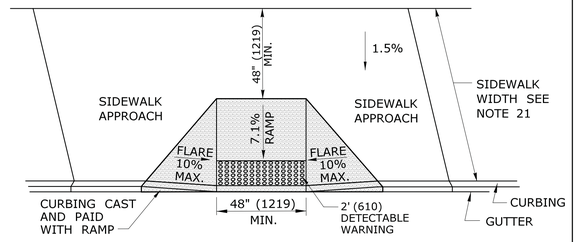
PARALLEL SIDEWALK RAMP (TYPE 1a) WITH UTILITY / GRASS STRIP



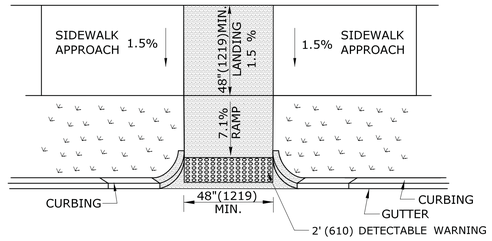
PARALLEL/PERPENDICULAR SIDEWALK RAMP NO UTILITY/GRASS STRIP (TYPE 1b)



PARALLEL SIDEWALK RAMP (TYPE 1c) WITH UTILITY / GRASS STRIP

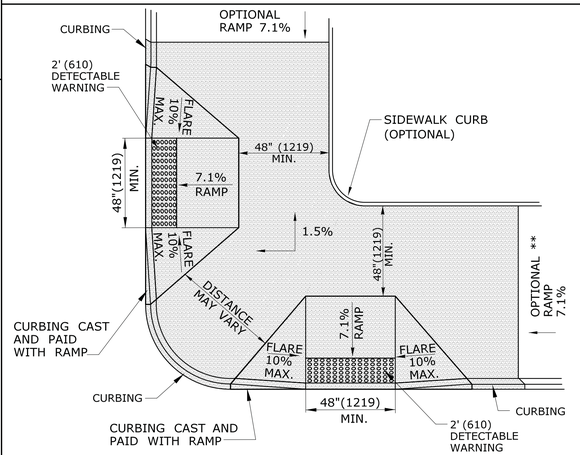


PERPENDICULAR SIDEWALK RAMP W/ 48" (1219) MIN. BY PASS LANDING (TYPE 2)



PERPENDICULAR SIDEWALK RAMP W/CURB RETURNS / UTILITY GRASS STRIP (TYPE 2a)

* OPTIONAL FLARE ONE SIDE OF RAMP

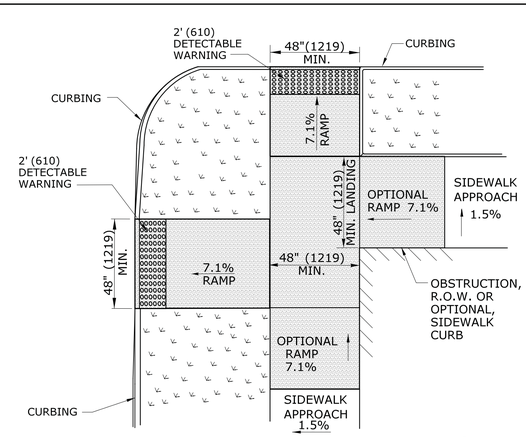


DUAL PERPENDICULAR SIDEWALK RAMPS (TYPE 3)

SEE NOTES 19

* OPTIONAL CURB RETURN ON ONE SIDE OF RAMP

** SEE NOTE 23



DUAL PERPENDICULAR SIDEWALK RAMPS (TYPE 3a) WITH UTILITY / GRASS STRIP

SEE NOTE 20

GENERAL NOTES:

1. MAXIMUM SLOPES OF ADJOINING GUTTERS AND ROAD SURFACES IMMEDIATELY ADJACENT TO THE SIDEWALK RAMP SHOULD NOT EXCEED 5%. THE MAXIMUM GRADE DIFFERENCE BETWEEN THE GUTTER AND CURB RAMP SHALL NOT EXCEED 13%. SEE DETAIL 1 ON SHEET 4.
2. RAMP GRADE SHALL BE UNIFORM, FREE OF SAGS AND ABRUPT GRADE CHANGES. RUNNING SLOPES OF RAMPS SHALL NOT EXCEED 8.33% AND SHALL NOT EXCEED 15' (4.5m) WITHOUT PROVIDING A LANDING.
3. ALL RAMPS SHALL BE CONSTRUCTED OF CLASS "F" CONCRETE IN ACCORDANCE WITH CONNECTICUT STANDARD SPECIFICATIONS.
4. SIDEWALK RAMPS SHALL HAVE A COARSE BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP. THE SURFACE OF ALL SIDEWALK RAMPS SHALL BE STABLE, FIRM AND SLIP RESISTANT. SURFACE DISCONTINUITIES SHALL NOT EXCEED $\frac{1}{2}$" (13) MAX. VERTICAL DISCONTINUITIES BETWEEN $\frac{1}{4}$" (6.4) AND $\frac{1}{2}$" (13) MAX. SHALL BE BEVELED 1:2 MINIMUM APPLIED ACROSS THE ENTIRE LEVEL CHANGE.
5. DIAGONAL SIDEWALK RAMPS AT MARKED CROSSINGS SHALL BE WHOLLY CONTAINED WITHIN THE MARKINGS, EXCLUDING ANY FLARED SIDES. DIAGONAL AND PERPENDICULAR RAMPS SHALL HAVE THE RAMP CUT PERPENDICULAR TO THE TANGENT OF THE CURB RADIUS FOR THE DESIGNATED ACCESSIBLE ROUTE. BOTH LONGITUDINAL SIDES OF THE RAMP SHOULD BE THE SAME LENGTH. SKEWED RAMPS SHOULD BE AVOIDED. FLARES ARE NOT CONSIDERED PART OF PEDESTRIAN ACCESS ROUTE. DIAGONAL RAMPS SHOULD NOT BE INSTALLED WHERE CURB RADII IS LESS THAN 20'(6096).
6. REMOVAL OF EXISTING SIDEWALK FOR NEW RAMP INSTALLATIONS SHALL BE TO THE NEAREST EXPANSION OR CONTRACTION JOINT. 8.3% MAXIMUM SLOPE MAY NOT BE ACHIEVABLE DUE TO EXISTING SIDEWALK GRADE. IN RECOGNITION OF THIS, A LIMIT OF 15' (4572) FOR REMOVAL SHALL BE USED UNLESS OTHERWISE SHOWN ON THE PLANS OR DIRECTED BY THE ENGINEER. SAW CUT REQUIRED FOR DUMMY JOINTS SHALL BE INCLUDED IN THE COST OF "CONCRETE SIDEWALK RAMP" OR "CONCRETE SIDEWALK".
7. EXPANSION JOINTS IN CONCRETE SHALL MATCH THOSE IN ADJACENT SIDEWALKS BUT IN NO CASE SHALL THE SPACING BETWEEN EXPANSION JOINTS EXCEED 12' (3658) UNLESS OTHERWISE NOTED.
8. CONCRETE SIDEWALK RAMPS, SHALL BE PAID FOR UNDER THE ITEM "CONCRETE SIDEWALK RAMP", AS DEFINED BY THE CONSTRUCTION LIMITS ON THE PLANS AND SHALL BE FIELD VERIFIED.
9. SIDEWALK RAMPS SHALL BE CONSTRUCTED WITH THE TOE AT THE GUTTER CAST INTEGRALLY WITH RAMP UNLESS DIRECTED OTHERWISE BY THE ENGINEER (SEE TYPICAL SECTION ON SHEET 3). CURB REMOVAL AND CAST IN PLACE CURBING REQUIRED FOR THE RAMP, SHALL BE INCLUDED WITH PAY ITEM "CONCRETE SIDEWALK RAMP". CURBING OUTSIDE LIMITS OF RAMP OR LANDING SHOWN ON SHEET 3 SHALL BE CONSTRUCTED AND PAID FOR IN ACCORDANCE WITH CONNECTICUT STANDARD SPECIFICATIONS.
10. PREFERRED LOCATION TO INSTALL DETECTABLE WARNING STRIP SHALL BE 6" (152) FROM THE EDGE OF ROAD ALONG THE FULL WIDTH OF THE RAMP. FOR ALTERNATE LOCATIONS, REFER TO DETECTABLE WARNING PLACEMENT DETAILS ON SHEET 4.
11. TO PERMIT WHEELCHAIR WHEELS TO ROLL BETWEEN DOMES, ALIGN DOMES ON A SQUARE GRID IN THE DIRECTION OF RUNNING SLOPE (PERPENDICULAR TO CURB OR SLOPE BREAK). THE TRANSITION FROM RAMP TO GUTTER SHALL BE FLUSH WITHOUT A LIP.
12. WHERE COMMERCIAL DRIVEWAYS ARE PROVIDED WITH TRAFFIC SIGNALS AND THE SIDEWALK IS CONTINUOUS THROUGH DRIVEWAY, DETECTABLE WARNINGS ARE REQUIRED AT THE JUNCTION BETWEEN THE PEDESTRIAN ROUTE AND DRIVEWAY.
13. CONSTRUCT A SIDEWALK CURB WHEN THERE IS INSUFFICIENT BUFFER AVAILABLE TO GRADE OR WHEN CALLED FOR IN PLANS. PAID FOR WITH SIDEWALK RAMP WHEN REQUIRED FOR RAMP.
14. THE TOP AND BOTTOM OF RAMPS SHOULD BE PROVIDED WITH A 4'x 4'(1219 x 1219) MINIMUM LEVEL LANDING AREA WITH A CROSS SLOPE LESS THAN OR EQUAL TO 2% IN ANY DIRECTION.
15. UTILITY POLES, LUMINAIRE, PEDESTRIAN OR SIGNAL POLES, GRATES, ACCESS COVERS, AND OTHER APPURTENANCES SHALL NOT BE LOCATED ON RAMPS, LANDINGS, BLENDED TRANSITIONS, AND @ GUTTERS WITHIN THE PEDESTRIAN ACCESS ROUTE.
16. APPROACH SIDEWALK WIDTHS, GRASS STRIP OR UTILITY STRIP WIDTHS MAY VARY.
17. APPROACH SIDEWALK AND LANDING CROSS SLOPE SHALL NOT EXCEED 2%.
18. THE RUNNING OR CROSS SLOPES ON LANDINGS AT MID BLOCK CROSSING MAY BE WARPED TO MEET STREET OR HIGHWAY GRADE.
19. FOR PERPENDICULAR CURB RAMPS A MIN. 4'(1.2m) x 4'(1.2m) LEVEL LANDING SHALL BE PROVIDED AT THE TOP OF CURB RAMP. WHERE THE LEVEL LANDING IS RESTRICTED AT THE BACK OF SIDEWALK THE LEVEL LANDING SHALL BE 4'(1.2m) x 5'(1.5m) WITH THE 5'(1.5m) DIMENSION PROVIDED IN THE DIRECTION OF THE RAMP RUN.
20. FOR PARALLEL CURB RAMPS, A MIN. 4'(1.2m) x 4'(1.2m) LEVEL LANDING SHALL BE PROVIDED AT THE BOTTOM OF CURB RAMP. IF THE LEVEL LANDING IS RESTRICTED ON 2 OR MORE SIDES, THE LEVEL LANDING SHALL BE 4'(1.2m)x 5'(1.5m) WITH THE 5' (1.5m) DIMENSION PROVIDED IN THE DIRECTION OF THE PEDESTRIAN STREET CROSSING.
21. WHEN WIDTH OF SIDEWALK IS ≥48" AND A PERPENDICULAR SIDEWALK RAMP IS INSTALLED, THE FLARED SIDES SHALL BE 10% MAX. IF WIDTH OF SIDEWALK IS <48" THE FLARED SIDES MUST NOT EXCEED 8.33% (12:1).
22. SHADED AREAS ARE TYPICAL PAY LIMITS FOR CONCRETE SIDEWALK RAMP BUT, MAY VARY AS DIRECTED BY THE ENGINEER.
23. OPTIONAL RAMP, WHEN REQUIRED, SHALL BE PAID FOR AS PART OF CONCRETE SIDEWALK RAMP.

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED.

REV.	DATE	REVISION DESCRIPTION	SHEET NO.
-	-	-	-
-	-	-	-
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1	7/13	Created new sheets (4 total).	-

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 6/17/2014

DESIGNER/DRAFTER: MGB/EMK

CHECKED BY: LLF

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\SIDEWALK RAMP 1_GD.dgn

SIGNATURE/BLOCK: OFFICE OF ENGINEERING

APPROVED BY:

PROJECT TITLE:

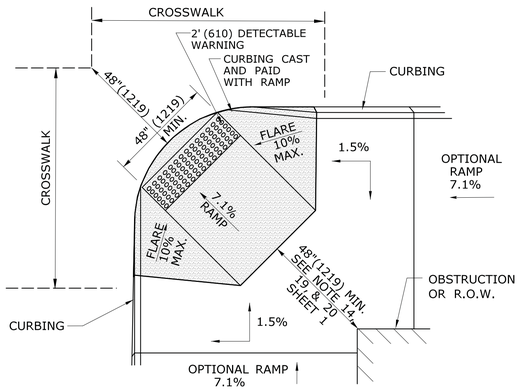
TOWN:

PROJECT NO.

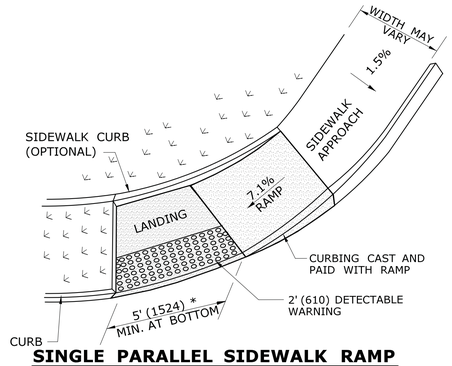
DRAWING NO.

DRAWING TITLE: SIDEWALK RAMPS SHEET 1

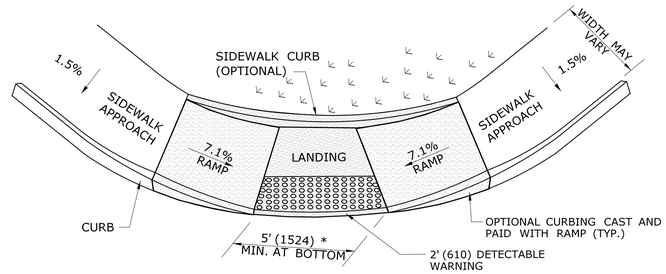
SHEET NO.



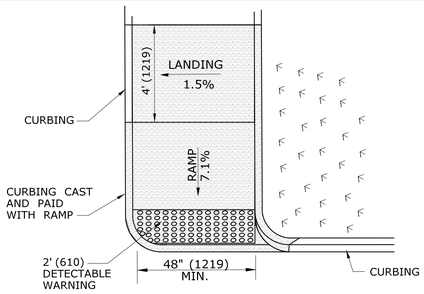
DIAGONAL SIDEWALK RAMP (TYPE 4)
W/LANDING AT TOP



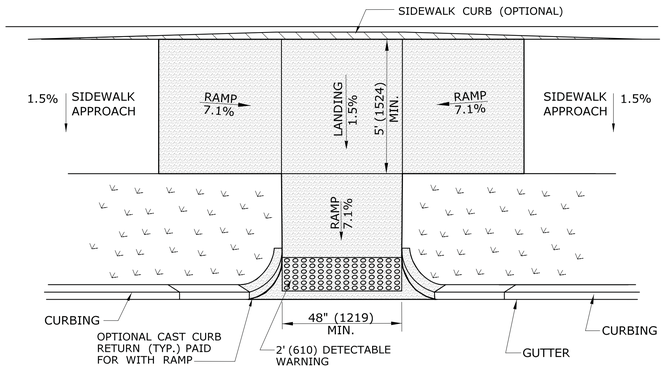
SINGLE PARALLEL SIDEWALK RAMP
W/LANDING AT BOTTOM ON
CORNER (TYPE 4c)
* SEE NOTE 20 SHEET 1



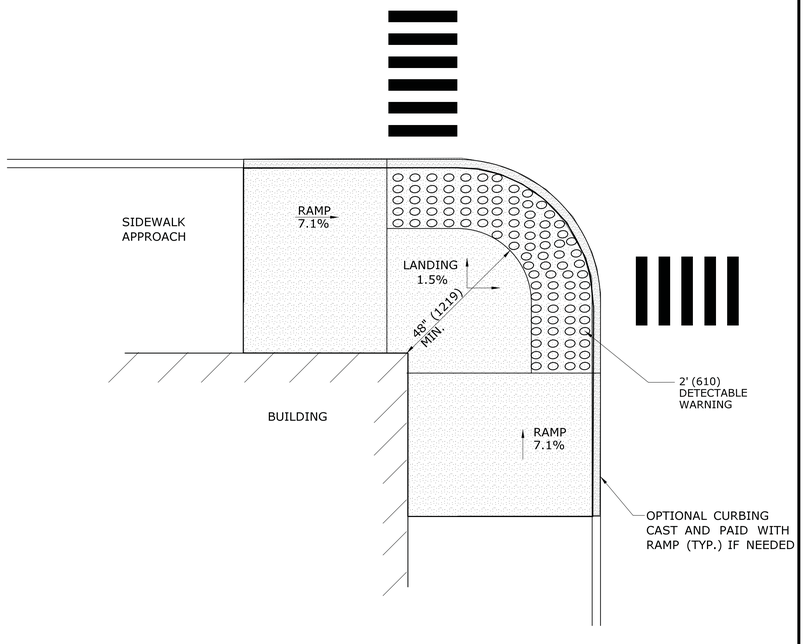
DOUBLE PARALLEL SIDEWALK RAMP
W/LANDING AT BOTTOM ON CORNER (TYPE 4f)
* SEE NOTE 20 SHEET 1



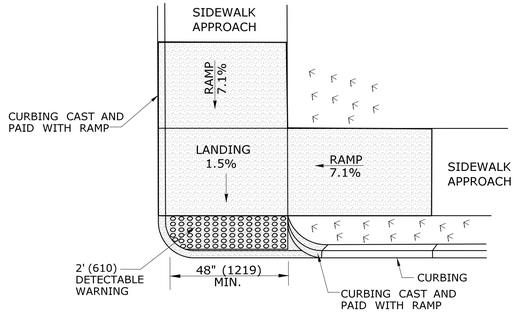
SINGLE DIRECTION
PERPENDICULAR SIDEWALK RAMP
NO / UTILITY GRASS STRIP
(TYPE 4a)



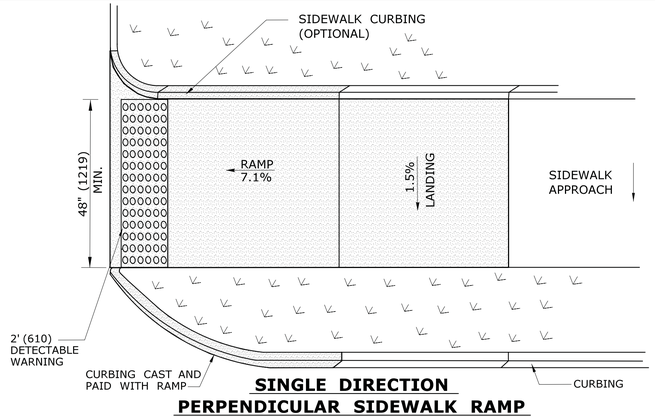
PARALLEL/PERPENDICULAR SIDEWALK RAMP
COMBINATION W/ CURB RETURNS (TYPE 4d)
* OPTIONAL FLARE ONE SIDE



RESTRICTED CONDITION
DIAGONAL SIDEWALK RAMP
(TYPE 4g)



DOUBLE DIRECTION
PARALLEL SIDEWALK RAMP
NO / UTILITY GRASS STRIP
(TYPE 4b)
SEE NOTE 20 SHEET 1



SINGLE DIRECTION
PERPENDICULAR SIDEWALK RAMP
W/ UTILITY GRASS STRIP (TYPE 4e)
REFER TO DETECTABLE WARNING PLACEMENT ON SHEET 4

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

REV.	DATE	REVISION DESCRIPTION	SHEET NO.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 5/27/2014

DESIGNER/DRAFTER: MGB/EMK
CHECKED BY: LLF

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\SIDEWALK RAMP 2_GD.dgn

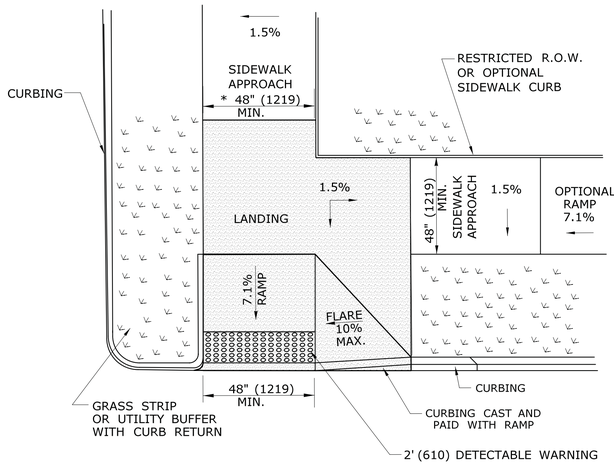
SIGNATURE/BLOCK:
OFFICE OF ENGINEERING
APPROVED BY:

PROJECT TITLE:

TOWN:

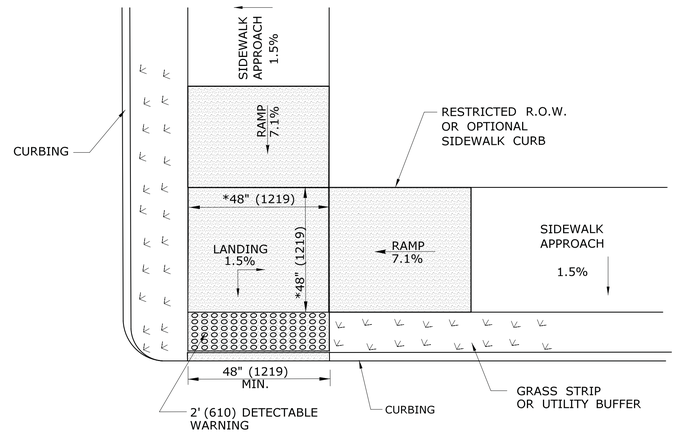
DRAWING TITLE:
SIDEWALK RAMP
SHEET 2

PROJECT NO.
DRAWING NO.
SHEET NO.



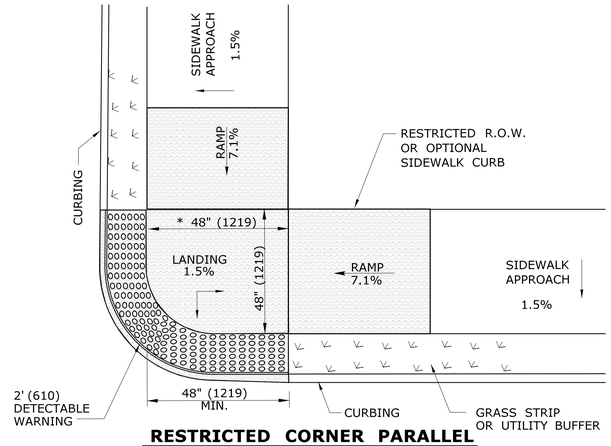
RESTRICTED CORNER PERPENDICULAR SIDEWALK RAMP
(TYPE 5)

* SEE NOTE 19 SHEET 1



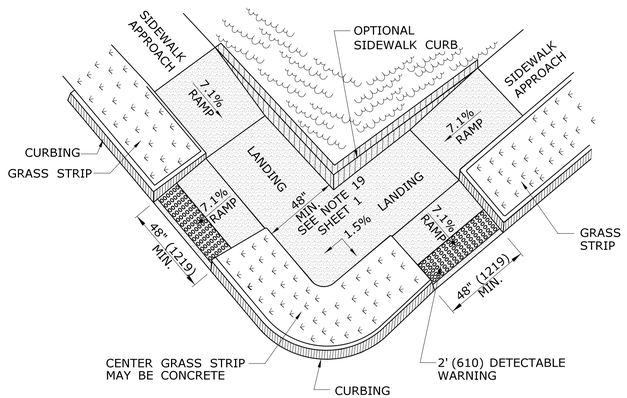
RESTRICTED CORNER PARALLEL DOUBLE SIDEWALK RAMPS
W/CENTER LANDING AND UTILITY GRASS STRIP
(TYPE 5a)

* SEE NOTE 20 SHEET 1



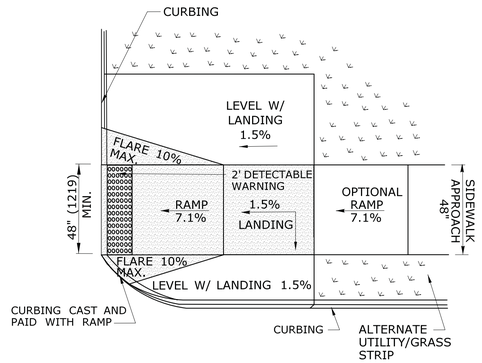
RESTRICTED CORNER PARALLEL
SIDEWALK RAMPS W/ CENTER LANDING
TWO DIRECTION AND UTILITY GRASS STRIP
(TYPE 5b)

* SEE NOTE 20 SHEET 1

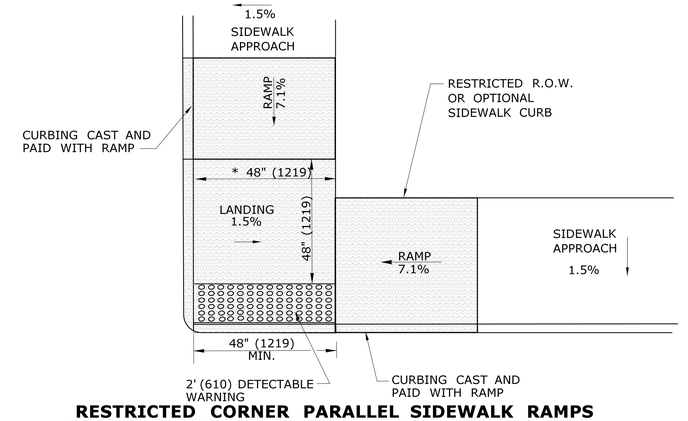


COMBINATION SIDEWALK RAMP (TYPE 6)

* SEE NOTE 19 SHEET 1

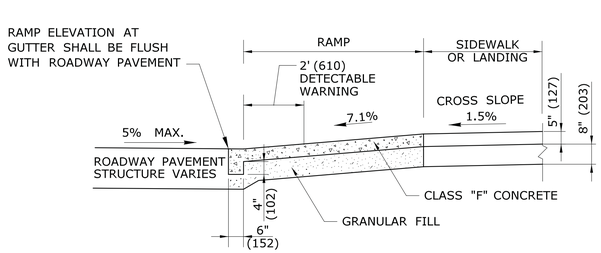


PERPENDICULAR SIDEWALK RAMP
ONE DIRECTION ON CORNER
(TYPE 6a)



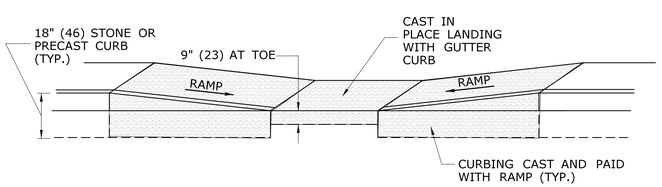
RESTRICTED CORNER PARALLEL SIDEWALK RAMPS
W/CENTER LANDING NO GRASS STRIP
(TYPE 5c)

* SEE NOTE 20 SHEET 1

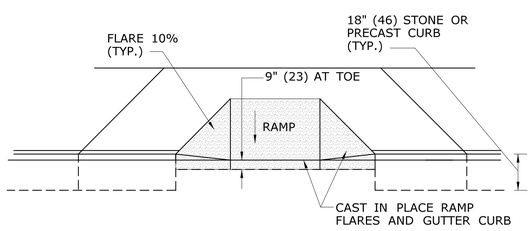


TYPICAL SECTION THRU SIDEWALK RAMP

SEE NOTE 2 AND 17 SHEET 1



TYPICAL ELEVATION PARALLEL SIDEWALK RAMP
WITH CAST IN PLACE GUTTER



TYPICAL ELEVATION PERPENDICULAR SIDEWALK RAMP
WITH CAST IN PLACE GUTTER

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

REV.	DATE	REVISION DESCRIPTION	SHEET NO.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 5/27/2014

DESIGNER/DRAFTER: MGB/EMK
CHECKED BY: LLF

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...\SIDEWALK RAMP 3_GD.dgn

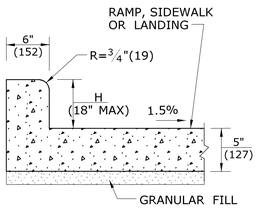
SIGNATURE/BLOCK: OFFICE OF ENGINEERING
APPROVED BY:

PROJECT TITLE:

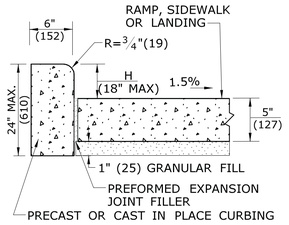
TOWN:

DRAWING TITLE: SIDEWALK RAMP SHEET 3

PROJECT NO.
DRAWING NO.
SHEET NO.

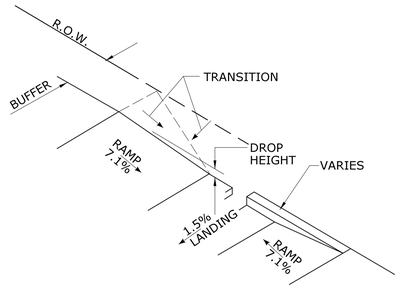


MONOLITHIC CAST CURB

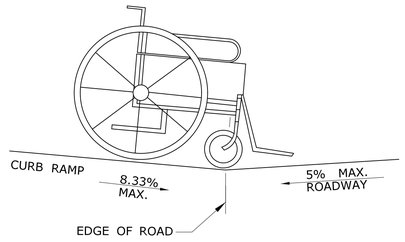


SEPARATELY CAST CURB

SIDEWALK CURB OPTIONS AT BACK OF SIDEWALK



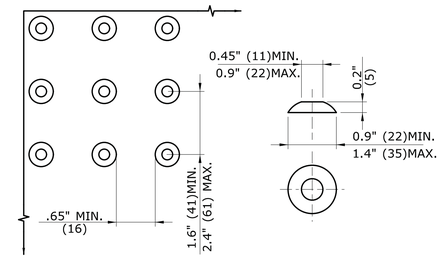
BACK OF SIDEWALK CURB OR BUFFER TRANSITION



DETAIL 1
SEE GRADE CHANGE AT ROADWAY INTERFACE
SEE NOTE 1 SHEET HW-921 02a

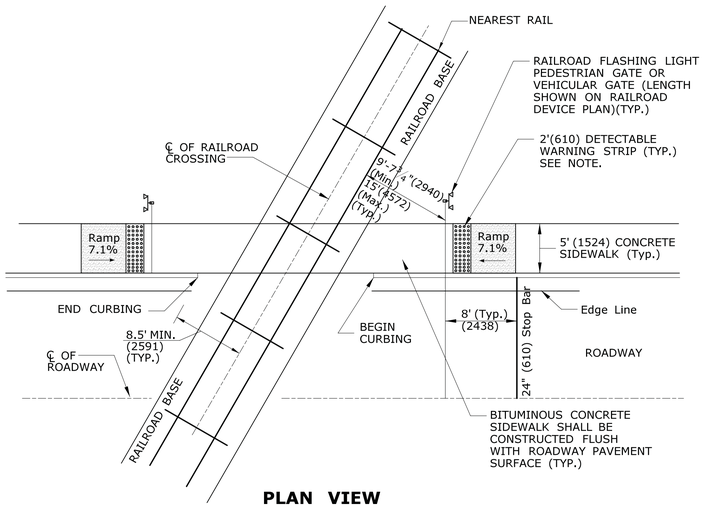
GENERAL NOTES:

1. RAMPED MEDIANS SHALL HAVE A CURB RAMP AT EITHER END AND LEVEL LANDING A MINIMUM OF 5' x 5' (1.5m x 1.5m) IN BETWEEN. CUT-THROUGH MEDIANS SHALL BE A MINIMUM OF 6' (1.8m) LONG AND 5' (1.5m) WIDE. FOR ALL MEDIANS, CUT-THROUGH OR RAMPED, A 2' (610) STRIP OF DETECTABLE WARNINGS SHALL BE INSTALLED AT THE ENTRANCE AND EXIT.
2. SEE GENERAL NOTES ON SHEET 1.

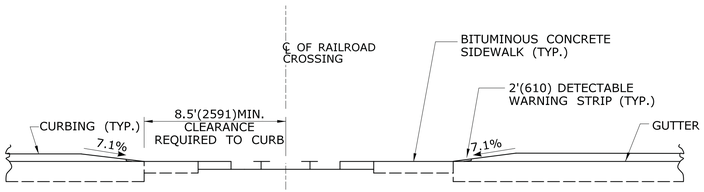


DOME SPACING **DOME SECTION**

STANDARD DOME ON DETECTABLE WARNING TILES



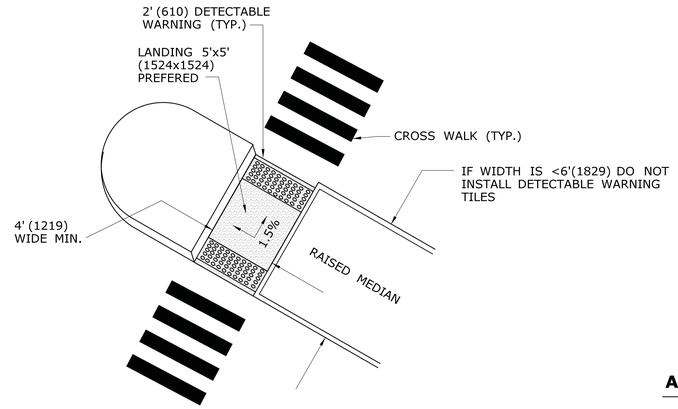
PLAN VIEW



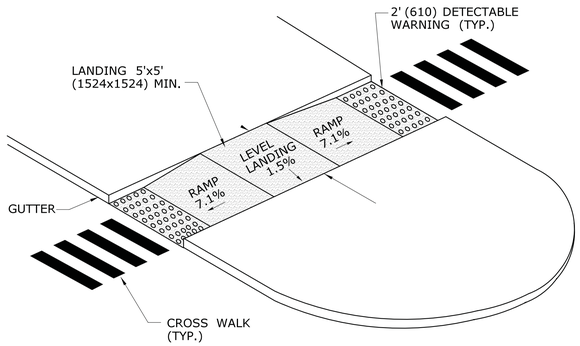
ELEVATION VIEW

DETECTABLE WARNINGS AT RAILROAD CROSSING

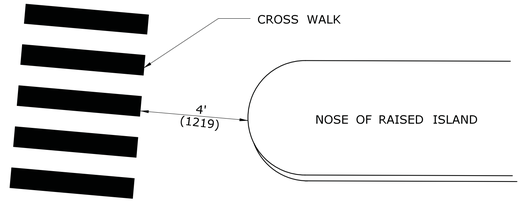
NOTE: WHEN NO GATE IS PRESENT, INSTALL DETECTABLE WARNING SURFACE 12' (3.6m) FROM THE NEAREST RAIL. IF GATE IS PRESENT, INSTALL DETECTABLE WARNING 2' (610) PRIOR TO GATE. THE ROWS OF TRUNCATED DOMES IN A DETECTABLE WARNING SURFACE SHALL BE INSTALLED PARALLEL WITH THE DIRECTION OF PEDESTRIAN TRAVEL.



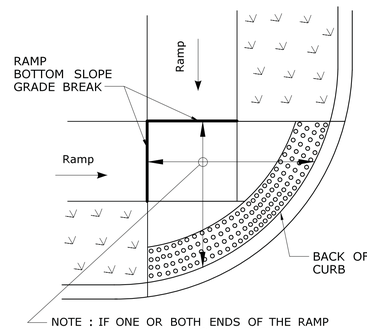
CUT-THROUGH MEDIAN ISLAND



RAISED MEDIAN ISLAND WITH LANDING AND RAMPS



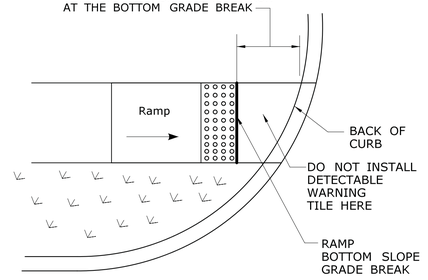
ALTERNATE CROSSWALK WITH MEDIAN ISLAND PULLED BACK



NOTE : IF ONE OR BOTH ENDS OF THE RAMP BOTTOM SLOPE GRADE BREAK ARE GREATER THAN 5' (1.5m) FROM BACK OF CURB, PLACE DETECTABLE WARNING TILE ON THE LOWER LANDING AT BACK OF CURB ALONG FULL WIDTH.

DETECTABLE WARNING PLACEMENT DETAIL 1

NOTE : IF BOTH ENDS OF THE RAMP BOTTOM SLOPE GRADE BREAK ARE LESS THAN 5' (1.5m) FROM BACK OF CURB, PLACE DETECTABLE WARNING TILE ON THE RAMP RUN AT THE BOTTOM GRADE BREAK



DETECTABLE WARNING PLACEMENT DETAIL 2

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

REV.	DATE	REVISION DESCRIPTION	SHEET NO.

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Plotted Date: 5/27/2014

DESIGNER/DRAFTER: **MGB/EMK**
CHECKED BY: **LLF**

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

FIlename: ...\SIDEWALK RAMP 4_GD.dgn

SIGNATURE/ BLOCK: **OFFICE OF ENGINEERING**
APPROVED BY:

PROJECT TITLE:

TOWN:

PROJECT NO.

DRAWING NO.

DRAWING TITLE: **SIDEWALK RAMP SHEET 4**

SHEET NO.

R1 - SERIES

R1-1

LEGEND - WHITE
BACKGROUND - RED

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
3.31	24	31-0536	1	.080
5.19	30	31-0552	1	.080
7.98	36	31-0553	1	.080
13.3	48	31-0557	2	.100

R1-2

LEGEND - RED
BACKGROUND -WHITE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
3.90	36	31-0523	1	.080
6.77	48	31-0522	2B	.100
10.83	60	31-0528	2B	.100

R1-3

LEGEND - WHITE
BACKGROUND - RED

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
0.75	18X6	31-0508		.080

**

LEGEND - RED
BACKGROUND - WHITE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
1.75	18	31-0532	1	.080

R2 - SERIES

R2-1

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
5.00	24X30	31-5505	1	.080
7.50	30X36	31-5504	1	.080
12.00	36X48	31-5506	2	.100
20.00	48X60	31-5507	2	.100

R2-4a

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
32.00	48X96	31-5510	2B	.125

R2-5

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
5.00	24X30	31-5512	1	.080
20.00	48X60	31-5532	2	.100

R3 - SERIES

R3-1

31-1604 OVERHEAD MTD
LEGEND - BLACK
BACKGROUND - WHITE
CIRCLE & DIAGONAL - RED

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
4.00	24X24	31-1604		.080
6.25	30X30	31-1617	1	.080
5.00	30X24	31-1618		.080
9.00	36X36	31-1627	2	.080
7.50	36X30	31-1628		.080

R3-2

31-1603 OVERHEAD MTD
LEGEND - BLACK
BACKGROUND - WHITE
CIRCLE & DIAGONAL - RED

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
4.00	24X24	31-1603		.080
6.25	30X30	31-1619	1	.080
5.00	30X24	31-1620		.080
9.00	36X36	31-1629	2	.080
7.50	36X30	31-1630		.080

R3-3

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
4.00	24X24	31-1648	1	.080
9.00	36X36	31-1647	1	.080

R3-4

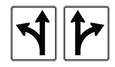
LEGEND - BLACK
BACKGROUND - WHITE
CIRCLE & DIAGONAL - RED

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
4.00	24X24	31-1622	1	.080
9.00	36X36	31-1632	1	.080

R3-5

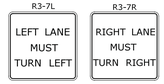
AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
4.00	24X24	31-0135L	1	.080
4.00	24X24	31-0138R	1	.080
7.50	30X36	31-0183L	1	.080
7.50	30X36	31-0184R	1	.080

R3-6



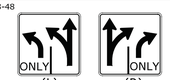
AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
7.50	30X36	31-0157L	1	.080
7.50	30X36	31-0158R	1	.080

R3-7L / R3-7R



AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.25	30X30	31-0117L	1	.080
6.25	30X30	31-0118R	1	.080
9.00	36X36	31-0120L	1	.080
9.00	36X36	31-0123R	1	.080

R3-48



AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.25	30X30	31-0262L	1	.080
6.25	30X30	31-0261R	1	.080

R3-8

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
4.00	24X24	31-0125	1	.080
6.25	30X30	31-0165	1	.080
7.50	30X36	31-0175	1	.080

R3-8a

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
5.00	30X24	31-0290	1	.080

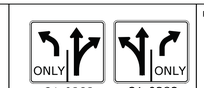
R3-8b

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.25	30X30		1	.080
6.25	30X30		1	.080

R3-9

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
8.75	42X30	31-0279	2	.100

R3-10



AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
7.50	36X30		2	.080

R3-11

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
7.50	36X30		2	.080

R3-12

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.25	30X30	31-0117L	1	.080
6.25	30X30	31-0118R	1	.080
9.00	36X36	31-0120L	1	.080
9.00	36X36	31-0123R	1	.080

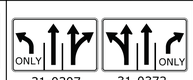
R3-13

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.25	30X30	31-0262L	1	.080
6.25	30X30	31-0261R	1	.080

R3-8b

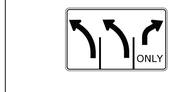
AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
10.00	48X30	31-0302	2	.100

R3-14



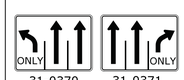
AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
10.00	48X30		2	.100

R3-15



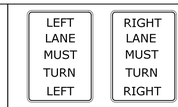
AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
8.75	42X30	31-0334	2	.100

R3-16



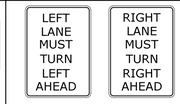
AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
8.75	42X30		2	.100

R3-17



AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
20.00	48X60	31-0121L	2	.100
20.00	48X60	31-0122R	2	.100

R3-18



AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
20.00	48X60	31-0124L	2	.100
20.00	48X60	31-0137R	2	.100

R4 - SERIES

R4-1

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
5.00	24X30	31-1502	1	.080

R4-2

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
20.00	48X60	31-1574	2	.100

R4-3

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
5.00	24X30	31-1562	1	.080
20.00	48X60	31-1564	2	.100

R4-4

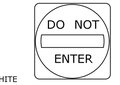
AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
20.00	48X60	31-1504	2	.100

R4-7

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
5.00	24X30	31-1526	1	.080
12.00	36X48	31-1536	2	.100
20.00	48X60	31-1546	2	.100

R5 - SERIES

R5-1



LEGEND - WHITE
BACKGROUND - WHITE
CIRCLE - RED

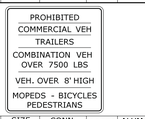
AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.25	30X30	31-1119	1	.080
9.00	36X36	31-1120	1	.080
16.00	48X48	31-1121	2	.100

R5-1

LEGEND - WHITE
BACKGROUND - RED

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.00	36X24	31-1122	2	.080
8.75	42X30	31-1123	2	.100

R5-2



AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
27.00	54X72	31-1719	2	.125

R5-3

LEGEND - BLACK
BACKGROUND - WHITE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
4.00	24X24	31-1727	1	.080

R5-4

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
2.00	24X12	31-1776	1	.080

R5-5

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.00	36X24	31-1777	2	.080

R5-6

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
5.00	24X30	31-1790	1	.080
20.00	48X60	31-1792	2	.100

R5-10c

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
2.00	24X12	31-1776	1	.080

R6 - SERIES

R6-1

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
3.00	36X12	31-1188L	1	.080
3.00	36X12	31-1177R	1	.080
6.75	54X18	31-1189L	1	.100
6.75	54X18	31-1178R	1	.100

R7 - SERIES

R7-1

LEGEND - WHITE
BACKGROUND - BLUE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
2.00	12X24	31-0629	1	.080

R7-1

LEGEND - RED
BACKGROUND - WHITE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
1.50	12X18	31-0630	1	.080

R7-2

LEGEND - RED
BACKGROUND - WHITE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
1.50	12X18	31-0603	1	.080

R8 - SERIES

R8-7

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
13.50	54X36	31-0667	2	.100

R9 - SERIES

R9-1

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
27.50	66X60	31-1733	2B	.125

R10 - SERIES

R10-6

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.00	24X36		1	.080

R10-7

OVERHEAD MOUNTED

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
2.25	18X18	31-0817		.080
4.00	24X24	31-0823		.080
9.00	36X36	31-0816		.080

R10-8

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
7.50	30X36	31-0847	1	.080
20.00	48X60	31-0849	2	.100

R10-9

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
5.00	24X30	31-0803	1	.080
12.00	36X48	31-0804	2	.100

R10-10L

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
2.25	18X18	31-0841	1	.080
5.00	24X24	31-0842	1	.080

R10-10

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
5.00	24X30	31-0806	1	.080
12.00	36X48	31-0807	2	.100

R10-11a

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
3.00	18X24	31-0818	1	.080
5.00	24X30	31-0824	1	.080
7.50	30X36	31-0830	1	.080
12.00	36X48	31-0819	2	.100

R11 - SERIES

R11-1

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
5.00	24X30	31-1915	1	.080
20.00	48X60	31-1918	2	.100

NOTES:

FOR METRIC SEE CONVERSION CHART.

1. THE LEGEND "O.S.T.A." SHALL APPEAR ON ALL R- SERIES SIGNS EXCEPT WHEN SUFFIXED WITH THE LETTER "Z".
2. FOR SPECIFIC SIGN DESIGN CONTACT CONN. D.O.T., DIVISION OF TRAFFIC ENGINEERING. FOR BOLT HOLE PATTERN REFER TO FHWA PUBLICATION "STANDARD HIGHWAY SIGNS". SIGNS OF DIFFERENT DIMENSIONS TO BE ERECTED ON THE SAME POSTS, OR SPAN/MAST ARM MOUNTED, MAY REQUIRE SPECIAL BOLT HOLE PATTERNS.
3. POSTS - SEE TYP. SHEET (SHT #9) - "TYPICAL METAL SIGN POSTS AND SIGN MOUNTING DETAILS."
4. POSTS - TYPE A (EXCEPT WHERE NOTED WITH A "B" FOR TYPE B)
5. FOR OVERHEAD MOUNTED SIGNS, SEE TYPICAL SIGNAL SHEET - "MISC. TRAFFIC SIGNAL INSTALLATION DETAILS."
6. SIGNS SHALL BE FABRICATED OF ONE CONTINUOUS PIECE OF SHEET ALUMINUM. SPLICING OF SHEET ALUMINUM WILL NOT BE ACCEPTED.
7. BACKGROUND SHEETING SHALL BE TYPE III RETROREFLECTIVE SHEETING, EXCEPT NOTED BY *.

** THIS SIGN IS ADDED FOR THIS PROJECT ONLY

COLORS:

R-SERIES
BACKGROUND - WHITE (SILVER) - EXCEPT AS NOTED.
LEGEND - BLACK - EXCEPT AS NOTED.
ALL COLORS SHALL BE RETROREFLECTIVE WITH THE EXCEPTION OF BLACK WHICH SHALL BE OPAQUE.
* SIGNS TO BE "BRIGHT WIDE ANGLE RETROREFLECTIVE SHEETING".

REV.	DATE	REVISION DESCRIPTION
3	6-2012	REVISED NOTE #1 TO REFERENCE "O.S.T.A."
2	6-2010	INCLUDED SIGNS TO MEET 2009 MUTCD REQUIREMENTS.
1	1-2010	REVISED SHEETING FROM TYPE I TO III.

METRIC CONVERSION CHART (1" = 25mm)

ENGLISH	METRIC	ENGLISH	METRIC	ENGLISH	METRIC
12"	300	42"	1050	72"	1800
18"	450	48"	1200	78"	1950
24"	600	54"	1350	84"	2100
30"	750	60"	1500	90"	2250
36"	900	66"	1650	96"	2400

DESIGNER/DRAFTER: D. K. SWINBURNE
CHECKED BY: L.N. CONROY
Plotted: 4/30/2015

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

SIGNATURE/BLOCK:
OFFICE OF ENGINEERING
APPROVED BY: DATE:

PROJECT TITLE:

TOWN:

DRAWING TITLE:
SIGN FACE SHEET ALUMINUM
R-SERIES SIGNS TYPICAL DETAILS

PROJECT NO.

DRAWING NO.
GS-1

SHEET NO.

Filename: CTDOT_TRAFFIC_GD.dgn

S - SERIES

S1-1

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.75	36	✱ 41-2112	1	.080
12.00	48	✱ 41-2113	2	.100

W1 - SERIES

W1-1L

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.25	30	41-4006	1	.080
9.00	36	41-4031	1	.080
16.00	48	41-4056	2	.100

W1-1R

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.25	30	41-4005	1	.080
9.00	36	41-4160	1	.080
16.00	48	41-4055	2	.100

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
9.00	36	41-4141	1	.080
16.00	48	41-4062	2	.100

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
9.00	36	41-4140	1	.080
16.00	48	41-4061	2	.100

W1-2

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.25	30	41-4028	1	.080
9.00	36	41-4084	1	.080
16.00	48	41-4068	2	.100

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.25	30	41-4027	1	.080
9.00	36	41-4083	1	.080
16.00	48	41-4067	2	.100

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
9.0	36	41-4139	1	.080
16.00	48	41-4088	2	.100

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
9.0	36	41-4040	1	.080
16.00	48	41-4087	2	.100

W1-6 (LEFT OR RIGHT)

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
8.00	48X24	41-4222	2	.100
12.50	60X30	41-4224	2	.100

W1-7

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
8.00	48X24	41-4207	2	.100
12.50	60X30	41-4208	2	.100

W1-10L

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
9.00	36	41-4085	1	.080
16.00	48	41-4090	2	.100

W1-10R

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
9.00	36	41-4043	1	.080
16.00	48	41-4089	2	.100

W1-8 (LEFT OR RIGHT)

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
3.00	18X24	41-4250	1	.080
5.00	24X30	41-4203	1	.080
7.50	30X36	41-4204	1	.080
12.00	36X48	41-4251	2	.080

W2 - SERIES

W2-1

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.25	30	41-4226	1	.080
9.00	36	41-4236	1	.080
16.00	48	41-4246	2	.100

W2-2 (LEFT OR RIGHT)

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.25	30	41-4225	1	.080
9.00	36	41-4235	1	.080
16.00	48	41-4245	2	.100

W2-3 (L) (R)

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.25	30	41-4220L	1	.080
6.25	30	41-4219R	1	.080
9.00	36	41-4253L	1	.080
9.00	36	41-4252R	1	.080
16.00	48	41-4244L	2	.100
16.00	48	41-4243R	2	.100

W2-4 **

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
2.25	18	41-4217	1	.080
6.25	30	41-4217	1	.080
9.00	36	41-4255	1	.080
16.00	48	41-4247	2	.100

W3 - SERIES

W3-1 OCTAGON - RED

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.25	30	41-0501	1	.080
9.00	36	41-0502	1	.080
16.00	48	41-0503	2	.100

W3-2 TRIANGLE - RED & WHITE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.25	30	41-0504	1	.080
9.00	36	41-0505	2	.100
16.00	48	41-0506	2	.100

W3-3 SYMBOL & LEGEND - BLACK
BACKGROUND - YELLOW
TOP CIRCLE - RED
BOTTOM CIRCLE - GREEN

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.25	30	41-0835	1	.080
9.00	36	41-0836	1	.080
16.00	48	41-0848	2	.100
24.00	60	41-0860	2	.125

WHEN FLASHING STOP AHEAD
W/ FLASHERS

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
25.00	60	41-0534	2B	.125

W4 - SERIES

W4-1L

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
9.00	36	41-4448	1	.080
16.00	48	41-4452	2	.100

W4-1R

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
9.00	36	41-4449	1	.080
16.00	48	41-4459	2	.100

W4-2L

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
9.00	36	41-4442	1	.080
16.00	48	41-4446	2	.100

W4-2R

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
9.00	36	41-4441	1	.080
16.00	48	41-4447	2	.100

W4-3L

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.25	30	41-4433	1	.080
9.00	36	41-4468	1	.080
16.00	48	41-4435	2	.100

W4-3R

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.25	30	41-4432	1	.080
9.00	36	41-4470	1	.080
16.00	48	41-4434	2	.100

W5 - SERIES

W5-1 ROAD NARROWS

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
9.00	36	41-4402	1	.080
16.00	48	41-4404	2	.100

W5-4 ** BIKEWAY NARROWS
LEGEND - BLACK
BACKGROUND - YELLOW

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
2.25	18	41-1490	1	.080

W6 - SERIES

W6-1 W6-2

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.25	30	41-4333	1	.080
9.00	36	41-4335	1	.080
16.00	48	41-4330	2	.100

W6-3

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
9.00	36	41-4329	1	.080
16.00	48	41-4331	2	.100

W7 - SERIES

W7-1

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.25	30	41-4506	1	.080
9.00	36	41-4530	1	.080
16.00	48	41-4508	2	.100

W8 - SERIES

W8-5

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
9.00	36	41-4443L	1	.080
6.25	30	41-4519	1	.080
9.00	36	41-4520	1	.080
16.00	48	41-4521	2	.100

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
9.00	36	41-4440R	1	.080
16.00	48	41-4444L	2	.100
16.00	48	41-4445R	2	.100

W9-2 LANE ENDS MERGE LEFT (L) LANE ENDS MERGE RIGHT (R)

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
9.00	36	41-4454L	1	.080
9.00	36	41-4453R	1	.080
16.00	48	41-4456L	2	.100
16.00	48	41-4455R	2	.100

SLOW VEHICLE LANE ENDS

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
22.00	66X48	41-1510	2	.100

W9 - SERIES

W9-1 LEFT LANE ENDS (L) RIGHT LANE ENDS (R)

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
9.00	36	41-4443L	1	.080
6.25	30	41-4440R	1	.080
9.00	36	41-4444L	2	.100
16.00	48	41-4445R	2	.100

W10 - SERIES

W10-1 R R

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
7.07	36	41-2201	1	.080

W11-1 ** LEGEND - BLACK
BACKGROUND - YELLOW

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.25	30	41-4840	1	.080

W11-15P ** TRAIL X-ING
LEGEND - BLACK
BACKGROUND - YELLOW

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
3.0	24X18	41-4839	1	.080

W11 - SERIES

W11-2

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.25	30	41-4810	1	.080
9.00	36	41-4811	1	.080
16.00	48	41-4812	2	.100

W11-8

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.25	30	41-4680	1	.080
9.00	36	41-4681	1	.080
16.00	48	41-4692	2	.100

W11-15 ** LEGEND - BLACK
BACKGROUND - YELLOW

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.25	30	41-4846	1	.080

W12 - SERIES

W12-1

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.25	30	41-4213	1	.080
9.00	36	41-4215	1	.080
16.00	48	41-4214	2	.100

LEFT LANE (SUBPLATE)

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
7.50	36X30	41-4729		.080

W12-2 00'-0"

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
9.00	36	41-4706	1	.080
16.00	48	41-4708	2	.100

RIGHT LANE (SUBPLATE)

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
7.50	36X30	41-4728		.080

W13 - SERIES

W13-1 00 M.P.H.

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
2.25	18X18	41-5568	1	.080
4.00	24X24	41-5569	1	.080
6.25	30X30	41-5573	1	.080
16.00	48X48	41-5572	2	.100

W13-2 EXIT 00 M.P.H.

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
20.00	48X60	41-5596	2	.100

W13-3 RAMP 00 M.P.H.

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
20.00	48X60	41-5598	2B	.100

W16 - SERIES

W16-2aP 41-6130 000 FT / 41-6132 000 FT / 41-6134 0000 FT
(SUBPLATE) USE AT ADVANCE SIGN LOCATION

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
2.00	24X12	41-6130		.080
3.75	30X18	41-6132		.080
7.00	42X24	41-6134		.100
2.0	24X12	✱ 41-6140		.080
3.75	30X18	✱ 41-6142		.080

W16-7PL (SUBPLATE) USE WITH S1-1 & W11-2 AT CROSSWALK

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
2.00	24X12	41-2607		.080
3.75	30X18	41-2608		.080
2.00	24X12	✱ 41-2617		.080
3.75	30X18	✱ 41-2618		.080

W16-7PR (SUBPLATE) USE WITH S1-1 & W11-2 AT CROSSWALK

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
2.00	24X12	41-2609		.080
3.75	30X18	41-2610		.080
2.00	24X12	✱ 41-2619		.080
3.75	30X18	✱ 41-2620		.080

W16-9P AHEAD

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
2.00	24X12	41-4333	1	.080
3.00	36X24	41-4335	1	.080
6.00	36X24	41-4330	2	.100
12.00	48X36	41-4115	2	.100

W16-9P AHEAD

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
2.00	24X12	✱ 41-6126	1	.080
3.75	30X18	✱ 41-6127	1	.080
6.00	36X24	✱ 41-6138	1	.080
12.00	48X36	✱ 41-6139	2	.100

W16-1P ** SHARE THE ROAD
LEGEND - BLACK
BACKGROUND - YELLOW

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
3.00	18X24	41-5906	1	.080

NOTES:

FOR METRIC SEE CONVERSION CHART.
1. FOR SPECIFIC SIGN DESIGN CONTACT CONN. D.O.T., DIVISION OF TRAFFIC ENGINEERING. FOR BOLT HOLE PATTERN REFER TO FHWA PUBLICATION "STANDARD HIGHWAY SIGNS". SIGNS OF DIFFERENT DIMENSIONS TO BE ERECTED ON THE SAME POSTS, OR SPAN/MAST ARM MOUNTED, MAY REQUIRE SPECIAL BOLT HOLE PATTERNS.
2. POSTS - SEE STANDARD SHEET TR-1208_02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS."
3. POSTS - TYPE A (EXCEPT WHERE NOTED WITH A "B" FOR TYPE B)
4. SIGNS SHALL BE FABRICATED OF ONE CONTINUOUS PIECE OF SHEET ALUMINUM. SPLICING OF SHEET ALUMINUM WILL NOT BE ACCEPTED.

** THIS SIGN IS ADDED FOR THIS PROJECT ONLY

COLORS:

S & W-SERIES
BACKGROUND - YELLOW - EXCEPT AS NOTED.
✱ BACKGROUND - FLUORESCENT YELLOW GREEN
LEGEND - BLACK - EXCEPT AS NOTED.
ALL COLORS SHALL BE TYPE III RETROREFLECTIVE WITH THE EXCEPTION OF BLACK WHICH SHALL BE OPAQUE.

REV.	DATE	REVISION DESCRIPTION
1	11-2013	UPDATED PER MUTCD.
1	1-2010	REVISED SHEETING FROM TYPE I TO III.

METRIC CONVERSION CHART (1" = 25mm)

ENGLISH	METRIC	ENGLISH	METRIC	ENGLISH	METRIC
12"	300	42"	1050	72"	1800
18"	450	48"	1200	78"	1950
24"	600	54"	1350	84"	2100
30"	750	60"	1500	90"	2250
36"	900	66"	1650	96"	2400

DESIGNER/DRAFTER: D.K. SWINBURNE
CHECKED BY: L.N. CONROY
Plotted: 4/30/2015

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

SIGNATURE/BLOCK:
OFFICE OF ENGINEERING
APPROVED BY: DATE:

PROJECT TITLE:

TOWN:

DRAWING TITLE:
SIGN FACE SHEET ALUMINUM (Y)
S&W SERIES SIGNS TYPICAL DETAILS

PROJECT NO.

DRAWING NO.
GS-2

SHEET NO.

Filename: CTDOT_TRAFFIC_GD.dgn

D1 - SERIES

D1-1
51-5202 (VARIABLE) →
D1-2
51-5203 (VARIABLE) (VARIABLE) →
VARIABLE LEGEND & ARROW DIRECTION

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
4.17	60X10	51-5202	2	.100
8.33	60X20	51-5203	2	.100

D1-1
51-5202 RIVERSIDE DR →

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
4.17	60X10	51-5202	2	.100

D1-1
51-5202 WILLOW ST ↑

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
4.17	60X10	51-5202	2	.100

D1-1
51-5202 GOODWIN COLLEGE →

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
4.17	60X10	51-5202	2	.100

D2 - SERIES

D2-1
51-5222 (VARIABLE) 00
D2-2
51-5223 (VARIABLE) (VARIABLE) 00
VARIABLE LEGEND & MILEAGE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
4.17	60X10	51-5222	2	.100
8.33	60X20	51-5223	2	.100

D3 - SERIES

(VARIABLE ROAD NAME)

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
4.00	48X12	51-2010	2	.100
5.00	60X12	51-2011	2	.100
6.00	72X12	51-2012	2	.100
7.00	84X12	51-2013	2	.100

D4 - SERIES

PARK & RIDE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
5.00	24X30	51-6001	1	.080

PARK & RIDE 51-6002
PARK & RIDE 51-6003

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
5.00	24X30	51-6002	1	.080
5.00	24X30	51-6003	1	.080

51-2666 51-2646 L or R
51-2645

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
0.75	12X9	51-2666		.080
2.19	21X15	51-2646		.080
2.19	21X15	51-2645		.080

51-2650 51-2651

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
2.19	21X15	51-2650		.080
2.19	21X15	51-2651		.080

NOTICE
THIS PARKING AREA PATROLLED
LEGEND - BLACK BACKGROUND - WHITE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
15.00	60X36	51-5937	2	.100

PARK AT YOUR OWN RISK
REMOVE KEYS AND LOCK CAR
NOT RESPONSIBLE FOR
PROPERTY LOST OR STOLEN
LEGEND - BLACK BACKGROUND - WHITE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
4.50	36X18	51-6037	1	.080

D5 - SERIES

D5-2b
REST AREA
LEGEND - WHITE BACKGROUND - BLUE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
42.25	78	51-6901	2B	.125

SERVICE AREA
LEGEND - WHITE BACKGROUND - BLUE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
35.00	84X60	51-6158	2B	.125

D7 - SERIES

D-7
51-6802 (VARIABLE) →
51-6803 (VARIABLE) (VARIABLE) →
W or W/O ARROW
VARIABLE ARROW DIRECTION

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
4.17	60X10	51-6802	2	.100
8.33	60X20	51-6803	2	.100

D-7
51-6803 RENTSCHLER ↑ FIELD

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
8.33	60X20	51-6803	2	.100

D9 - SERIES

D9-2
H
** FOR ARROW SUBPLATES USE M5 & M6 SERIES

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
4.00	24	51-6788	1	.080

D9-3a
** FOR ARROW SUBPLATES USE M5 & M6 SERIES

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
4.00	24	51-6762	1	.080

51-6705 DIESEL →
51-6706 FOOD →
51-6707 PHONE ↑
51-6708 GAS →
51-6709 LODGING ↑
VARIABLE ARROW DIRECTION

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
2.25	36X9		1	.080

D10 - SERIES

D10-1 MILE 0
D10-2 MILE 0 0
D10-3 MILE 0 0 0

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
2.00	12X24	51-5291	1	.080
3.00	12X36	51-5292	1	.080
4.00	12X48	51-5293	1	.080

N 0 .0 | N 0 0 .0 | N 0 0 0 .0
CARDINAL DIRECTION VARIABLE - N, S, E, W

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
1.88	10X27	51-5300	1	.080
2.49	10X36	51-5301	1	.080
3.33	10X48	51-5302	1	.080

NORTH 95 MILE 000
CARDINAL DIRECTION, SHIELD AND NUMERALS ARE VARIABLE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.75	18X54	51-5307	1	.080

RECREATIONAL RW- SERIES

RW-080

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
4.00	24	51-6873	1	.080

51-2705 51-2706 L or R

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
2.19	21X15	51-2705		.080
2.19	21X15	51-2706		.080

D11-1
** BIKE ROUTE
LEGEND - WHITE BACKGROUND - GREEN

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
3.0	24X18	51-1354	1	.080

E5 - SERIES

E5-1a
EXIT (L) | EXIT (R)
FOR USE AT "NO NUMBERED" EXITS

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. # (L or R)	POSTS	ALUM. THK.
30.00	72X60	51-6150	2B	.125

E5-1a
EXIT 00 (L) | EXIT 00 (R)

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. # (L or R)	POSTS	ALUM. THK.
30.00	72X60	51-6152	2B	.125
37.50	90X60	51-6153	2B	.125
47.50	114X60	51-6154	2B	.125

I - SERIES

(VARIABLE) RIVER
VARIABLE: RIVER, BROOK, CREEK

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
1.50	18X12	51-2009	1	.080
12.00	48X36	51-2051	2	.100

(TOWN NAME)
(INCORPORATED OR SETTLED) (DATE)
TOWN LINE
VARIABLE: TOWN / CITY

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
7.50	40X27	51-2020	2	.080

(NAME) Connecticut

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
10.00	48X30	51-2040	2	.100

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
6.25	30X30	51-1344		.080

M1 - SERIES

M1-1
INTERSTATE CONNECTICUT 00
VARIABLE: 1, 2 or 3 DIGITS
LEGEND - WHITE
BACKGROUND - RED & BLUE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
3.20	24X24	51-6662	1	.080
3.99	30X24	51-6663	1	.080
7.20	36X36	51-6666	2	.080
8.99	45X36	51-6667	2	.100

M1-4
00
VARIABLE: 1, 2 or 3 DIGITS
LEGEND - BLACK
BACKGROUND - WHITE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
4.00	24X24	51-6615	1	.080
9.00	36X36	51-6635	2	.080

M1-5
00
VARIABLE: 1, 2 or 3 DIGITS
LEGEND - BLACK
BACKGROUND - WHITE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
4.00	24X24	51-6616	1	.080
9.00	36X36	51-6636	2	.080

M2 - SERIES

M2-1
JCT

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	COLOR	ALUM. THK.
2.19	21X15	51-6640	(1)	.080
2.19	21X15	51-6610	(2)	.080

M3 - SERIES

M3-1
NORTH

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	COLOR	ALUM. THK.
2.00	24X12	51-6651	(1)	.080
2.00	24X12	51-6611	(2)	.080
3.12	30X15	51-6671	(1)	.080
3.12	30X15	51-6631	(2)	.080

M3-2
EAST

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	COLOR	ALUM. THK.
2.00	24X12	51-6652	(1)	.080
2.00	24X12	51-6612	(2)	.080
3.12	30X15	51-6672	(1)	.080
3.12	30X15	51-6632	(2)	.080

M3-3
SOUTH

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	COLOR	ALUM. THK.
2.00	24X12	51-6653	(1)	.080
2.00	24X12	51-6613	(2)	.080
3.12	30X15	51-6673	(1)	.080
3.12	30X15	51-6633	(2)	.080

M3-4
WEST

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	COLOR	ALUM. THK.
2.00	24X12	51-6654	(1)	.080
3.12	30X15	51-6674	(1)	.080
3.12	30X15	51-6634	(2)	.080

M4 - SERIES

M4-5
TO

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	COLOR	ALUM. THK.
2.00	24X12	51-6650	(1)	.080
2.00	24X12	51-6620	(2)	.080

M4-6
** END
LEGEND - WHITE BACKGROUND - GREEN

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	COLOR	ALUM. THK.
1.00	24X6	51-1356	(1)	.080

M5 - SERIES

M5-1
L or R

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	COLOR	ALUM. THK.
2.19	21X15	51-2690L	(1)	.080
2.19	21X15	51-2691R	(1)	.080
2.19	21X15	51-2630L	(2)	.080
2.19	21X15	51-2631R	(2)	.080

M5-2
L or R

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	COLOR	ALUM. THK.
2.19	21X15	51-2692L	(1)	.080
2.19	21X15	51-2693R	(1)	.080
2.19	21X15	51-2632L	(2)	.080
2.19	21X15	51-2633R	(2)	.080

M6 - SERIES

M6-1
L or R

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	COLOR	ALUM. THK.
2.19	21X15	51-2686	(1)	.080
2.19	21X15	51-2626	(2)	.080

M6-4

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	COLOR	ALUM. THK.
0.75	12X9	51-2667	(3)	.080
2.19	21X15	51-2697	(1)	.080
2.19	21X15	51-2627	(2)	.080

M6-2
L or R

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	COLOR	ALUM. THK.
2.19	21X15	51-2688L	(1)	.080
2.19	21X15	51-2689R	(1)	.080
2.19	21X15	51-2628L	(2)	.080
2.19	21X15	51-2629R	(2)	.080
4.89	32X22	51-2609R	(2)	.080

M6-3

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	COLOR	ALUM. THK.
2.19	21X15	51-2685	(1)	.080
2.19	21X15	51-2625	(2)	.080

** SYMBOL - YELLOW BACKGROUND - BLACK

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
2.25	18X18	51-5031	1	.080

SYMBOL - WHITE BACKGROUND - BLUE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
3.00	18X24	51-5943	1	.080

51-6504 (VARIABLE)
51-6505 (VARIABLE) (VARIABLE)
TO BE SUBMOUNTED W/ 51-5943
LEGEND - WHITE BACKGROUND - BLUE

AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.
0.75	18X6	51-6504	1	.080
1.50	18X12	51-6505	1	.080

NOTES:

FOR METRIC SEE CONVERSION CHART.

1. FOR SPECIFIC SIGN DESIGN CONTACT CONN. D.O.T., DIVISION OF TRAFFIC ENGINEERING. FOR BOLT HOLE PATTERN REFER TO FHWA PUBLICATION "STANDARD HIGHWAY SIGNS". SIGNS OF DIFFERENT DIMENSIONS TO BE ERECTED ON THE SAME POSTS, OR SPAN/MAST ARM MOUNTED, MAY REQUIRE SPECIAL BOLT HOLE PATTERNS.
2. POSTS - SEE TYP. SHEET (SHT #9) - "TYPICAL METAL SIGN POSTS AND SIGN MOUNTING DETAILS."
3. POSTS - TYPE A (EXCEPT WHERE NOTED WITH A "B" FOR TYPE B)
4. SIGNS SHALL BE FABRICATED OF ONE CONTINUOUS PIECE OF SHEET ALUMINUM. SPLICING OF SHEET ALUMINUM WILL NOT BE ACCEPTED.

** THIS SIGN IS ADDED FOR THIS PROJECT ONLY

COLORS:

D - SERIES
D1, 2, 3, 4, 10 BACKGROUND-GREEN, LEGEND-WHITE
D7, RW BACKGROUND-BROWN, LEGEND-WHITE
D9 BACKGROUND-BLUE, LEGEND-WHITE
ALL OTHER D-SERIES AS NOTED
E - SERIES
BACKGROUND-GREEN, LEGEND-WHITE
I - SERIES
BACKGROUND-GREEN, LEGEND-WHITE (EXCEPT AS NOTED)
M2 - M6 SERIES
(1) BACKGROUND-BLUE, LEGEND-WHITE
(2) BACKGROUND-WHITE, LEGEND-BLACK
(3) BACKGROUND-GREEN, LEGEND-WHITE

ALL COLORS SHALL BE TYPE III RETROREFLECTIVE WITH THE EXCEPTION OF BLACK WHICH SHALL BE OPAQUE.

REV.	DATE	REVISION DESCRIPTION
1	1-2010	REVISED SHEETING FROM TYPE I TO III.

METRIC CONVERSION CHART (1" = 25mm)

ENGLISH	METRIC	ENGLISH	METRIC	ENGLISH	METRIC
12"	300	42"	1050	72"	1800
18"	450	48"	1200	78"	1950
24"	600	54"	1350	84"	2100
30"	750	60"	1500	90"	2250
36"	900	66"	1650	96"	2400

DESIGNER/DRAFTER: D.K. SWINBURNE
CHECKED BY: L.N. CONROY
Plotted: 6/8/2015

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

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TOWN:

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SIGN FACE SHEET ALUMINUM
D,E,I,&M SERIES SIGNS TYPICAL DETAILS

PROJECT NO.

DRAWING NO.
GS-3

SHEET NO.